

**ADDENDUM TO AN ADOPTED MITIGATED NEGATIVE DECLARATION**

The City of Sacramento, California, a municipal corporation, does hereby prepare, make declare, and publish the Addendum to an adopted Mitigated Negative Declaration for the following described project:

**Mack Road Mini Storage Revision (P18-076)**

The City of Sacramento, Community Development Department, has reviewed the proposed project and on the basis of the whole record before it, has determined that there is no substantial evidence that the project would have a significant effect on the environment beyond that which was evaluated in the attached Mitigated Negative Declaration (MND) for the Mack Road Storage Facility (P04-176). A Subsequent MND is not required pursuant to the California Environmental Quality Act of 1970 (Sections 21000, et. Seq., Public Resources Code of the State of California).

This Addendum to an adopted MND has been prepared pursuant to Title 14, Section 15164 of the California Code of Regulations; the Sacramento Local Environmental Regulations (Resolution 91-892) adopted by the City of Sacramento.

A copy of this document and all supportive documentation may be reviewed or obtained at the City of Sacramento, Community Development Department, Planning Division, 300 Richards Boulevard, Sacramento, California 95811.

Environmental Services Manager, City of Sacramento,  
California, a municipal corporation

By: 

Date: April 23, 2019

**Mack Road Storage Facility (P18-076)**  
**Addendum to Mitigated Negative Declaration**

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**File Number/Project Name:** Mack Road Storage Facility (P18-076)

**Project Location:** 5701 Mack Road (APNs 118-0111-062, -063, -064, and -065)

**Existing Plan Designations and Zoning:** The proposed project is zoned C-2 – General Commercial. The existing 2035 General Plan land use designation for the site is Regional Commercial. The site is in the South Area Community Plan Area.

**Project Background:** The project site was approved to be developed with a 79,919 square feet mini storage facility located on 7.68 acres of land (P04-146). (See Attachment D)

Following project approval, the site has remained vacant land.

**Project Description:** This proposed project would modify the previously approved project as follows: add 10,697 square feet to the total square footage of the proposed storage buildings, reconfigure the storage buildings, which includes eliminating a portion of the proposed storage building on the northeastern portion; add a new building to the southwestern portion, relocate RV parking spaces from the southern portion to the northern portion, remove one on-site residence (presumably for security) and replace a proposed 7 to 12 foot high concrete masonry perimeter wall with a mix of perimeter wall/fence solutions.

Changes are briefly summarized as follows:

| <b>Project Components</b> | <b>Original Plan</b>   | <b>Revised Plan</b>    | <b>Change</b>            |
|---------------------------|------------------------|------------------------|--------------------------|
| Number of buildings       | 5 buildings            | 6 buildings            | +1 building              |
| Building square feet      | 79,919 ft <sup>2</sup> | 90,616 ft <sup>2</sup> | + 10,697 ft <sup>2</sup> |
| Number of RV spaces       | 137 spaces             | 100 spaces             | - 37 spaces              |

**Discussion**

An Addendum to an adopted mitigated negative declaration may be prepared if only minor technical changes or additions are required, and none of the conditions identified in CEQA Guidelines Section 15162 are present. The following identifies the standards set forth in section 15162 as they relate to the project.

- 1. No substantial changes are proposed in the project which would require major revisions of the previous MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.**

*The previously adopted MND included mitigation for cultural resources (Mitigation Measures CR-1 and CR-2). Since new excavation work will proceed with the proposed project, the mitigation measure for cultural resources will remain in effect and will be included in the mitigation monitoring program.*

*The proposed modifications to the project do not increase any previously identified impacts in the original project as evaluated in previous Mitigated Negative Declaration, approved on March 10, 2005, in P04-146.*

- 2. No substantial changes have occurred with respect to circumstances under which the project is undertaken that would require major revisions of the previous MND due to the involvement of new significant environmental effect or a substantial increase in the severity of previously identified significant effects.**

*The new information and evaluations are considered to be technical changes and do not include new impacts that have not already been discussed in the previous Mitigated Negative Declaration.*

- 3. No new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous MND was certified as complete or adopted, shows any of the following:**

- a) The project will have one or more significant effects not discussed in the previous MND;
- b) Significant effects previously examined will be substantially more severe than shown in the previous MND;
- c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative, or;
- d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous MND would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

*Based on the limited changes to the project description, there are no sections in the Initial Study checklist that require revisions and therefore will not result in environmental impacts that were not previously identified in the Mitigated Negative Declaration.*

*The proposed revised project description will not result in effects any more significant than those evaluated in the 2004 Mitigated Negative Declaration. Mitigation measures adopted for the previous Mitigated Negative Declaration are consistent with what has been previously analyzed.*

Section 15162 provides that the lead agency's role in a project approval is completed upon certification of the Environmental Impact Report or Negative Declaration (in this case Mitigated Negative Declaration) and approval of the project, unless further discretionary action is required. The approvals requested as part of the proposed project are considered discretionary actions, and CEQA review is therefore required.

The original Mitigated Negative Declaration for the Mack Road Mini Storage (P04-146), adopted on March 10, 2005, evaluated the property for development with a mini storage facility. The changes to the project since 2004 consist of an additional 10,697 square feet to 90,616 square feet of building space and a reduction of RV parking spaces from 137 spaces to 100 RV spaces.

#### Land Use, Agricultural, Aesthetics

The 2004 Initial Study/Mitigated Negative Declaration (2004 IS/MND) indicated that the project was located in the South Sacramento Community Plan and was zoned General Commercial Review (C-2-R). The project was not being used for agricultural purposes. The project was not located within an adopted view or scenic vista. The proposed project is consistent with the findings of the 2004 IS/MND.

#### Air Quality and Greenhouse Gas Emissions

Air quality and greenhouse gas modeling was completed for the revised project. The following table summarizes air quality impacts generated by the model for the modified project and the Sacramento Metropolitan Air Quality Management District (SMAQMD) Threshold of Significance:

|                                     | Nitrogen Oxide<br>(pounds per day) | Particulate<br>Matter 10<br>(pounds per day) | Particulate<br>Matter 2.5<br>(pounds per day) | Reactive Organic<br>Gases<br>(pounds per day) | Carbon Dioxide<br>equivalents<br>(Metric<br>Tons/Year) |
|-------------------------------------|------------------------------------|--|---|---|--|
| Construction 2019                   | 45.61                              | 20.59  | 12.16   | 8.21  | 391  |
| Construction 2020                   | 37.40                              | 2.65   | 2.04  | 7.80  | 300  |
| Operational-Related<br>Emissions    | 6.64                               | 4.41   | 1.23  | 4.24  | 947  |
| SMAQMD Threshold<br>of Significance | 65                                 | 80   | 82  | 65  | 1,100  |

The air quality model generated predicted maximum daily construction-generated emissions and operational-generated emissions, which were below the Threshold of Significance established by the SMAQMD. The air quality greenhouse gas modeling report is attached as Attachment C, which found no significant impact related to the proposed project changes.

The City has since adopted the 2035 General Plan Update. The update incorporated measures and actions from the CAP into Appendix B, General Plan CAP Policies and Programs, of the General Plan Update. Appendix B includes all City-Wide policies and programs that are supportive of reducing GHG emissions. The General Plan CAP Policies and Programs per the General Plan Update supersede the City's CAP. Rather than compliance and consistency with the CAP, all proposed projects must now be compliant and consistent with the General Plan CAP Policies and Programs outlined in Appendix B of the General Plan Update. As such, the proposed project would be required to comply with the General Plan CAP Policies and Programs set forth in Appendix B of the General Plan Update.

In addition to the City's General Plan CAP Policies and Programs outlined in Appendix B of the General Plan Update, a number of regulations have been enacted since the Mack Road Mini Storage (P04-146) was approved for the purpose of, or with an underlying goal for, reducing GHG emissions, such as the California Green Building Standards Code (CALGreen Code) and the California Building Energy Efficiency Standards Code. Such regulations have become increasingly stringent since the Mack Road Mini Storage (P04-146) MND was adopted. The proposed project would be required to comply with all current applicable regulations associated with GHG emissions, including the CALGreen Code and California Building Energy Efficiency Standards Code.

### Biological Resources

The 2004 IS/MND stated that the project site consisted of vacant, partially disturbed land dominated by annual grassland species. It concluded that the existing site conditions, the lack of habitat and the absence of evidence of special status species occurring on the site, impacts would be less-than-significant. No changes have occurred at the project site that would require new mitigation measures.

In preparing this addendum to the adopted 2004 IS/MND, WKA reviewed the California Natural Diversity Database (CNDDB) then conducted a follow up site inspection April 8, 2019. The CNDDB

reported no observations of listed species or critical habitat on the site. The closest listing of a special status species occurrence was for the plant *Sagittaria sanfordii* (Sanford's arrowhead) approximately 0.15 miles west of the property, west of Center Parkway and a siting of *Buteo swainsonii*, Swainson's Hawk, approximately 1-mile northwest of the site. On the day of the inspection, the site was covered with annual grasses. The overall condition of the property remains consistent the observations documented in the 2004 and no new information was found that would change the conclusions of the original IS/MND.

### Cultural Resources

The project site has been highly disturbed and segments of asphalt driveways were present in 2004. The 2004 IS/MND indicated that no known cultural or historical resources were identified for the project site. The 2004 IS/MND did indicate that previously unidentified cultural or historical resources may be identified during construction activities and that mitigation measures should be imposed. The proposed project is required to comply with 2004 Mitigation Measures CR-1 and CR-2.

### Hazards

The proposed project site is vacant land and has historically been vacant land. Structures do not exist, so no demolition will occur. The 2004 IS/MND noted that the project would be required to comply with all federal, state, and local laws concerning hazardous materials. A recent search of the California Department of Toxic Substances Control (DTSC) EnviroStor Database and the California Regional Water Quality Control Board (RWQCB) GeoTracker Database provided no record of identified hazardous materials contamination at the site. It also noted that the development of the property would reduce the possibility of flammable brush being present on the property. The proposed project is consistent with the findings of the 2004 IS/MND.

### Hydrology and Water Quality

The 2004 IS/MND indicated that there was an existing on-site drainage from earlier site improvements. The existing drainage was to be abandoned. No surface waters are located on the site; however, a channelized portion of Elder Creek is located adjacent to the north. Because the project will disturb greater than one acre, California law requires the project developer (as a project condition) to file a Notice of Intent with the State Water Resources Control Board to obtain a General Construction Activity Storm Water Permit prior to construction. Prior to construction, the developer will also be required to prepare a Stormwater Pollution Prevention Plan (SWPPP). As indicated in the 2004 IS/MND, the proposed project is expected to have a less-than-significant impact with regard to compliance of all applicable regulatory requirements.

### Noise

The 2004 IS/MND indicated that the main source of noise in the project area is from Mack Road and noted that noise in the area may temporarily increase due to grading and construction

activities. The 2004 IS/MND stated that “noise levels from Mack Road are listed at 70 dB Ldn at 75 feet from the centerline (SGPU DEIR, AA-25). The proposed mini storage facility is not anticipated to be impacted from the noise generated from Mack Road as the normally acceptable exterior noise levels for this type of use are 70 dB Ldn.” The 2004 project studied in the IS/MND proposed a 7 to 12-foot masonry wall surrounding the facility; and used the wall to imply that noise levels from Mack Road would be reduced.

The new project is essentially the same as that proposed under the 2004 adopted IS/MND with the minor changes outlined in the introduction section. The new project will differ in that there will be no on-site residence and the perimeter security fencing will now consist of a mix of options including: a 6-foot pre-cast textured concrete wall along the south side (Mack Road) with 30-foot wrought iron-fenced aesthetic gaps every 60 feet of wall; solid sections of 6-foot CMU or pre-cast concrete wall along the west and southeast sides, chain-link fence along Elder Creek to the north and up to 18 feet of building sidewall along the east side. Because the noise generated from Mack road is the dominant source in the area and because the project is largely the same as that approved in 2004, noise issues related to the proposed project and it’s construction are considered less-than-significant.

### Public Services

According to the 2004 IS/MND, government services would be needed but would not result in the need to construct new facilities. The proposed project is consistent with the zoning and 2035 General Plan designations and the project would have a less-than-significant impact.

### Transportation and Circulation

The 2004 IS/MND indicated that traffic from the development of a mini storage facility is not expected to create a significant impact on the existing roadway system. Public improvements for the proposed project will be designed to the appropriate, applicable standards. The traffic generation rate from the revised project would be generally similar to what was anticipated for the project area in the 2004 IS/MND. Therefore, no significant additional traffic impact is anticipated from the proposed project. The City of Sacramento, Department of Public Works reviewed the project on March 20, 2019 and had no comments.

### Utilities

The proposed project would not result in a significant change in water demand. The proposed project would not generate wastewater in excess of what was anticipated in the 2004 IS/MND. The proposed project is expected to generate similar amounts of solid waste as anticipated in the 2004 IS/MND. The Department of Utilities has reviewed this project and determined that this project will be required to prepare a project specific water study and construct a water main extension on Mack Road to the satisfaction of the DOU. The project will be increasing imperviousness, consistent with the previously approved project, and will be required to design the drainage system consistent with

the DOU current design and procedures manual. Therefore, no significant additional impacts to utilities is expected from the proposed project.

### Conclusion

As established in the discussions above regarding potential effects of the proposed project, substantial changes are not proposed for the project and no substantial changes have occurred that would require major revisions to the 2004 IS/MND. The Mack Road Mini Storage (P04-146) MND retains relevance and substantial evidence supports the use of IS/MND and subsequent review provisions of the CEQA Guidelines Section 15162. There is no substantial evidence of a fair argument that major revisions are required to the 2004 IS/MND. Overall, the proposed modifications to the project would not result in any new information of substantial importance that would have new, more significant impacts, new mitigation measure, or new or revised alternatives that was identified for the original project in the 2004 IS/MND. The analysis conducted, and the conclusions reached in the MND adopted on March 10, 2005, remain relevant and valid and an Addendum is the appropriate document. The proposed project would not result in any conditions identified in the CEQA Guidelines Section 15162, and neither a subsequent EIR or MND is required for the proposed project modifications. The propose project would be subject to all applicable previously required mitigation measures from the 2004 MND.

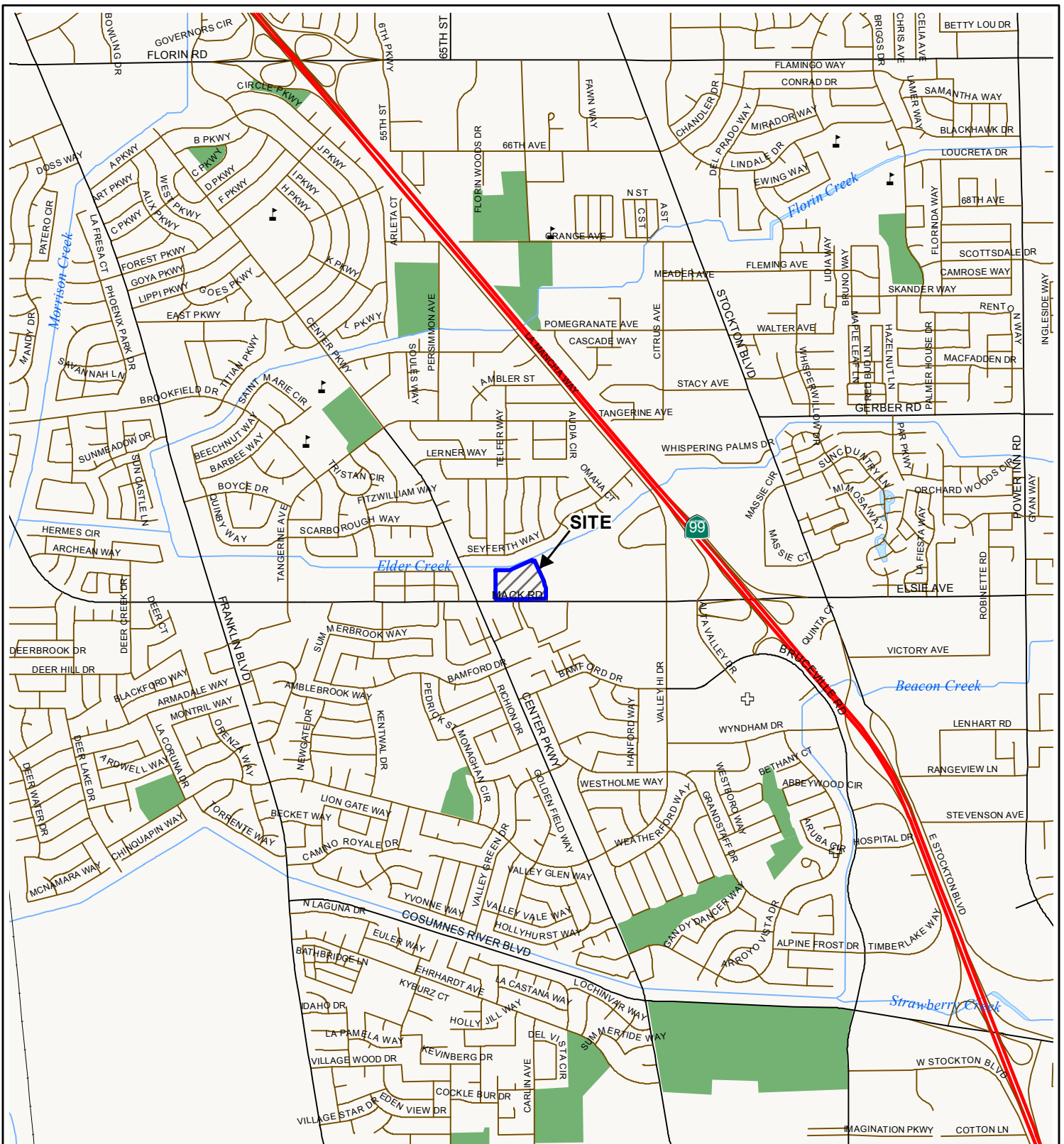
**Based on the above analysis, this Addendum to the previously adopted Mitigated Negative Declaration for the project has been prepared.**

### **Attachments:**

- A) *Vicinity Map*
- B) *Site Plan*
- C) *Mack Road Mini Storage Modified Project – Emissions Memorandum*
- D) *Notice of Decision and Findings of Fact for Mack Road Mini Storage Located at 5701 Mack Road Sacramento, California in the General Commercial Review (C-2-R) Zone (P04-146)*
- E) *Initial Study/Mitigated Negative Declaration for Mack Road Storage Facility (P04-146)*



**ATTACHMENT A**  
**Vicinity Map**



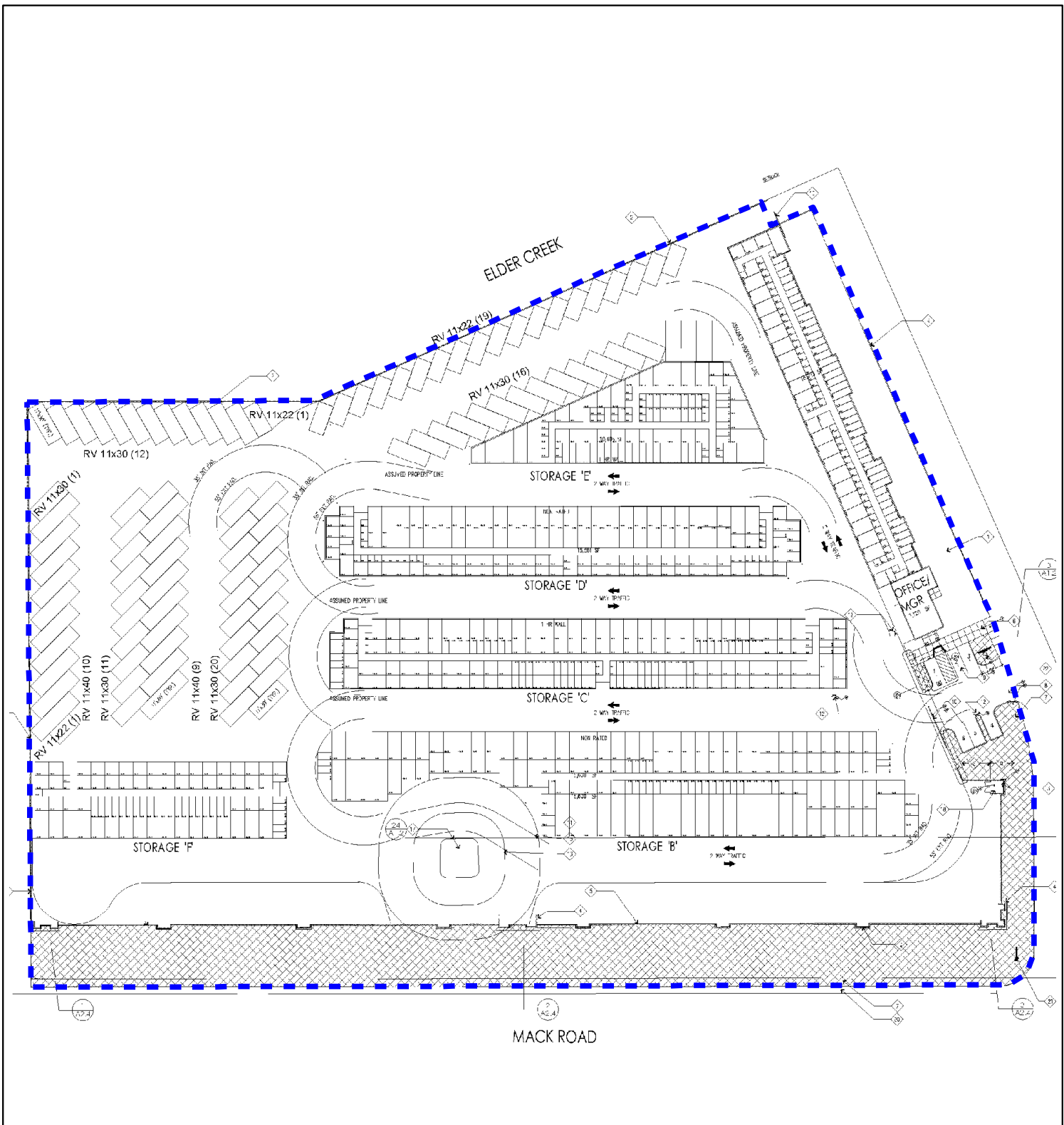
Street data courtesy of Sacramento County.  
 Hydrography courtesy of the U.S. Geological Survey  
 acquired from the GIS Data Depot, December, 2007.  
 Projection: NAD 83, California State Plane, Zone II



**VICINITY MAP**  
**MACK ROAD STORAGE FACILITY (P18-076)**  
 Sacramento, California

|                         |       |
|-------------------------|-------|
| <b>FIGURE 1</b>         |       |
| DRAWN BY                | RWO   |
| CHECKED BY              | NMM   |
| PROJECT MGR             | KMB   |
| DATE                    | 03/19 |
| <b>WKA NO. 12017.02</b> |       |

**ATTACHMENT B**  
**Site Plan**



**Legend**

 Approximate Site Boundary

N



0 50 100  
Feet

**SITE PLAN**

MACK ROAD STORAGE FACILITY (P18-076)

Sacramento, California

**FIGURE 2**

|             |       |
|-------------|-------|
| DRAWN BY    | RWO   |
| CHECKED BY  | NMM   |
| PROJECT MGR | KMB   |
| DATE        | 03/19 |

WKA NO. 12017.02



**ATTACHMENT C**  
**Mack Road Mini Storage Modified Project – Emissions Memorandum**

April 15, 2019

Kurt Balasek, PG, CHG, QSD  
Wallace-Kuhl & Associates  
3050 Industrial Boulevard  
West Sacramento, CA 95691

**RE: Mack Road Mini Storage Modified Project – Emissions Memorandum**

Dear Mr. Balasek:

ECORP Consulting, Inc. (Seth Myers- Senior AQ and Noise Specialists) has conducted an Emissions Memorandum for the proposed modifications to the approved Mack Road Mini Storage Project (Modified Project) located in City of Sacramento, California. The purpose of this memorandum is to assess any potential impacts to air quality and/or greenhouse gas (GHG) emissions as a result.

## **INTRODUCTION**

This memorandum was prepared in support of an addendum to an adopted mitigated negative declaration for the originally approved Mack Road Mini Storage Project (Originally Approved Project) located in Sacramento, California. The original analysis evaluated the air quality impacts associated with a 79,919 square foot, five building mini storage facility with 137 RV storage spaces. This memorandum addresses the affects to air quality and GHG emissions associated with proposed changes to the Originally Approved Project. The Modified Project includes a ±90,000 square foot, six building mini storage facility with 100 RV storage spaces. The Modified Project represents an increase of ±10,081 square feet of building space and a reduction of 37 RV storage spaces.

The original Initial Study was approved in 2004 and does not evaluate the effects of GHG emissions generated since such was not required at that time. In 2010 amendments to the State CEQA Guidelines took effect which set forth requirements for the analysis of GHG emissions under CEQA. GHG emissions are analyzed in this memorandum. This memorandum compares emissions of the Originally Approved Project to that of the Modified Project as well as to the significant thresholds identified by the Sacramento Metropolitan Air Quality Management District (SMAQMD), the air pollution control officer with jurisdiction over the area.

Air Quality and GHG impacts are assessed in accordance with methodologies recommended by the California Air Resource Board (CARB) and the SMAQMD. Where quantification was required, emissions were modeled using the California Emissions Estimator Model (CalEEMod), version 2016.3.2. CalEEMod is a statewide land use emissions computer model designed to quantify potential criteria pollutants emissions associated with both constructions and operations from a variety of land use projects.

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## **PROJECT LOCATION & DESCRIPTION**

The Modified Project consists of a ±90,000 square foot, six building mini storage facility with 100 RV storage spaces located on an undeveloped 7.52-acre parcel located at 5701 Mack Road in the South Sacramento Community Plan in the City of Sacramento. The site is generally bound by residences to the north and east, Mack Road to the south, and a retail strip mall to the west. The site has a General Plan land use designation of Regional Commercial.

### **Air Quality**

#### ***Environmental Setting***

The Modified Project area is located within Sacramento County. CARB has divided California into regional air basins according to topographic features. Sacramento County and the site area are located in a region identified as the Sacramento Valley Air Basin (SVAB). The SVAB is bound by the Coast and Diablo Mountain Ranges to the west, the Sierra Nevada Mountain Range to the east, and the San Joaquin Valley to the south. Air flows into the SVAB through the Carquinez Strait, moving air across the Sacramento Delta, and bringing with it pollutants from the San Francisco Bay Area. The climate is characterized by hot, dry summers and cold, rainy winters.

Both the U.S. Environmental Protection Agency (USEPA) and the CARB have established ambient air quality standards for common pollutants. These ambient air quality standards are levels of contaminants representing safe levels that avoid specific adverse health effects associated with each pollutant. The ambient air quality standards cover what are called “criteria” pollutants because the health and other effects of each pollutant are described in criteria documents. The six criteria pollutants are ozone (O<sub>3</sub>) (precursor emissions include nitrogen oxide (NO<sub>x</sub>) and reactive organic gases (ROG)), carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO<sub>2</sub>), sulfur dioxide (SO<sub>2</sub>), and lead. Areas that meet ambient air quality standards are classified as attainment areas, while areas that do not meet these standards are classified as nonattainment areas. The SVAB region is designated as a nonattainment area for the state standards for O<sub>3</sub> and PM<sub>10</sub>.

#### ***Regulatory Setting***

The local air quality agency affecting the SVAB is the SMAQMD, which is charged with the responsibility of implementing air quality programs and ensuring that national and state ambient air quality standards are not exceeded and that air quality conditions are maintained in the SVAB. In an attempt to achieve national and state ambient air quality standards and maintain air quality, the air district has completed the several air quality attainment plans and reports, which together constitute the State Implementation Plan (SIP) for the portion of the SVAB encompassing the Modified Project.

The SMAQMD has also adopted various rules and regulations for the control of stationary and area sources of emissions, including Rule 403 (Fugitive Dust). This rule requires fugitive dust sources to implement best available control measures for all sources, and all forms of visible particulate matter are prohibited from crossing any property line. This rule is intended to reduce coarse particulate matter (PM<sub>10</sub>) emissions from any transportation, handling, construction, or storage activity that has the potential to generate fugitive dust.

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## ***Air Quality (III) Environmental Checklist and Discussion***

### ***Would the Project conflict or obstruct implementation of the applicable air quality plan?***

As part of its enforcement responsibilities, the EPA requires each state with nonattainment areas to prepare and submit a State Implementation Plan (SIP) that demonstrates the means to attain the federal standards. The SIP must integrate federal, state, and local plan components and regulations to identify specific measures to reduce pollution in areas that do not meet federal and/or state air quality standards (nonattainment areas), using a combination of performance standards and market-based programs. Similarly, under state law, the California Clean Air Act requires an air quality attainment plan to be prepared for areas designated as nonattainment with regard to the federal and state ambient air quality standards. Air quality attainment plans outline emissions limits and control measures to achieve and maintain these standards by the earliest practical date. The site lies within the boundaries of the SVAB and is in nonattainment for exceeding state and federal criteria pollutant levels. The SMAQMD incorporates the latest scientific and technical information and planning assumptions, including the latest population growth forecasts for the SMAQMD jurisdiction, to implement into its air quality improvement strategies.

Projections for achieving regional air quality goals are based on assumptions regarding population, housing, and growth trends. A project is consistent with regional air quality planning efforts if it is consistent with the population, housing, and employment assumptions that were used in the development of the SMAQMD air quality plans. Thus, determining project consistency with the SMAQMD air quality efforts focuses on whether or not the Modified Project exceeds the assumptions utilized in preparing the forecasts presented in its air quality planning documents. The primary source of data employed to form the basis for the projections of air pollutant emissions in Sacramento, which encompasses the site, is the City of Sacramento General Plan.

The Modified Project is consistent with the land use designation and development density presented in the City of Sacramento General Plan. The site is designated by the City of Sacramento General Plan as Regional Commercial and the Modified Project proposes a land use allowed under the Regional Commercial designation and is thus consistent with the intensity and patterns of land use envisioned for the site vicinity in the General Plan. Additionally, the Modified Project is located within the Southern Area Community Plan Area, which is a policy document that provides further development-level guidance specific to a defined Sacramento 'Community'. For instance, Southern Area Community Plan Area contains Policy SA.ED 1.5, *Mack Road Business Improvement*, which seeks to support the development of new businesses along the Mack Road corridor. The Modified Project is consistent with the Community Policy SA.ED 1.5 and the Southern Area Community Plan Area.

For these reasons, the Modified Project would not conflict with or obstruct the implementation of SMAQMD air quality planning efforts.

### ***Would the Project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?***

By its very nature, air pollution is largely a cumulative impact. No single project is sufficient in size, by itself, to result in nonattainment of ambient air quality standards. Instead, a project's individual



emissions contribute to existing cumulatively significant adverse air quality impacts. If a project's individual emissions exceed its identified significance thresholds, the project would be cumulatively considerable. Projects that do not exceed significance thresholds would not be considered cumulative considerable.

The Modified Project would emit air pollutant emissions during construction activities. Operations at the storage facility would predominately result from the operation of motor vehicles traveling to and from the site. For purposes of this analysis, air quality impacts have been separated into construction impacts and operational impacts. Additionally, the emissions associated with the Originally Approved Project will be compared to that of the Modified Project.

### Construction Emission Impacts

The SMAQMD identifies significance construction thresholds for NO<sub>x</sub>, PM<sub>10</sub>, and fine particulate matter (PM<sub>2.5</sub>). Predicted maximum daily construction-generated emissions of criteria air pollutants for the both Projects are summarized in **Table 1**.

| <b>Table 1. Construction-Related Emissions.</b>                |                                   |                        |                         |
|--|-----------------------------------|------------------------|-------------------------|
| <b>Construction Year</b>                                       | <b>Pollutant (pounds per day)</b> |                        |                         |
|  | <b>NO<sub>x</sub></b>             | <b>PM<sub>10</sub></b> | <b>PM<sub>2.5</sub></b> |
| <b>Modified Project</b>  |                                   |                        |                         |
| <b>Construction 2019</b>                                       | 45.61                             | 20.59                  | 12.16                   |
| <b>Construction 2020</b>                                       | 37.40                             | 2.65                   | 2.04                    |
| <i>SMAQMD Threshold of Significance</i>                        | 85                                | 80                     | 82                      |
| <b>Exceed SMAQMD Threshold?</b>                                | <b>No</b>                         | <b>No</b>              | <b>No</b>               |
| <b>Originally Approved Project</b>                             |                                   |                        |                         |
| <b>Construction 2019</b>                                       | 45.61                             | 20.59                  | 21.16                   |
| <b>Construction 2020</b>                                       | 37.52                             | 2.69                   | 2.05                    |
| <i>SMAQMD Threshold of Significance</i>                        | 85                                | 80                     | 82                      |
| <b>Exceed SMAQMD Threshold?</b>                                | <b>No</b>                         | <b>No</b>              | <b>No</b>               |
| <b>Difference in Original Project v. Modified Project 2019</b> | <b>0.00</b>                       | <b>0.00</b>            | <b>0.00</b>             |
| <b>Difference in Original Project v. Modified Project 2020</b> | <b>-0.12</b>                      | <b>-0.04</b>           | <b>-0.01</b>            |

Source: CalEEMod version 2016.3.2. Refer to **Appendix A** for Model Data Outputs.

Notes: Emissions estimates account for the site preparation and grading of 7.52 acres. Building construction, paving, and painting assumed to occur simultaneously.

SMAQMD states that projects generating less than 80 pounds of PM<sub>10</sub> and less than 82 pounds of PM<sub>2.5</sub> daily while also implementing SMAQMD's Basic Construction Emission Control Practices, known as Best Management Practices (BMPs) are considered less than significant.

As shown in **Table 1**, construction-generated emissions would not exceed the SMAQMD significance thresholds. Additionally, the construction-generated emissions associated with the Modified Project would be reduced compared to that of the Originally Approved Project.

**Operational Emission Impacts**

The SMAQMD identifies significance construction thresholds for NO<sub>x</sub>, ROG, PM<sub>10</sub>, and PM<sub>2.5</sub>. Predicted maximum daily construction-generated emissions of criteria air pollutants for the both the Originally Approved Project and the Modified Project are summarized in **Table 2**.

| <b>Table 2. Operational-Related Emissions.</b>                 |                                   |              |                        |                         |
|--|-----------------------------------|--------------|------------------------|-------------------------|
| <b>Source</b>  | <b>Pollutant (pounds per day)</b> |              |                        |                         |
|  | <b>NO<sub>x</sub></b>             | <b>ROG</b>   | <b>PM<sub>10</sub></b> | <b>PM<sub>2.5</sub></b> |
| <b>Modified Project</b>  |                                   |              |                        |                         |
| Area   | 0.00                              | 2.17         | 0.00                   | 0.00                    |
| Energy   | 0.31                              | 0.03         | 0.02                   | 0.02                    |
| Mobile   | 6.33                              | 2.03         | 4.39                   | 1.20                    |
| <b>Total</b>   | <b>6.64</b>                       | <b>4.24</b>  | <b>4.41</b>            | <b>1.23</b>             |
| <i>SMAQMD Threshold of Significance</i>                        | 65                                | 65           | 80                     | 82                      |
| <b>Exceed SMAQMD Threshold?</b>                                | <b>No</b>                         | <b>No</b>    | <b>No</b>              | <b>No</b>               |
| <b>Originally Approved Project</b>                             |                                   |              |                        |                         |
| Area   | 0.00                              | 1.93         | 0.00                   | 0.00                    |
| Energy   | 0.28                              | 0.03         | 0.02                   | 0.02                    |
| Mobile   | 5.61                              | 1.81         | 3.90                   | 1.07                    |
| <b>Total</b>   | <b>5.89</b>                       | <b>3.77</b>  | <b>3.92</b>            | <b>1.09</b>             |
| <i>SMAQMD Threshold of Significance</i>                        | 65                                | 65           | 80                     | 82                      |
| <b>Exceed SMAQMD Threshold?</b>                                | <b>No</b>                         | <b>No</b>    | <b>No</b>              | <b>No</b>               |
| <b>Difference in Original Project v. Modified Project 2020</b> | <b>+0.75</b>                      | <b>+0.47</b> | <b>+0.49</b>           | <b>+0.14</b>            |

Source: CalEEMod version 2016.3.2. Refer to **Appendix A** for Model Data Outputs.

Notes: Emissions estimates account for the site preparation and grading of 7.52 acres. Building construction, paving, and painting assumed to occur simultaneously.

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As indicated in **Table 2**, operational-generated emissions associated with the Modified Project would not exceed SMAQMD significance thresholds. Additionally, the increase in emissions is negligible compared to the Originally Approved Project.

***Would the Project expose sensitive receptors to substantial pollutant concentrations?***

Sensitive receptors are defined as facilities or land uses that include members of the population that are particularly sensitive to the effects of air pollutants, such as children, the elderly, and people with illnesses. Examples of these sensitive receptors are residences, schools, hospitals, and daycare centers. The CARB has identified the following groups of individuals as the most likely to be affected by air pollution: the elderly over 65, children under 14, athletes, and persons with cardiovascular and chronic respiratory diseases such as asthma, emphysema, and bronchitis. The nearest sensitive receptor to the Project site is a single-family residence located approximately 75 feet to the east.

**Construction-Generated Air Contaminants**

Construction-related activities would result in temporary, short-term emissions of diesel particulate matter (DPM) from the exhaust of off-road, heavy-duty diesel equipment for site preparation (e.g., clearing, grading); soil hauling truck traffic; paving; and other miscellaneous activities. For construction activity, DPM is the primary TAC of concern. Particulate exhaust emissions from diesel-fueled engines (i.e., DPM) were identified as a TAC by the CARB in 1998. The potential cancer risk from the inhalation of DPM, as discussed below, outweighs the potential for all other health impacts (i.e., non-cancer chronic risk, short-term acute risk) and health impacts from other TACs. Accordingly, DPM is the focus of this discussion.

Based on the emission modeling conducted for the Modified Project, the maximum construction-related emissions of exhaust  $PM_{2.5}$ , considered a surrogate for DPM, would be 2.2 pounds per day during construction in the year 2019 and 1.9 pounds per day during construction activity occurring in 2020 (see **Appendix A**). ( $PM_{2.5}$  is considered a surrogate for DPM because more than 90 percent of DPM is less than 1 microgram in diameter and therefore is a subset of particulate matter under 2.5 microns in diameter (i.e.,  $PM_{2.5}$ ), according to CARB. Most  $PM_{2.5}$  derives from combustion, such as use of gasoline and diesel fuels by motor vehicles.) Furthermore, even during the most intense month of construction, emissions of DPM would be generated from different locations on the site, rather than a single location, because different types of construction activities (e.g., site preparation, grading, paving) would not occur at the same place at the same time.

The dose to which receptors are exposed is the primary factor used to determine health risk (i.e., potential exposure to TAC emission levels that exceed applicable standards). Dose is a function of the concentration of a substance or substances in the environment and the duration of exposure to the substance. Dose is positively correlated with time, meaning that a longer exposure period would result in a higher exposure level for any exposed receptor. Thus, the risks estimated for an exposed individual are higher if a fixed exposure occurs over a longer period of time. According to the Office of Environmental Health Hazard Assessment (OEHHA), health risk assessments, which determine the exposure of sensitive receptors to TAC emissions, should be based on a 70-, 30-, or 9-year exposure period; however, such assessments should be limited to the period/duration of activities associated with the Modified Project. Consequently, an important consideration is the fact that construction of the Modified Project is anticipated to last less than a year. Therefore, considering the relatively low mass of DPM emissions that would be generated during even the most intense season of construction, the

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relatively short duration of construction activities required to develop the site, and the highly dispersive properties of DPM, construction-related TAC emissions would not expose sensitive receptors to substantial amounts of air toxics.

### **Operational Air Contaminants**

#### *Carbon Monoxide Hot Spots*

It has long been recognized that CO exceedances are caused by vehicular emissions, primarily when idling at intersections. Concentrations of CO are a direct function of the number of vehicles, length of delay, and traffic flow conditions. Under certain meteorological conditions, CO concentrations close to congested intersections that experience high levels of traffic and elevated background concentrations may reach unhealthy levels, affecting nearby sensitive receptors. Given the high traffic volume potential, areas of high CO concentrations, or “hot spots,” are typically associated with intersections that are projected to operate at unacceptable levels of service during the peak commute hours. However, transport of this criteria pollutant is extremely limited, and CO disperses rapidly with distance from the source under normal meteorological conditions. Furthermore, vehicle emissions standards have become increasingly more stringent in the last 20 years. Currently, the CO standard in California is a maximum of 3.4 grams per mile for passenger cars (requirements for certain vehicles are more stringent). With the turnover of older vehicles, introduction of cleaner fuels, and implementation of control technology on industrial facilities, CO concentrations in the Modified Project vicinity have steadily declined.

Accordingly, with the steadily decreasing CO emissions from vehicles, even very busy intersections do not result in exceedances of the CO standard. The analysis prepared for CO attainment in the South Coast Air Quality Management District 1992 Federal Attainment Plan for Carbon Monoxide (SCAQMD 1992) in Southern California can be used to demonstrate the potential for CO exceedances. The South Coast CO hot spot analysis was conducted for four busy intersections in Los Angeles County during the peak morning and afternoon time periods. The intersections evaluated included Long Beach Boulevard and Imperial Highway (Lynwood), Wilshire Boulevard and Veteran Avenue (Westwood), Sunset Boulevard and Highland Avenue (Hollywood), and La Cienega Boulevard and Century Boulevard (Inglewood). The busiest intersection evaluated was at Wilshire Boulevard and Veteran Avenue, which has a traffic volume of approximately 100,000 vehicles per day. The Los Angeles County Metropolitan Transportation Authority evaluated the level of service in the vicinity of the Wilshire Boulevard/Veteran Avenue intersection and found it to be level of service (LOS) E at peak morning traffic and LOS F at peak afternoon traffic. Even with the inefficient LOS and volume of traffic, the CO analysis concluded that there was no violation of CO standards (SCAQMD 1992).

Per the ITE generation trip manual used in CalEEMod, the Originally Approved Project will generate a total of 881 maximum trips per day on average and the Modified Proposed Project will generate a total 891 maximum trips per day on average. Because the Modified Project would not increase traffic volumes at any intersection to more than 100,000 vehicles per day, there is no likelihood of the Modified Project traffic exceeding CO values.

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***Would the Project result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?***

During construction, the Modified Project presents the potential for generation of objectionable odors in the form of diesel exhaust in the immediate vicinity of the site. However, these emissions are short-term in nature and will rapidly dissipate and be diluted by the atmosphere downwind of the emission sources. Additionally, odors would be localized and generally confined to the construction area. Therefore, construction odors would not result in substantial amounts of odor emissions.

The land uses generally identified as sources of odors include wastewater treatment plants, wastewater pumping facilities, sanitary landfills, transfer stations, composting facilities, petroleum refineries, asphalt batch plants, chemical manufacturing and fiberglass manufacturing facilities, painting/coating operations, rendering plants, coffee roasters, food processing facilities, confined animal facilities, feedlots, dairies, green waste and recycling operations, and metal smelting plants. If a source of odors is proposed to be located near existing or planned sensitive receptors, this could have the potential to cause operational-related odor impacts. The Modified Project does not include any uses identified as being associated with odors.

## **Greenhouse Gas Emissions**

### ***Environmental Setting***

GHGs are released as byproducts of fossil fuel combustion, waste disposal, energy use, land use changes, and other human activities. This release of gases, such as carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), and chlorofluorocarbons, creates a blanket around the earth that allows light to pass through, but traps heat at the surface, preventing its escape into space. While this is a naturally occurring process known as the greenhouse effect, human activities have accelerated the generation of GHGs beyond natural levels. The overabundance of GHGs in the atmosphere has led to an unexpected warming of the earth and has the potential to severely impact the earth's climate system.

Each GHG differs in its ability to absorb heat in the atmosphere based on the lifetime, or persistence, of the gas molecule in the atmosphere. CH<sub>4</sub> traps over 25 times more heat per molecule than CO<sub>2</sub>, and N<sub>2</sub>O absorbs 298 times more heat per molecule than CO<sub>2</sub>. Often, estimates of GHG emissions are presented in carbon dioxide equivalents (CO<sub>2</sub>e). Expressing GHG emissions in carbon dioxide equivalents takes the contribution of all GHG emissions to the greenhouse effect and converts them to a single unit equivalent to the effect that would occur if only CO<sub>2</sub> were being emitted.

### ***Regulatory Setting***

The local air pollution control officer with jurisdiction regulating pollutant emissions throughout the SVAB is the SMAQMD, which is charged with the responsibility of implementing air quality programs and ensuring that national and state ambient air quality standards are not exceeded. The SMAQMD has adopted a GHG threshold of 1,100 metric tons per year for all land development and construction projects.

In 2012, the City adopted a community wide Climate Action Plan (CAP). The CAP identifies a GHG emission reduction target of 15 percent below 2005 levels by 2020 for communitywide emission sources and sets a long term communitywide GHG emission reduction goals of 38 percent below 2005 levels by

2030 and 83 percent below 2005 levels by 2050. The CAP contains a comprehensive set of strategies, measures and implementing actions to achieve the 2020 GHG reduction target. The CAP also identifies potential adverse physical effects related to climate change on the community and includes specific adaptation measures to address mitigation and such effects.

**Greenhouse Gas Emissions (VIII) Environmental Checklist and Discussion**

**Would the Project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?**

GHG emissions associated with the Modified Project would be emitted during the combustion of fossil fuels during short-term construction activities as well as vehicle trips to the to the site during on-going operations.

**Construction-Generated Greenhouse Gas Emissions**

**Table 3** illustrates the specific construction-generated GHG emissions that would result from construction of the Originally Approved Project and the Modified Project.

| <b>Table 3. Construction-Related Greenhouse Gas Emissions</b>        |  |
|--|--|
| <b>Emissions Source</b>  | <b>CO<sub>2</sub>e (Metric Tons/ Year)</b> |
| <b>Modified Project</b>  |  |
| Construction in 2019   | 391  |
| Construction in 2020   | 300  |
| <b>Total</b>   | <b>691</b>                                 |
| <i>SMAQMD Threshold of Significance</i>                              | 1,100                                      |
| <b>Exceed SMAQMD Threshold?</b>                                      | <b>No</b>                                  |
| <b>Originally Approved Project</b>                                   |  |
| Construction in 2019   | 439  |
| Construction in 2020   | 257  |
| <b>Total</b>   | <b>696</b>                                 |
| <i>SMAQMD Threshold of Significance</i>                              | 1,100                                      |
| <b>Exceed SMAQMD Threshold?</b>                                      | <b>No</b>                                  |
| <b>Difference in Originally Approved Project v. Modified Project</b> | <b>-5</b>                                  |

Source: CalEEMod version 2016.3.2. Refer to **Appendix A** for Model Data Outputs.

Notes: Emissions estimates account for the site preparation and grading of 7.52 acres.

As shown in **Table 3**, Modified Project construction would not exceed the recommended significance threshold of 1,100 metric tons per year. Additionally, the Modified Project would produce fewer metric tons of CO<sub>2</sub>e annually compared to that of the Originally Approved Project.

#### Operational-Generated Greenhouse Gas Emissions

Operation of the Modified Project would result in GHG emissions predominantly associated with motor vehicle use. Long-term operational GHG emissions attributable to the Modified Project are identified in **Table 4** and compared to SMAQMD's numeric bright-line threshold of 1,100 metric tons of CO<sub>2</sub>e annually.

| <b>Table 4. Operational-Related Greenhouse Gas Emissions</b>         |              |
|--|--------------|
| <b>Modified Project</b>  |              |
| Area Source Emissions  | 0            |
| Energy Source Emissions  | 238          |
| Mobile Source Emissions  | 642          |
| Solid Waste Emissions  | 42           |
| Water Emissions  | 25           |
| <b>Total Emissions</b>   | <b>947</b>   |
| <i>SMAQMD Screening Threshold</i>                                    | <i>1,100</i> |
| <b>Exceed SCAQMD Threshold?</b>                                      | <b>No</b>    |
| <b>Originally Approved Project</b>                                   |              |
| Area Source Emissions  | 0            |
| Energy Source Emissions  | 212          |
| Mobile Source Emissions  | 570          |
| Solid Waste Emissions  | 37           |
| Water Emissions  | 22           |
| <b>Total Emissions</b>   | <b>842</b>   |
| <i>SMAQMD Screening Threshold</i>                                    | <i>1,100</i> |
| <b>Difference in Originally Approved Project v. Modified Project</b> | <b>+105</b>  |

Source: CalEEMod version 2016.3.2. Refer to **Appendix A** for Model Data Outputs.

As shown in **Table 4**, operation of the Modified Project would result in annual emissions of 947 metric tons of CO<sub>2</sub>e per year, which is below the recommended threshold of 1,100 metric tons of CO<sub>2</sub>e per year. Compared with the Originally Approved Project, the Modified Project would result in 105 additional metric tons of CO<sub>2</sub>e.

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***Would the Project conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?***

The City of Sacramento CAP is a strategic planning document that identifies sources of GHG emissions within the City's boundaries, presents current and future emission estimates, identifies a GHG reduction target for future years, and presents strategies, policies and actions to reduce emissions from the energy, transportation, land use, water use, and waste sectors. The GHG reduction strategies in the CAP build on inventory results and key opportunities prioritized by the City staff and members of the public. The CAP consists of strategies that identify steps the City will take to support reductions in GHG emissions. The City will achieve these reductions in GHG emissions through a mix of voluntary programs and new strategic standards. All standards presented in the CAP respond to the needs of development through achieving more efficient and sustainable resources.

Both the existing and the projected GHG inventories in the CAP were derived based on the land use designations and associated designations defined in the City of Sacramento General Plan. The Modified Project proposal to develop a mini storage and RV parking is consistent with the General Plan as it conforms with the types, intensity, and patterns of land use envisioned for the site in the General Plan. The Modified Project would not conflict with the land use assumptions or exceed the population or job growth projections used by the City to develop the CAP.

Long-term operational emissions would occur over the life of the Modified Project. However, all development in Sacramento, including the Modified Project, is required to adhere to all City-adopted policy provisions, including those contained in the adopted CAP. The City ensures all provisions of the City CAP are incorporated into projects and their permits through development review and applications of conditions of approval as applicable. Furthermore, the Modified Project would be required to comply with the City's 2035 General Plan Land Use and Urban Form Designations and Development Standards and would also have to conform with the allowable density standards specified in the General Plan. In addition, the Modified Project is consistent with General Plan goals supporting infill development (LU 1.1, 1.1.4, 1.1.5 and 1.1.10), sustainable development patterns (LU 2.6.1), and preservation of neighborhoods (LU 4.1.1, 4.1.3, 4.1.10) that all support the City's CAP.

The Modified Project would not conflict with an adopted plan, policy, or regulation pertaining to GHGs.



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## References

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**Attachment A – CalEEMod Output File for Air Quality Emissions**

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Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**Mack Road Storage Facility- Modified Project**  
**Sacramento County, Summer**

**1.0 Project Characteristics**

**1.1 Land Usage**

| Land Uses               | Size   | Metric   | Lot Acreage | Floor Surface Area | Population |
|-------------------------|--------|----------|-------------|--------------------|------------|
| General Office Building | 90.00  | 1000sqft | 5.00        | 90,000.00          | 0          |
| Parking Lot             | 100.00 | Space    | 2.52        | 40,000.00          | 0          |

**1.2 Other Project Characteristics**

|                                |                                |                                |       |                                  |       |
|--------------------------------|--------------------------------|--------------------------------|-------|----------------------------------|-------|
| <b>Urbanization</b>            | Urban                          | <b>Wind Speed (m/s)</b>        | 3.5   | <b>Precipitation Freq (Days)</b> | 58    |
| <b>Climate Zone</b>            | 6                              |                                |       | <b>Operational Year</b>          | 2021  |
| <b>Utility Company</b>         | Pacific Gas & Electric Company |                                |       |                                  |       |
| <b>CO2 Intensity (lb/MWhr)</b> | 290                            | <b>CH4 Intensity (lb/MWhr)</b> | 0.029 | <b>N2O Intensity (lb/MWhr)</b>   | 0.006 |

**1.3 User Entered Comments & Non-Default Data**

Project Characteristics - PG&E 2020 CO2 Intensity Factor

Land Use - Lot acreage updated to match that of the project.

Construction Phase - Building, paving and coating assumed to occur at the same time.

## Mack Road Storage Facility- Modified Project - Sacramento County, Summer

| Table Name                | Column Name        | Default Value | New Value |
|---------------------------|--------------------|---------------|-----------|
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblConstructionPhase      | PhaseEndDate       | 5/5/2020      | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 3/10/2020     | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 4/23/2019     | 7/12/2019 |
| tblConstructionPhase      | PhaseEndDate       | 4/7/2020      | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 3/26/2019     | 6/14/2019 |
| tblConstructionPhase      | PhaseStartDate     | 4/8/2020      | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 4/24/2019     | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/27/2019     | 6/15/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/11/2020     | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/13/2019     | 6/1/2019  |
| tblLandUse                | LotAcreage         | 2.07          | 5.00      |
| tblLandUse                | LotAcreage         | 0.90          | 2.52      |
| tblProjectCharacteristics | CO2IntensityFactor | 641.35        | 290       |

## 2.0 Emissions Summary

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Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**2.2 Overall Operational**

**Unmitigated Operational**

|              | ROG           | NOx           | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O                | CO2e              |
|--------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|--------------------|-------------------|
| Category     | lb/day        |               |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |                    |                   |
| Area         | 2.1736        | 1.8000e-004   | 0.0195         | 0.0000        |               | 7.0000e-005   | 7.0000e-005   |                | 7.0000e-005   | 7.0000e-005   |          | 0.0416            | 0.0416            | 1.1000e-004   |                    | 0.0443            |
| Energy       | 0.0348        | 0.3167        | 0.2660         | 1.9000e-003   |               | 0.0241        | 0.0241        |                | 0.0241        | 0.0241        |          | 380.0161          | 380.0161          | 7.2800e-003   | 6.9700e-003        | 382.2744          |
| Mobile       | 2.0394        | 6.3256        | 19.2428        | 0.0547        | 4.3494        | 0.0459        | 4.3953        | 1.1629         | 0.0430        | 1.2059        |          | 5,529.1026        | 5,529.1026        | 0.2662        |                    | 5,535.7570        |
| <b>Total</b> | <b>4.2479</b> | <b>6.6425</b> | <b>19.5283</b> | <b>0.0566</b> | <b>4.3494</b> | <b>0.0700</b> | <b>4.4195</b> | <b>1.1629</b>  | <b>0.0671</b> | <b>1.2300</b> |          | <b>5,909.1603</b> | <b>5,909.1603</b> | <b>0.2736</b> | <b>6.9700e-003</b> | <b>5,918.0757</b> |

**Mitigated Operational**

|              | ROG           | NOx           | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O                | CO2e              |
|--------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|--------------------|-------------------|
| Category     | lb/day        |               |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |                    |                   |
| Area         | 2.1736        | 1.8000e-004   | 0.0195         | 0.0000        |               | 7.0000e-005   | 7.0000e-005   |                | 7.0000e-005   | 7.0000e-005   |          | 0.0416            | 0.0416            | 1.1000e-004   |                    | 0.0443            |
| Energy       | 0.0348        | 0.3167        | 0.2660         | 1.9000e-003   |               | 0.0241        | 0.0241        |                | 0.0241        | 0.0241        |          | 380.0161          | 380.0161          | 7.2800e-003   | 6.9700e-003        | 382.2744          |
| Mobile       | 2.0394        | 6.3256        | 19.2428        | 0.0547        | 4.3494        | 0.0459        | 4.3953        | 1.1629         | 0.0430        | 1.2059        |          | 5,529.1026        | 5,529.1026        | 0.2662        |                    | 5,535.7570        |
| <b>Total</b> | <b>4.2479</b> | <b>6.6425</b> | <b>19.5283</b> | <b>0.0566</b> | <b>4.3494</b> | <b>0.0700</b> | <b>4.4195</b> | <b>1.1629</b>  | <b>0.0671</b> | <b>1.2300</b> |          | <b>5,909.1603</b> | <b>5,909.1603</b> | <b>0.2736</b> | <b>6.9700e-003</b> | <b>5,918.0757</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

|                   | ROG  | NOx  | CO   | SO2  | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio-CO2 | Total CO2 | CH4  | N2O  | CO2e |
|-------------------|------|------|------|------|---------------|--------------|------------|----------------|---------------|-------------|----------|----------|-----------|------|------|------|
| Percent Reduction | 0.00 | 0.00 | 0.00 | 0.00 | 0.00          | 0.00         | 0.00       | 0.00           | 0.00          | 0.00        | 0.00     | 0.00     | 0.00      | 0.00 | 0.00 | 0.00 |

### 3.0 Construction Detail

#### Construction Phase

| Phase Number | Phase Name            | Phase Type            | Start Date | End Date  | Num Days Week | Num Days | Phase Description |
|--------------|-----------------------|-----------------------|------------|-----------|---------------|----------|-------------------|
| 1            | Site Preparation      | Site Preparation      | 6/1/2019   | 6/14/2019 | 5             | 10       |                   |
| 2            | Grading               | Grading               | 6/15/2019  | 7/12/2019 | 5             | 20       |                   |
| 3            | Building Construction | Building Construction | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |
| 4            | Paving                | Paving                | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |
| 5            | Architectural Coating | Architectural Coating | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 10

Acres of Paving: 2.52

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 135,000; Non-Residential Outdoor: 45,000; Striped Parking Area: 2,400 (Architectural Coating – sqft)

#### OffRoad Equipment

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

| Phase Name            | Offroad Equipment Type    | Amount | Usage Hours | Horse Power | Load Factor |
|-----------------------|---------------------------|--------|-------------|-------------|-------------|
| Site Preparation      | Rubber Tired Dozers       | 3      | 8.00        | 247         | 0.40        |
| Site Preparation      | Tractors/Loaders/Backhoes | 4      | 8.00        | 97          | 0.37        |
| Grading               | Excavators                | 1      | 8.00        | 158         | 0.38        |
| Grading               | Graders                   | 1      | 8.00        | 187         | 0.41        |
| Grading               | Rubber Tired Dozers       | 1      | 8.00        | 247         | 0.40        |
| Grading               | Tractors/Loaders/Backhoes | 3      | 8.00        | 97          | 0.37        |
| Building Construction | Cranes                    | 1      | 7.00        | 231         | 0.29        |
| Building Construction | Forklifts                 | 3      | 8.00        | 89          | 0.20        |
| Building Construction | Generator Sets            | 1      | 8.00        | 84          | 0.74        |
| Building Construction | Tractors/Loaders/Backhoes | 3      | 7.00        | 97          | 0.37        |
| Building Construction | Welders                   | 1      | 8.00        | 46          | 0.45        |
| Paving                | Pavers                    | 2      | 8.00        | 130         | 0.42        |
| Paving                | Paving Equipment          | 2      | 8.00        | 132         | 0.36        |
| Paving                | Rollers                   | 2      | 8.00        | 80          | 0.38        |
| Architectural Coating | Air Compressors           | 1      | 6.00        | 78          | 0.48        |

**Trips and VMT**

| Phase Name            | Offroad Equipment Count | Worker Trip Number | Vendor Trip Number | Hauling Trip Number | Worker Trip Length | Vendor Trip Length | Hauling Trip Length | Worker Vehicle Class | Vendor Vehicle Class | Hauling Vehicle Class |
|-----------------------|-------------------------|--------------------|--------------------|---------------------|--------------------|--------------------|---------------------|----------------------|----------------------|-----------------------|
| Site Preparation      | 7                       | 18.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Grading               | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Building Construction | 9                       | 46.00              | 21.00              | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Paving                | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Architectural Coating | 1                       | 9.00               | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |

**3.1 Mitigation Measures Construction**



Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.2 Site Preparation - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10  | Exhaust PM10  | PM10 Total     | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total    | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|----------------|---------------|----------------|----------------|---------------|----------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |                |               |                |                |               |                | lb/day   |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 18.0663        | 0.0000        | 18.0663        | 9.9307         | 0.0000        | 9.9307         |          |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 4.3350        | 45.5727        | 22.0630        | 0.0380        |                | 2.3904        | 2.3904         |                | 2.1991        | 2.1991         |          | 3,766.4529        | 3,766.4529        | 1.1917        |     | 3,796.2445        |
| <b>Total</b>  | <b>4.3350</b> | <b>45.5727</b> | <b>22.0630</b> | <b>0.0380</b> | <b>18.0663</b> | <b>2.3904</b> | <b>20.4566</b> | <b>9.9307</b>  | <b>2.1991</b> | <b>12.1298</b> |          | <b>3,766.4529</b> | <b>3,766.4529</b> | <b>1.1917</b> |     | <b>3,796.2445</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0843        | 0.0463        | 0.6499        | 1.4800e-003        | 0.1369        | 9.8000e-004        | 0.1379        | 0.0363         | 9.0000e-004        | 0.0372        |          | 147.3555        | 147.3555        | 4.6400e-003        |     | 147.4714        |
| <b>Total</b> | <b>0.0843</b> | <b>0.0463</b> | <b>0.6499</b> | <b>1.4800e-003</b> | <b>0.1369</b> | <b>9.8000e-004</b> | <b>0.1379</b> | <b>0.0363</b>  | <b>9.0000e-004</b> | <b>0.0372</b> |          | <b>147.3555</b> | <b>147.3555</b> | <b>4.6400e-003</b> |     | <b>147.4714</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.2 Site Preparation - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10  | Exhaust PM10  | PM10 Total     | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total    | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|----------------|---------------|----------------|----------------|---------------|----------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |                |               |                |                |               |                | lb/day        |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 18.0663        | 0.0000        | 18.0663        | 9.9307         | 0.0000        | 9.9307         |               |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 4.3350        | 45.5727        | 22.0630        | 0.0380        |                | 2.3904        | 2.3904         |                | 2.1991        | 2.1991         | 0.0000        | 3,766.4529        | 3,766.4529        | 1.1917        |     | 3,796.2445        |
| <b>Total</b>  | <b>4.3350</b> | <b>45.5727</b> | <b>22.0630</b> | <b>0.0380</b> | <b>18.0663</b> | <b>2.3904</b> | <b>20.4566</b> | <b>9.9307</b>  | <b>2.1991</b> | <b>12.1298</b> | <b>0.0000</b> | <b>3,766.4529</b> | <b>3,766.4529</b> | <b>1.1917</b> |     | <b>3,796.2445</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0843        | 0.0463        | 0.6499        | 1.4800e-003        | 0.1369        | 9.8000e-004        | 0.1379        | 0.0363         | 9.0000e-004        | 0.0372        |          | 147.3555        | 147.3555        | 4.6400e-003        |     | 147.4714        |
| <b>Total</b> | <b>0.0843</b> | <b>0.0463</b> | <b>0.6499</b> | <b>1.4800e-003</b> | <b>0.1369</b> | <b>9.8000e-004</b> | <b>0.1379</b> | <b>0.0363</b>  | <b>9.0000e-004</b> | <b>0.0372</b> |          | <b>147.3555</b> | <b>147.3555</b> | <b>4.6400e-003</b> |     | <b>147.4714</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.3 Grading - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 6.5523        | 0.0000        | 6.5523        | 3.3675         | 0.0000        | 3.3675        |          |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 2.5805        | 28.3480        | 16.2934        | 0.0297        |               | 1.3974        | 1.3974        |                | 1.2856        | 1.2856        |          | 2,936.8068        | 2,936.8068        | 0.9292        |     | 2,960.0361        |
| <b>Total</b>  | <b>2.5805</b> | <b>28.3480</b> | <b>16.2934</b> | <b>0.0297</b> | <b>6.5523</b> | <b>1.3974</b> | <b>7.9497</b> | <b>3.3675</b>  | <b>1.2856</b> | <b>4.6531</b> |          | <b>2,936.8068</b> | <b>2,936.8068</b> | <b>0.9292</b> |     | <b>2,960.0361</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.3 Grading - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 6.5523        | 0.0000        | 6.5523        | 3.3675         | 0.0000        | 3.3675        |               |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 2.5805        | 28.3480        | 16.2934        | 0.0297        |               | 1.3974        | 1.3974        |                | 1.2856        | 1.2856        | 0.0000        | 2,936.8068        | 2,936.8068        | 0.9292        |     | 2,960.0361        |
| <b>Total</b>  | <b>2.5805</b> | <b>28.3480</b> | <b>16.2934</b> | <b>0.0297</b> | <b>6.5523</b> | <b>1.3974</b> | <b>7.9497</b> | <b>3.3675</b>  | <b>1.2856</b> | <b>4.6531</b> | <b>0.0000</b> | <b>2,936.8068</b> | <b>2,936.8068</b> | <b>0.9292</b> |     | <b>2,960.0361</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.4 Building Construction - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                        |                        |               |     |                        |
| Off-Road     | 2.3612        | 21.0788        | 17.1638        | 0.0269        |               | 1.2899        | 1.2899        |                | 1.2127        | 1.2127        |          | 2,591.580<br>2         | 2,591.580<br>2         | 0.6313        |     | 2,607.363<br>5         |
| <b>Total</b> | <b>2.3612</b> | <b>21.0788</b> | <b>17.1638</b> | <b>0.0269</b> |               | <b>1.2899</b> | <b>1.2899</b> |                | <b>1.2127</b> | <b>1.2127</b> |          | <b>2,591.580<br/>2</b> | <b>2,591.580<br/>2</b> | <b>0.6313</b> |     | <b>2,607.363<br/>5</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0994        | 2.5154        | 0.7510        | 5.2700e-003        | 0.1264        | 0.0182        | 0.1445        | 0.0364         | 0.0174        | 0.0537        |          | 557.0270        | 557.0270        | 0.0335        |     | 557.8638        |
| Worker       | 0.2156        | 0.1184        | 1.6609        | 3.7900e-003        | 0.3499        | 2.4900e-003   | 0.3524        | 0.0928         | 2.3000e-003   | 0.0951        |          | 376.5752        | 376.5752        | 0.0119        |     | 376.8714        |
| <b>Total</b> | <b>0.3149</b> | <b>2.6338</b> | <b>2.4119</b> | <b>9.0600e-003</b> | <b>0.4763</b> | <b>0.0206</b> | <b>0.4970</b> | <b>0.1292</b>  | <b>0.0197</b> | <b>0.1489</b> |          | <b>933.6021</b> | <b>933.6021</b> | <b>0.0453</b> |     | <b>934.7353</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.4 Building Construction - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                        |                        |               |     |                        |
| Off-Road     | 2.3612        | 21.0788        | 17.1638        | 0.0269        |               | 1.2899        | 1.2899        |                | 1.2127        | 1.2127        | 0.0000        | 2,591.580<br>2         | 2,591.580<br>2         | 0.6313        |     | 2,607.363<br>5         |
| <b>Total</b> | <b>2.3612</b> | <b>21.0788</b> | <b>17.1638</b> | <b>0.0269</b> |               | <b>1.2899</b> | <b>1.2899</b> |                | <b>1.2127</b> | <b>1.2127</b> | <b>0.0000</b> | <b>2,591.580<br/>2</b> | <b>2,591.580<br/>2</b> | <b>0.6313</b> |     | <b>2,607.363<br/>5</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0994        | 2.5154        | 0.7510        | 5.2700e-003        | 0.1264        | 0.0182        | 0.1445        | 0.0364         | 0.0174        | 0.0537        |          | 557.0270        | 557.0270        | 0.0335        |     | 557.8638        |
| Worker       | 0.2156        | 0.1184        | 1.6609        | 3.7900e-003        | 0.3499        | 2.4900e-003   | 0.3524        | 0.0928         | 2.3000e-003   | 0.0951        |          | 376.5752        | 376.5752        | 0.0119        |     | 376.8714        |
| <b>Total</b> | <b>0.3149</b> | <b>2.6338</b> | <b>2.4119</b> | <b>9.0600e-003</b> | <b>0.4763</b> | <b>0.0206</b> | <b>0.4970</b> | <b>0.1292</b>  | <b>0.0197</b> | <b>0.1489</b> |          | <b>933.6021</b> | <b>933.6021</b> | <b>0.0453</b> |     | <b>934.7353</b> |

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**3.4 Building Construction - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 2.1198        | 19.1860        | 16.8485        | 0.0269        |               | 1.1171        | 1.1171        |                | 1.0503        | 1.0503        |          | 2,553.0631        | 2,553.0631        | 0.6229        |     | 2,568.6345        |
| <b>Total</b> | <b>2.1198</b> | <b>19.1860</b> | <b>16.8485</b> | <b>0.0269</b> |               | <b>1.1171</b> | <b>1.1171</b> |                | <b>1.0503</b> | <b>1.0503</b> |          | <b>2,553.0631</b> | <b>2,553.0631</b> | <b>0.6229</b> |     | <b>2,568.6345</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0792        | 2.3059        | 0.6198        | 5.2300e-003        | 0.1264        | 0.0120        | 0.1384        | 0.0364         | 0.0115        | 0.0479        |          | 553.6764        | 553.6764        | 0.0314        |     | 554.4607        |
| Worker       | 0.1984        | 0.1053        | 1.5019        | 3.6700e-003        | 0.3499        | 2.4300e-003   | 0.3524        | 0.0928         | 2.2400e-003   | 0.0951        |          | 365.0160        | 365.0160        | 0.0105        |     | 365.2774        |
| <b>Total</b> | <b>0.2776</b> | <b>2.4112</b> | <b>2.1217</b> | <b>8.9000e-003</b> | <b>0.4763</b> | <b>0.0145</b> | <b>0.4908</b> | <b>0.1292</b>  | <b>0.0137</b> | <b>0.1429</b> |          | <b>918.6924</b> | <b>918.6924</b> | <b>0.0418</b> |     | <b>919.7381</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.4 Building Construction - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                        |                        |               |     |                        |
| Off-Road     | 2.1198        | 19.1860        | 16.8485        | 0.0269        |               | 1.1171        | 1.1171        |                | 1.0503        | 1.0503        | 0.0000        | 2,553.063<br>1         | 2,553.063<br>1         | 0.6229        |     | 2,568.634<br>5         |
| <b>Total</b> | <b>2.1198</b> | <b>19.1860</b> | <b>16.8485</b> | <b>0.0269</b> |               | <b>1.1171</b> | <b>1.1171</b> |                | <b>1.0503</b> | <b>1.0503</b> | <b>0.0000</b> | <b>2,553.063<br/>1</b> | <b>2,553.063<br/>1</b> | <b>0.6229</b> |     | <b>2,568.634<br/>5</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0792        | 2.3059        | 0.6198        | 5.2300e-003        | 0.1264        | 0.0120        | 0.1384        | 0.0364         | 0.0115        | 0.0479        |          | 553.6764        | 553.6764        | 0.0314        |     | 554.4607        |
| Worker       | 0.1984        | 0.1053        | 1.5019        | 3.6700e-003        | 0.3499        | 2.4300e-003   | 0.3524        | 0.0928         | 2.2400e-003   | 0.0951        |          | 365.0160        | 365.0160        | 0.0105        |     | 365.2774        |
| <b>Total</b> | <b>0.2776</b> | <b>2.4112</b> | <b>2.1217</b> | <b>8.9000e-003</b> | <b>0.4763</b> | <b>0.0145</b> | <b>0.4908</b> | <b>0.1292</b>  | <b>0.0137</b> | <b>0.1429</b> |          | <b>918.6924</b> | <b>918.6924</b> | <b>0.0418</b> |     | <b>919.7381</b> |



Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.5 Paving - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 1.4544        | 15.2441        | 14.6648        | 0.0228        |               | 0.8246        | 0.8246        |                | 0.7586        | 0.7586        |          | 2,257.0025        | 2,257.0025        | 0.7141        |     | 2,274.8548        |
| Paving       | 0.0287        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.4831</b> | <b>15.2441</b> | <b>14.6648</b> | <b>0.0228</b> |               | <b>0.8246</b> | <b>0.8246</b> |                | <b>0.7586</b> | <b>0.7586</b> |          | <b>2,257.0025</b> | <b>2,257.0025</b> | <b>0.7141</b> |     | <b>2,274.8548</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.5 Paving - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Off-Road     | 1.4544        | 15.2441        | 14.6648        | 0.0228        |               | 0.8246        | 0.8246        |                | 0.7586        | 0.7586        | 0.0000        | 2,257.0025        | 2,257.0025        | 0.7141        |     | 2,274.8548        |
| Paving       | 0.0287        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.4831</b> | <b>15.2441</b> | <b>14.6648</b> | <b>0.0228</b> |               | <b>0.8246</b> | <b>0.8246</b> |                | <b>0.7586</b> | <b>0.7586</b> | <b>0.0000</b> | <b>2,257.0025</b> | <b>2,257.0025</b> | <b>0.7141</b> |     | <b>2,274.8548</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.5 Paving - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 1.3566        | 14.0656        | 14.6521        | 0.0228        |               | 0.7528        | 0.7528        |                | 0.6926        | 0.6926        |          | 2,207.7334        | 2,207.7334        | 0.7140        |     | 2,225.5841        |
| Paving       | 0.0287        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.3853</b> | <b>14.0656</b> | <b>14.6521</b> | <b>0.0228</b> |               | <b>0.7528</b> | <b>0.7528</b> |                | <b>0.6926</b> | <b>0.6926</b> |          | <b>2,207.7334</b> | <b>2,207.7334</b> | <b>0.7140</b> |     | <b>2,225.5841</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0647        | 0.0343        | 0.4898        | 1.2000e-003        | 0.1141        | 7.9000e-004        | 0.1149        | 0.0303         | 7.3000e-004        | 0.0310        |          | 119.0269        | 119.0269        | 3.4100e-003        |     | 119.1122        |
| <b>Total</b> | <b>0.0647</b> | <b>0.0343</b> | <b>0.4898</b> | <b>1.2000e-003</b> | <b>0.1141</b> | <b>7.9000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.3000e-004</b> | <b>0.0310</b> |          | <b>119.0269</b> | <b>119.0269</b> | <b>3.4100e-003</b> |     | <b>119.1122</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.5 Paving - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Off-Road     | 1.3566        | 14.0656        | 14.6521        | 0.0228        |               | 0.7528        | 0.7528        |                | 0.6926        | 0.6926        | 0.0000        | 2,207.7334        | 2,207.7334        | 0.7140        |     | 2,225.5841        |
| Paving       | 0.0287        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.3853</b> | <b>14.0656</b> | <b>14.6521</b> | <b>0.0228</b> |               | <b>0.7528</b> | <b>0.7528</b> |                | <b>0.6926</b> | <b>0.6926</b> | <b>0.0000</b> | <b>2,207.7334</b> | <b>2,207.7334</b> | <b>0.7140</b> |     | <b>2,225.5841</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0647        | 0.0343        | 0.4898        | 1.2000e-003        | 0.1141        | 7.9000e-004        | 0.1149        | 0.0303         | 7.3000e-004        | 0.0310        |          | 119.0269        | 119.0269        | 3.4100e-003        |     | 119.1122        |
| <b>Total</b> | <b>0.0647</b> | <b>0.0343</b> | <b>0.4898</b> | <b>1.2000e-003</b> | <b>0.1141</b> | <b>7.9000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.3000e-004</b> | <b>0.0310</b> |          | <b>119.0269</b> | <b>119.0269</b> | <b>3.4100e-003</b> |     | <b>119.1122</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.6 Architectural Coating - 2019**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Archit. Coating | 3.6758        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2664        | 1.8354        | 1.8413        | 2.9700e-003        |               | 0.1288        | 0.1288        |                | 0.1288        | 0.1288        |          | 281.4481        | 281.4481        | 0.0238        |     | 282.0423        |
| <b>Total</b>    | <b>3.9422</b> | <b>1.8354</b> | <b>1.8413</b> | <b>2.9700e-003</b> |               | <b>0.1288</b> | <b>0.1288</b> |                | <b>0.1288</b> | <b>0.1288</b> |          | <b>281.4481</b> | <b>281.4481</b> | <b>0.0238</b> |     | <b>282.0423</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0422        | 0.0232        | 0.3250        | 7.4000e-004        | 0.0685        | 4.9000e-004        | 0.0690        | 0.0182         | 4.5000e-004        | 0.0186        |          | 73.6778        | 73.6778        | 2.3200e-003        |     | 73.7357        |
| <b>Total</b> | <b>0.0422</b> | <b>0.0232</b> | <b>0.3250</b> | <b>7.4000e-004</b> | <b>0.0685</b> | <b>4.9000e-004</b> | <b>0.0690</b> | <b>0.0182</b>  | <b>4.5000e-004</b> | <b>0.0186</b> |          | <b>73.6778</b> | <b>73.6778</b> | <b>2.3200e-003</b> |     | <b>73.7357</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.6 Architectural Coating - 2019**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day        |                 |                 |               |     |                 |
| Archit. Coating | 3.6758        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2664        | 1.8354        | 1.8413        | 2.9700e-003        |               | 0.1288        | 0.1288        |                | 0.1288        | 0.1288        | 0.0000        | 281.4481        | 281.4481        | 0.0238        |     | 282.0423        |
| <b>Total</b>    | <b>3.9422</b> | <b>1.8354</b> | <b>1.8413</b> | <b>2.9700e-003</b> |               | <b>0.1288</b> | <b>0.1288</b> |                | <b>0.1288</b> | <b>0.1288</b> | <b>0.0000</b> | <b>281.4481</b> | <b>281.4481</b> | <b>0.0238</b> |     | <b>282.0423</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0422        | 0.0232        | 0.3250        | 7.4000e-004        | 0.0685        | 4.9000e-004        | 0.0690        | 0.0182         | 4.5000e-004        | 0.0186        |          | 73.6778        | 73.6778        | 2.3200e-003        |     | 73.7357        |
| <b>Total</b> | <b>0.0422</b> | <b>0.0232</b> | <b>0.3250</b> | <b>7.4000e-004</b> | <b>0.0685</b> | <b>4.9000e-004</b> | <b>0.0690</b> | <b>0.0182</b>  | <b>4.5000e-004</b> | <b>0.0186</b> |          | <b>73.6778</b> | <b>73.6778</b> | <b>2.3200e-003</b> |     | <b>73.7357</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.6 Architectural Coating - 2020**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Archit. Coating | 3.6758        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2422        | 1.6838        | 1.8314        | 2.9700e-003        |               | 0.1109        | 0.1109        |                | 0.1109        | 0.1109        |          | 281.4481        | 281.4481        | 0.0218        |     | 281.9928        |
| <b>Total</b>    | <b>3.9179</b> | <b>1.6838</b> | <b>1.8314</b> | <b>2.9700e-003</b> |               | <b>0.1109</b> | <b>0.1109</b> |                | <b>0.1109</b> | <b>0.1109</b> |          | <b>281.4481</b> | <b>281.4481</b> | <b>0.0218</b> |     | <b>281.9928</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0388        | 0.0206        | 0.2939        | 7.2000e-004        | 0.0685        | 4.8000e-004        | 0.0689        | 0.0182         | 4.4000e-004        | 0.0186        |          | 71.4162        | 71.4162        | 2.0500e-003        |     | 71.4673        |
| <b>Total</b> | <b>0.0388</b> | <b>0.0206</b> | <b>0.2939</b> | <b>7.2000e-004</b> | <b>0.0685</b> | <b>4.8000e-004</b> | <b>0.0689</b> | <b>0.0182</b>  | <b>4.4000e-004</b> | <b>0.0186</b> |          | <b>71.4162</b> | <b>71.4162</b> | <b>2.0500e-003</b> |     | <b>71.4673</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**3.6 Architectural Coating - 2020**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day        |                 |                 |               |     |                 |
| Archit. Coating | 3.6758        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2422        | 1.6838        | 1.8314        | 2.9700e-003        |               | 0.1109        | 0.1109        |                | 0.1109        | 0.1109        | 0.0000        | 281.4481        | 281.4481        | 0.0218        |     | 281.9928        |
| <b>Total</b>    | <b>3.9179</b> | <b>1.6838</b> | <b>1.8314</b> | <b>2.9700e-003</b> |               | <b>0.1109</b> | <b>0.1109</b> |                | <b>0.1109</b> | <b>0.1109</b> | <b>0.0000</b> | <b>281.4481</b> | <b>281.4481</b> | <b>0.0218</b> |     | <b>281.9928</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0388        | 0.0206        | 0.2939        | 7.2000e-004        | 0.0685        | 4.8000e-004        | 0.0689        | 0.0182         | 4.4000e-004        | 0.0186        |          | 71.4162        | 71.4162        | 2.0500e-003        |     | 71.4673        |
| <b>Total</b> | <b>0.0388</b> | <b>0.0206</b> | <b>0.2939</b> | <b>7.2000e-004</b> | <b>0.0685</b> | <b>4.8000e-004</b> | <b>0.0689</b> | <b>0.0182</b>  | <b>4.4000e-004</b> | <b>0.0186</b> |          | <b>71.4162</b> | <b>71.4162</b> | <b>2.0500e-003</b> |     | <b>71.4673</b> |

**4.0 Operational Detail - Mobile**

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Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**4.1 Mitigation Measures Mobile**

|             | ROG    | NOx    | CO      | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2  | Total CO2  | CH4    | N2O | CO2e       |
|-------------|--------|--------|---------|--------|---------------|--------------|------------|----------------|---------------|-------------|----------|------------|------------|--------|-----|------------|
| Category    | lb/day |        |         |        |               |              |            |                |               |             | lb/day   |            |            |        |     |            |
| Mitigated   | 2.0394 | 6.3256 | 19.2428 | 0.0547 | 4.3494        | 0.0459       | 4.3953     | 1.1629         | 0.0430        | 1.2059      |          | 5,529.1026 | 5,529.1026 | 0.2662 |     | 5,535.7570 |
| Unmitigated | 2.0394 | 6.3256 | 19.2428 | 0.0547 | 4.3494        | 0.0459       | 4.3953     | 1.1629         | 0.0430        | 1.2059      |          | 5,529.1026 | 5,529.1026 | 0.2662 |     | 5,535.7570 |

**4.2 Trip Summary Information**

| Land Use                | Average Daily Trip Rate |          |        | Unmitigated | Mitigated  |
|-------------------------|-------------------------|----------|--------|-------------|------------|
|                         | Weekday                 | Saturday | Sunday | Annual VMT  | Annual VMT |
| General Office Building | 992.70                  | 221.40   | 94.50  | 1,557,501   | 1,557,501  |
| Parking Lot             | 0.00                    | 0.00     | 0.00   |             |            |
| Total                   | 992.70                  | 221.40   | 94.50  | 1,557,501   | 1,557,501  |

**4.3 Trip Type Information**

| Land Use                | Miles      |            |             | Trip %     |            |             | Trip Purpose % |          |         |
|-------------------------|------------|------------|-------------|------------|------------|-------------|----------------|----------|---------|
|                         | H-W or C-W | H-S or C-C | H-O or C-NW | H-W or C-W | H-S or C-C | H-O or C-NW | Primary        | Diverted | Pass-by |
| General Office Building | 10.00      | 5.00       | 6.50        | 33.00      | 48.00      | 19.00       | 77             | 19       | 4       |
| Parking Lot             | 10.00      | 5.00       | 6.50        | 0.00       | 0.00       | 0.00        | 0              | 0        | 0       |

**4.4 Fleet Mix**

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

| Land Use                | LDA      | LDT1     | LDT2     | MDV      | LHD1     | LHD2     | MHD      | HHD      | OBUS     | UBUS     | MCY      | SBUS     | MH       |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| General Office Building | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |
| Parking Lot             | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |

**5.0 Energy Detail**

Historical Energy Use: N

**5.1 Mitigation Measures Energy**

|                        | ROG    | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O         | CO2e     |
|------------------------|--------|--------|--------|-------------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-------------|----------|
| Category               | lb/day |        |        |             |               |              |            |                |               |             | lb/day   |           |           |             |             |          |
| NaturalGas Mitigated   | 0.0348 | 0.3167 | 0.2660 | 1.9000e-003 |               | 0.0241       | 0.0241     |                | 0.0241        | 0.0241      |          | 380.0161  | 380.0161  | 7.2800e-003 | 6.9700e-003 | 382.2744 |
| NaturalGas Unmitigated | 0.0348 | 0.3167 | 0.2660 | 1.9000e-003 |               | 0.0241       | 0.0241     |                | 0.0241        | 0.0241      |          | 380.0161  | 380.0161  | 7.2800e-003 | 6.9700e-003 | 382.2744 |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

|                         | NaturalGas Use | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O                | CO2e            |
|-------------------------|----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|--------------------|--------------------|-----------------|
| Land Use                | kBTU/yr        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |                    |                    |                 |
| General Office Building | 3230.14        | 0.0348        | 0.3167        | 0.2660        | 1.9000e-003        |               | 0.0241        | 0.0241        |                | 0.0241        | 0.0241        |          | 380.0161        | 380.0161        | 7.2800e-003        | 6.9700e-003        | 382.2744        |
| Parking Lot             | 0              | 0.0000        | 0.0000        | 0.0000        | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             | 0.0000             | 0.0000          |
| <b>Total</b>            |                | <b>0.0348</b> | <b>0.3167</b> | <b>0.2660</b> | <b>1.9000e-003</b> |               | <b>0.0241</b> | <b>0.0241</b> |                | <b>0.0241</b> | <b>0.0241</b> |          | <b>380.0161</b> | <b>380.0161</b> | <b>7.2800e-003</b> | <b>6.9700e-003</b> | <b>382.2744</b> |

**Mitigated**

|                         | NaturalGas Use | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O                | CO2e            |
|-------------------------|----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|--------------------|--------------------|-----------------|
| Land Use                | kBTU/yr        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |                    |                    |                 |
| General Office Building | 3.23014        | 0.0348        | 0.3167        | 0.2660        | 1.9000e-003        |               | 0.0241        | 0.0241        |                | 0.0241        | 0.0241        |          | 380.0161        | 380.0161        | 7.2800e-003        | 6.9700e-003        | 382.2744        |
| Parking Lot             | 0              | 0.0000        | 0.0000        | 0.0000        | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             | 0.0000             | 0.0000          |
| <b>Total</b>            |                | <b>0.0348</b> | <b>0.3167</b> | <b>0.2660</b> | <b>1.9000e-003</b> |               | <b>0.0241</b> | <b>0.0241</b> |                | <b>0.0241</b> | <b>0.0241</b> |          | <b>380.0161</b> | <b>380.0161</b> | <b>7.2800e-003</b> | <b>6.9700e-003</b> | <b>382.2744</b> |

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

|             | ROG    | NOx         | CO     | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O | CO2e   |
|-------------|--------|-------------|--------|--------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-----|--------|
| Category    | lb/day |             |        |        |               |              |             |                |               |             | lb/day   |           |           |             |     |        |
| Mitigated   | 2.1736 | 1.8000e-004 | 0.0195 | 0.0000 |               | 7.0000e-005  | 7.0000e-005 |                | 7.0000e-005   | 7.0000e-005 |          | 0.0416    | 0.0416    | 1.1000e-004 |     | 0.0443 |
| Unmitigated | 2.1736 | 1.8000e-004 | 0.0195 | 0.0000 |               | 7.0000e-005  | 7.0000e-005 |                | 7.0000e-005   | 7.0000e-005 |          | 0.0416    | 0.0416    | 1.1000e-004 |     | 0.0443 |

6.2 Area by SubCategory

Unmitigated

|                       | ROG           | NOx                | CO            | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2 | NBio- CO2     | Total CO2     | CH4                | N2O | CO2e          |
|-----------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|----------|---------------|---------------|--------------------|-----|---------------|
| SubCategory           | lb/day        |                    |               |               |               |                    |                    |                |                    |                    | lb/day   |               |               |                    |     |               |
| Architectural Coating | 0.2316        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Consumer Products     | 1.9402        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Landscaping           | 1.8200e-003   | 1.8000e-004        | 0.0195        | 0.0000        |               | 7.0000e-005        | 7.0000e-005        |                | 7.0000e-005        | 7.0000e-005        |          | 0.0416        | 0.0416        | 1.1000e-004        |     | 0.0443        |
| <b>Total</b>          | <b>2.1736</b> | <b>1.8000e-004</b> | <b>0.0195</b> | <b>0.0000</b> |               | <b>7.0000e-005</b> | <b>7.0000e-005</b> |                | <b>7.0000e-005</b> | <b>7.0000e-005</b> |          | <b>0.0416</b> | <b>0.0416</b> | <b>1.1000e-004</b> |     | <b>0.0443</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

**6.2 Area by SubCategory**

Mitigated

|                       | ROG           | NOx                | CO            | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2 | NBio- CO2     | Total CO2     | CH4                | N2O | CO2e          |
|-----------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|----------|---------------|---------------|--------------------|-----|---------------|
| SubCategory           | lb/day        |                    |               |               |               |                    |                    |                |                    |                    | lb/day   |               |               |                    |     |               |
| Architectural Coating | 0.2316        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Consumer Products     | 1.9402        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Landscaping           | 1.8200e-003   | 1.8000e-004        | 0.0195        | 0.0000        |               | 7.0000e-005        | 7.0000e-005        |                | 7.0000e-005        | 7.0000e-005        |          | 0.0416        | 0.0416        | 1.1000e-004        |     | 0.0443        |
| <b>Total</b>          | <b>2.1736</b> | <b>1.8000e-004</b> | <b>0.0195</b> | <b>0.0000</b> |               | <b>7.0000e-005</b> | <b>7.0000e-005</b> |                | <b>7.0000e-005</b> | <b>7.0000e-005</b> |          | <b>0.0416</b> | <b>0.0416</b> | <b>1.1000e-004</b> |     | <b>0.0443</b> |

**7.0 Water Detail**

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**7.1 Mitigation Measures Water**

**8.0 Waste Detail**

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**8.1 Mitigation Measures Waste**

**9.0 Operational Offroad**

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| Equipment Type | Number | Hours/Day | Days/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|-----------|-------------|-------------|-----------|
|----------------|--------|-----------|-----------|-------------|-------------|-----------|

**10.0 Stationary Equipment**

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Fire Pumps and Emergency Generators

Mack Road Storage Facility- Modified Project - Sacramento County, Summer

| Equipment Type | Number | Hours/Day | Hours/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|------------|-------------|-------------|-----------|
|----------------|--------|-----------|------------|-------------|-------------|-----------|

**Boilers**

| Equipment Type | Number | Heat Input/Day | Heat Input/Year | Boiler Rating | Fuel Type |
|----------------|--------|----------------|-----------------|---------------|-----------|
|----------------|--------|----------------|-----------------|---------------|-----------|

**User Defined Equipment**

| Equipment Type | Number |
|----------------|--------|
|----------------|--------|

**11.0 Vegetation**

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Mack Road Storage Facility- Original Project - Sacramento County, Summer

**Mack Road Storage Facility- Original Project**  
**Sacramento County, Summer**

**1.0 Project Characteristics**

**1.1 Land Usage**

| Land Uses               | Size   | Metric   | Lot Acreage | Floor Surface Area | Population |
|-------------------------|--------|----------|-------------|--------------------|------------|
| General Office Building | 79.92  | 1000sqft | 4.52        | 79,919.00          | 0          |
| Parking Lot             | 137.00 | Space    | 3.00        | 54,800.00          | 0          |

**1.2 Other Project Characteristics**

|                                 |                                |                                 |       |                                  |       |
|---------------------------------|--------------------------------|---------------------------------|-------|----------------------------------|-------|
| <b>Urbanization</b>             | Urban                          | <b>Wind Speed (m/s)</b>         | 3.5   | <b>Precipitation Freq (Days)</b> | 58    |
| <b>Climate Zone</b>             | 6                              |                                 |       | <b>Operational Year</b>          | 2021  |
| <b>Utility Company</b>          | Pacific Gas & Electric Company |                                 |       |                                  |       |
| <b>CO2 Intensity (lb/MW hr)</b> | 641.35                         | <b>CH4 Intensity (lb/MW hr)</b> | 0.029 | <b>N2O Intensity (lb/MW hr)</b>  | 0.006 |

**1.3 User Entered Comments & Non-Default Data**

Project Characteristics -

Land Use - Lot acreage update to match that of the project.

Construction Phase - Building, paving and coating assumed to occur at the same time.

## Mack Road Storage Facility- Original Project - Sacramento County, Summer

| Table Name           | Column Name    | Default Value | New Value |
|----------------------|----------------|---------------|-----------|
| tblConstructionPhase | NumDays        | 20.00         | 230.00    |
| tblConstructionPhase | NumDays        | 20.00         | 230.00    |
| tblConstructionPhase | PhaseEndDate   | 6/2/2020      | 5/7/2020  |
| tblConstructionPhase | PhaseEndDate   | 4/7/2020      | 5/7/2020  |
| tblConstructionPhase | PhaseEndDate   | 5/21/2019     | 6/20/2019 |
| tblConstructionPhase | PhaseEndDate   | 5/5/2020      | 5/7/2020  |
| tblConstructionPhase | PhaseEndDate   | 4/23/2019     | 5/23/2019 |
| tblConstructionPhase | PhaseStartDate | 5/6/2020      | 6/21/2019 |
| tblConstructionPhase | PhaseStartDate | 5/22/2019     | 6/21/2019 |
| tblConstructionPhase | PhaseStartDate | 4/24/2019     | 5/24/2019 |
| tblConstructionPhase | PhaseStartDate | 4/8/2020      | 6/21/2019 |
| tblConstructionPhase | PhaseStartDate | 4/10/2019     | 5/10/2019 |
| tblLandUse           | LotAcreage     | 1.83          | 4.52      |
| tblLandUse           | LotAcreage     | 1.23          | 3.00      |

## 2.0 Emissions Summary

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Mack Road Storage Facility- Original Project - Sacramento County, Summer

**2.2 Overall Operational**

**Unmitigated Operational**

|              | ROG           | NOx           | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O                | CO2e              |
|--------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|--------------------|-------------------|
| Category     | lb/day        |               |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |                    |                   |
| Area         | 1.9389        | 2.0000e-004   | 0.0222         | 0.0000        |               | 8.0000e-005   | 8.0000e-005   |                | 8.0000e-005   | 8.0000e-005   |          | 0.0475            | 0.0475            | 1.3000e-004   |                    | 0.0506            |
| Energy       | 0.0309        | 0.2812        | 0.2362         | 1.6900e-003   |               | 0.0214        | 0.0214        |                | 0.0214        | 0.0214        |          | 337.4501          | 337.4501          | 6.4700e-003   | 6.1900e-003        | 339.4554          |
| Mobile       | 1.8110        | 5.6171        | 17.0874        | 0.0485        | 3.8622        | 0.0408        | 3.9030        | 1.0327         | 0.0382        | 1.0708        |          | 4,909.7817        | 4,909.7817        | 0.2364        |                    | 4,915.6907        |
| <b>Total</b> | <b>3.7808</b> | <b>5.8985</b> | <b>17.3459</b> | <b>0.0502</b> | <b>3.8622</b> | <b>0.0622</b> | <b>3.9244</b> | <b>1.0327</b>  | <b>0.0596</b> | <b>1.0923</b> |          | <b>5,247.2793</b> | <b>5,247.2793</b> | <b>0.2430</b> | <b>6.1900e-003</b> | <b>5,255.1967</b> |

**Mitigated Operational**

|              | ROG           | NOx           | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O                | CO2e              |
|--------------|---------------|---------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|--------------------|-------------------|
| Category     | lb/day        |               |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |                    |                   |
| Area         | 1.9389        | 2.0000e-004   | 0.0222         | 0.0000        |               | 8.0000e-005   | 8.0000e-005   |                | 8.0000e-005   | 8.0000e-005   |          | 0.0475            | 0.0475            | 1.3000e-004   |                    | 0.0506            |
| Energy       | 0.0309        | 0.2812        | 0.2362         | 1.6900e-003   |               | 0.0214        | 0.0214        |                | 0.0214        | 0.0214        |          | 337.4501          | 337.4501          | 6.4700e-003   | 6.1900e-003        | 339.4554          |
| Mobile       | 1.8110        | 5.6171        | 17.0874        | 0.0485        | 3.8622        | 0.0408        | 3.9030        | 1.0327         | 0.0382        | 1.0708        |          | 4,909.7817        | 4,909.7817        | 0.2364        |                    | 4,915.6907        |
| <b>Total</b> | <b>3.7808</b> | <b>5.8985</b> | <b>17.3459</b> | <b>0.0502</b> | <b>3.8622</b> | <b>0.0622</b> | <b>3.9244</b> | <b>1.0327</b>  | <b>0.0596</b> | <b>1.0923</b> |          | <b>5,247.2793</b> | <b>5,247.2793</b> | <b>0.2430</b> | <b>6.1900e-003</b> | <b>5,255.1967</b> |

## Mack Road Storage Facility- Original Project - Sacramento County, Summer

|                   | ROG  | NOx  | CO   | SO2  | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio-CO2 | Total CO2 | CH4  | N2O  | CO2e |
|-------------------|------|------|------|------|---------------|--------------|------------|----------------|---------------|-------------|----------|----------|-----------|------|------|------|
| Percent Reduction | 0.00 | 0.00 | 0.00 | 0.00 | 0.00          | 0.00         | 0.00       | 0.00           | 0.00          | 0.00        | 0.00     | 0.00     | 0.00      | 0.00 | 0.00 | 0.00 |

### 3.0 Construction Detail

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#### Construction Phase

| Phase Number | Phase Name            | Phase Type            | Start Date | End Date  | Num Days Week | Num Days | Phase Description |
|--------------|-----------------------|-----------------------|------------|-----------|---------------|----------|-------------------|
| 1            | Site Preparation      | Site Preparation      | 5/10/2019  | 5/23/2019 | 5             | 10       |                   |
| 2            | Grading               | Grading               | 5/24/2019  | 6/20/2019 | 5             | 20       |                   |
| 3            | Building Construction | Building Construction | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |
| 4            | Paving                | Paving                | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |
| 5            | Architectural Coating | Architectural Coating | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 10

Acres of Paving: 3

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 119,879; Non-Residential Outdoor: 39,960; Striped Parking Area: 3,288 (Architectural Coating – sqft)

#### OffRoad Equipment

Mack Road Storage Facility- Original Project - Sacramento County, Summer

| Phase Name            | Offroad Equipment Type    | Amount | Usage Hours | Horse Power | Load Factor |
|-----------------------|---------------------------|--------|-------------|-------------|-------------|
| Architectural Coating | Air Compressors           | 1      | 6.00        | 78          | 0.48        |
| Grading               | Excavators                | 1      | 8.00        | 158         | 0.38        |
| Building Construction | Cranes                    | 1      | 7.00        | 231         | 0.29        |
| Building Construction | Forklifts                 | 3      | 8.00        | 89          | 0.20        |
| Building Construction | Generator Sets            | 1      | 8.00        | 84          | 0.74        |
| Paving                | Pavers                    | 2      | 8.00        | 130         | 0.42        |
| Paving                | Rollers                   | 2      | 8.00        | 80          | 0.38        |
| Grading               | Rubber Tired Dozers       | 1      | 8.00        | 247         | 0.40        |
| Building Construction | Tractors/Loaders/Backhoes | 3      | 7.00        | 97          | 0.37        |
| Grading               | Graders                   | 1      | 8.00        | 187         | 0.41        |
| Grading               | Tractors/Loaders/Backhoes | 3      | 8.00        | 97          | 0.37        |
| Paving                | Paving Equipment          | 2      | 8.00        | 132         | 0.36        |
| Site Preparation      | Tractors/Loaders/Backhoes | 4      | 8.00        | 97          | 0.37        |
| Site Preparation      | Rubber Tired Dozers       | 3      | 8.00        | 247         | 0.40        |
| Building Construction | Welders                   | 1      | 8.00        | 46          | 0.45        |

**Trips and VMT**

| Phase Name            | Offroad Equipment Count | Worker Trip Number | Vendor Trip Number | Hauling Trip Number | Worker Trip Length | Vendor Trip Length | Hauling Trip Length | Worker Vehicle Class | Vendor Vehicle Class | Hauling Vehicle Class |
|-----------------------|-------------------------|--------------------|--------------------|---------------------|--------------------|--------------------|---------------------|----------------------|----------------------|-----------------------|
| Site Preparation      | 7                       | 18.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Grading               | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Building Construction | 9                       | 49.00              | 22.00              | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Paving                | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Architectural Coating | 1                       | 10.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |

**3.1 Mitigation Measures Construction**

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.2 Site Preparation - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10  | Exhaust PM10  | PM10 Total     | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total    | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|----------------|---------------|----------------|----------------|---------------|----------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |                |               |                |                |               |                | lb/day   |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 18.0663        | 0.0000        | 18.0663        | 9.9307         | 0.0000        | 9.9307         |          |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 4.3350        | 45.5727        | 22.0630        | 0.0380        |                | 2.3904        | 2.3904         |                | 2.1991        | 2.1991         |          | 3,766.4529        | 3,766.4529        | 1.1917        |     | 3,796.2445        |
| <b>Total</b>  | <b>4.3350</b> | <b>45.5727</b> | <b>22.0630</b> | <b>0.0380</b> | <b>18.0663</b> | <b>2.3904</b> | <b>20.4566</b> | <b>9.9307</b>  | <b>2.1991</b> | <b>12.1298</b> |          | <b>3,766.4529</b> | <b>3,766.4529</b> | <b>1.1917</b> |     | <b>3,796.2445</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0843        | 0.0463        | 0.6499        | 1.4800e-003        | 0.1369        | 9.8000e-004        | 0.1379        | 0.0363         | 9.0000e-004        | 0.0372        |          | 147.3555        | 147.3555        | 4.6400e-003        |     | 147.4714        |
| <b>Total</b> | <b>0.0843</b> | <b>0.0463</b> | <b>0.6499</b> | <b>1.4800e-003</b> | <b>0.1369</b> | <b>9.8000e-004</b> | <b>0.1379</b> | <b>0.0363</b>  | <b>9.0000e-004</b> | <b>0.0372</b> |          | <b>147.3555</b> | <b>147.3555</b> | <b>4.6400e-003</b> |     | <b>147.4714</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.2 Site Preparation - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10  | Exhaust PM10  | PM10 Total     | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total    | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|----------------|---------------|----------------|----------------|---------------|----------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |                |               |                |                |               |                | lb/day        |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 18.0663        | 0.0000        | 18.0663        | 9.9307         | 0.0000        | 9.9307         |               |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 4.3350        | 45.5727        | 22.0630        | 0.0380        |                | 2.3904        | 2.3904         |                | 2.1991        | 2.1991         | 0.0000        | 3,766.4529        | 3,766.4529        | 1.1917        |     | 3,796.2445        |
| <b>Total</b>  | <b>4.3350</b> | <b>45.5727</b> | <b>22.0630</b> | <b>0.0380</b> | <b>18.0663</b> | <b>2.3904</b> | <b>20.4566</b> | <b>9.9307</b>  | <b>2.1991</b> | <b>12.1298</b> | <b>0.0000</b> | <b>3,766.4529</b> | <b>3,766.4529</b> | <b>1.1917</b> |     | <b>3,796.2445</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0843        | 0.0463        | 0.6499        | 1.4800e-003        | 0.1369        | 9.8000e-004        | 0.1379        | 0.0363         | 9.0000e-004        | 0.0372        |          | 147.3555        | 147.3555        | 4.6400e-003        |     | 147.4714        |
| <b>Total</b> | <b>0.0843</b> | <b>0.0463</b> | <b>0.6499</b> | <b>1.4800e-003</b> | <b>0.1369</b> | <b>9.8000e-004</b> | <b>0.1379</b> | <b>0.0363</b>  | <b>9.0000e-004</b> | <b>0.0372</b> |          | <b>147.3555</b> | <b>147.3555</b> | <b>4.6400e-003</b> |     | <b>147.4714</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.3 Grading - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 6.5523        | 0.0000        | 6.5523        | 3.3675         | 0.0000        | 3.3675        |          |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 2.5805        | 28.3480        | 16.2934        | 0.0297        |               | 1.3974        | 1.3974        |                | 1.2856        | 1.2856        |          | 2,936.8068        | 2,936.8068        | 0.9292        |     | 2,960.0361        |
| <b>Total</b>  | <b>2.5805</b> | <b>28.3480</b> | <b>16.2934</b> | <b>0.0297</b> | <b>6.5523</b> | <b>1.3974</b> | <b>7.9497</b> | <b>3.3675</b>  | <b>1.2856</b> | <b>4.6531</b> |          | <b>2,936.8068</b> | <b>2,936.8068</b> | <b>0.9292</b> |     | <b>2,960.0361</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.3 Grading - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|---------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category      | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Fugitive Dust |               |                |                |               | 6.5523        | 0.0000        | 6.5523        | 3.3675         | 0.0000        | 3.3675        |               |                   | 0.0000            |               |     | 0.0000            |
| Off-Road      | 2.5805        | 28.3480        | 16.2934        | 0.0297        |               | 1.3974        | 1.3974        |                | 1.2856        | 1.2856        | 0.0000        | 2,936.8068        | 2,936.8068        | 0.9292        |     | 2,960.0361        |
| <b>Total</b>  | <b>2.5805</b> | <b>28.3480</b> | <b>16.2934</b> | <b>0.0297</b> | <b>6.5523</b> | <b>1.3974</b> | <b>7.9497</b> | <b>3.3675</b>  | <b>1.2856</b> | <b>4.6531</b> | <b>0.0000</b> | <b>2,936.8068</b> | <b>2,936.8068</b> | <b>0.9292</b> |     | <b>2,960.0361</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |



Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.4 Building Construction - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                        |                        |               |     |                        |
| Off-Road     | 2.3612        | 21.0788        | 17.1638        | 0.0269        |               | 1.2899        | 1.2899        |                | 1.2127        | 1.2127        |          | 2,591.580<br>2         | 2,591.580<br>2         | 0.6313        |     | 2,607.363<br>5         |
| <b>Total</b> | <b>2.3612</b> | <b>21.0788</b> | <b>17.1638</b> | <b>0.0269</b> |               | <b>1.2899</b> | <b>1.2899</b> |                | <b>1.2127</b> | <b>1.2127</b> |          | <b>2,591.580<br/>2</b> | <b>2,591.580<br/>2</b> | <b>0.6313</b> |     | <b>2,607.363<br/>5</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.1041        | 2.6352        | 0.7868        | 5.5200e-003        | 0.1324        | 0.0190        | 0.1514        | 0.0381         | 0.0182        | 0.0563        |          | 583.5520        | 583.5520        | 0.0351        |     | 584.4288        |
| Worker       | 0.2296        | 0.1261        | 1.7692        | 4.0300e-003        | 0.3727        | 2.6600e-003   | 0.3754        | 0.0989         | 2.4500e-003   | 0.1013        |          | 401.1344        | 401.1344        | 0.0126        |     | 401.4500        |
| <b>Total</b> | <b>0.3337</b> | <b>2.7613</b> | <b>2.5560</b> | <b>9.5500e-003</b> | <b>0.5052</b> | <b>0.0217</b> | <b>0.5268</b> | <b>0.1370</b>  | <b>0.0206</b> | <b>0.1576</b> |          | <b>984.6865</b> | <b>984.6865</b> | <b>0.0477</b> |     | <b>985.8788</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.4 Building Construction - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                        |                        |               |     |                        |
| Off-Road     | 2.3612        | 21.0788        | 17.1638        | 0.0269        |               | 1.2899        | 1.2899        |                | 1.2127        | 1.2127        | 0.0000        | 2,591.580<br>2         | 2,591.580<br>2         | 0.6313        |     | 2,607.363<br>5         |
| <b>Total</b> | <b>2.3612</b> | <b>21.0788</b> | <b>17.1638</b> | <b>0.0269</b> |               | <b>1.2899</b> | <b>1.2899</b> |                | <b>1.2127</b> | <b>1.2127</b> | <b>0.0000</b> | <b>2,591.580<br/>2</b> | <b>2,591.580<br/>2</b> | <b>0.6313</b> |     | <b>2,607.363<br/>5</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.1041        | 2.6352        | 0.7868        | 5.5200e-003        | 0.1324        | 0.0190        | 0.1514        | 0.0381         | 0.0182        | 0.0563        |          | 583.5520        | 583.5520        | 0.0351        |     | 584.4288        |
| Worker       | 0.2296        | 0.1261        | 1.7692        | 4.0300e-003        | 0.3727        | 2.6600e-003   | 0.3754        | 0.0989         | 2.4500e-003   | 0.1013        |          | 401.1344        | 401.1344        | 0.0126        |     | 401.4500        |
| <b>Total</b> | <b>0.3337</b> | <b>2.7613</b> | <b>2.5560</b> | <b>9.5500e-003</b> | <b>0.5052</b> | <b>0.0217</b> | <b>0.5268</b> | <b>0.1370</b>  | <b>0.0206</b> | <b>0.1576</b> |          | <b>984.6865</b> | <b>984.6865</b> | <b>0.0477</b> |     | <b>985.8788</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.4 Building Construction - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 2.1198        | 19.1860        | 16.8485        | 0.0269        |               | 1.1171        | 1.1171        |                | 1.0503        | 1.0503        |          | 2,553.0631        | 2,553.0631        | 0.6229        |     | 2,568.6345        |
| <b>Total</b> | <b>2.1198</b> | <b>19.1860</b> | <b>16.8485</b> | <b>0.0269</b> |               | <b>1.1171</b> | <b>1.1171</b> |                | <b>1.0503</b> | <b>1.0503</b> |          | <b>2,553.0631</b> | <b>2,553.0631</b> | <b>0.6229</b> |     | <b>2,568.6345</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0830        | 2.4157        | 0.6493        | 5.4800e-003        | 0.1324        | 0.0126        | 0.1450        | 0.0381         | 0.0121        | 0.0502        |          | 580.0419        | 580.0419        | 0.0329        |     | 580.8636        |
| Worker       | 0.2113        | 0.1121        | 1.5999        | 3.9100e-003        | 0.3727        | 2.5900e-003   | 0.3753        | 0.0989         | 2.3900e-003   | 0.1013        |          | 388.8214        | 388.8214        | 0.0111        |     | 389.0998        |
| <b>Total</b> | <b>0.2943</b> | <b>2.5278</b> | <b>2.2491</b> | <b>9.3900e-003</b> | <b>0.5051</b> | <b>0.0152</b> | <b>0.5203</b> | <b>0.1370</b>  | <b>0.0144</b> | <b>0.1514</b> |          | <b>968.8633</b> | <b>968.8633</b> | <b>0.0440</b> |     | <b>969.9635</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.4 Building Construction - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2              | Total CO2              | CH4           | N2O | CO2e                   |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|------------------------|------------------------|---------------|-----|------------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                        |                        |               |     |                        |
| Off-Road     | 2.1198        | 19.1860        | 16.8485        | 0.0269        |               | 1.1171        | 1.1171        |                | 1.0503        | 1.0503        | 0.0000        | 2,553.063<br>1         | 2,553.063<br>1         | 0.6229        |     | 2,568.634<br>5         |
| <b>Total</b> | <b>2.1198</b> | <b>19.1860</b> | <b>16.8485</b> | <b>0.0269</b> |               | <b>1.1171</b> | <b>1.1171</b> |                | <b>1.0503</b> | <b>1.0503</b> | <b>0.0000</b> | <b>2,553.063<br/>1</b> | <b>2,553.063<br/>1</b> | <b>0.6229</b> |     | <b>2,568.634<br/>5</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000        |     | 0.0000          |
| Vendor       | 0.0830        | 2.4157        | 0.6493        | 5.4800e-003        | 0.1324        | 0.0126        | 0.1450        | 0.0381         | 0.0121        | 0.0502        |          | 580.0419        | 580.0419        | 0.0329        |     | 580.8636        |
| Worker       | 0.2113        | 0.1121        | 1.5999        | 3.9100e-003        | 0.3727        | 2.5900e-003   | 0.3753        | 0.0989         | 2.3900e-003   | 0.1013        |          | 388.8214        | 388.8214        | 0.0111        |     | 389.0998        |
| <b>Total</b> | <b>0.2943</b> | <b>2.5278</b> | <b>2.2491</b> | <b>9.3900e-003</b> | <b>0.5051</b> | <b>0.0152</b> | <b>0.5203</b> | <b>0.1370</b>  | <b>0.0144</b> | <b>0.1514</b> |          | <b>968.8633</b> | <b>968.8633</b> | <b>0.0440</b> |     | <b>969.9635</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.5 Paving - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 1.4544        | 15.2441        | 14.6648        | 0.0228        |               | 0.8246        | 0.8246        |                | 0.7586        | 0.7586        |          | 2,257.0025        | 2,257.0025        | 0.7141        |     | 2,274.8548        |
| Paving       | 0.0342        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.4886</b> | <b>15.2441</b> | <b>14.6648</b> | <b>0.0228</b> |               | <b>0.8246</b> | <b>0.8246</b> |                | <b>0.7586</b> | <b>0.7586</b> |          | <b>2,257.0025</b> | <b>2,257.0025</b> | <b>0.7141</b> |     | <b>2,274.8548</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.5 Paving - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Off-Road     | 1.4544        | 15.2441        | 14.6648        | 0.0228        |               | 0.8246        | 0.8246        |                | 0.7586        | 0.7586        | 0.0000        | 2,257.0025        | 2,257.0025        | 0.7141        |     | 2,274.8548        |
| Paving       | 0.0342        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.4886</b> | <b>15.2441</b> | <b>14.6648</b> | <b>0.0228</b> |               | <b>0.8246</b> | <b>0.8246</b> |                | <b>0.7586</b> | <b>0.7586</b> | <b>0.0000</b> | <b>2,257.0025</b> | <b>2,257.0025</b> | <b>0.7141</b> |     | <b>2,274.8548</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0703        | 0.0386        | 0.5416        | 1.2300e-003        | 0.1141        | 8.1000e-004        | 0.1149        | 0.0303         | 7.5000e-004        | 0.0310        |          | 122.7963        | 122.7963        | 3.8600e-003        |     | 122.8929        |
| <b>Total</b> | <b>0.0703</b> | <b>0.0386</b> | <b>0.5416</b> | <b>1.2300e-003</b> | <b>0.1141</b> | <b>8.1000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.5000e-004</b> | <b>0.0310</b> |          | <b>122.7963</b> | <b>122.7963</b> | <b>3.8600e-003</b> |     | <b>122.8929</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.5 Paving - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day   |                   |                   |               |     |                   |
| Off-Road     | 1.3566        | 14.0656        | 14.6521        | 0.0228        |               | 0.7528        | 0.7528        |                | 0.6926        | 0.6926        |          | 2,207.7334        | 2,207.7334        | 0.7140        |     | 2,225.5841        |
| Paving       | 0.0342        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.3907</b> | <b>14.0656</b> | <b>14.6521</b> | <b>0.0228</b> |               | <b>0.7528</b> | <b>0.7528</b> |                | <b>0.6926</b> | <b>0.6926</b> |          | <b>2,207.7334</b> | <b>2,207.7334</b> | <b>0.7140</b> |     | <b>2,225.5841</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0647        | 0.0343        | 0.4898        | 1.2000e-003        | 0.1141        | 7.9000e-004        | 0.1149        | 0.0303         | 7.3000e-004        | 0.0310        |          | 119.0269        | 119.0269        | 3.4100e-003        |     | 119.1122        |
| <b>Total</b> | <b>0.0647</b> | <b>0.0343</b> | <b>0.4898</b> | <b>1.2000e-003</b> | <b>0.1141</b> | <b>7.9000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.3000e-004</b> | <b>0.0310</b> |          | <b>119.0269</b> | <b>119.0269</b> | <b>3.4100e-003</b> |     | <b>119.1122</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.5 Paving - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx            | CO             | SO2           | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2         | Total CO2         | CH4           | N2O | CO2e              |
|--------------|---------------|----------------|----------------|---------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-------------------|-------------------|---------------|-----|-------------------|
| Category     | lb/day        |                |                |               |               |               |               |                |               |               | lb/day        |                   |                   |               |     |                   |
| Off-Road     | 1.3566        | 14.0656        | 14.6521        | 0.0228        |               | 0.7528        | 0.7528        |                | 0.6926        | 0.6926        | 0.0000        | 2,207.7334        | 2,207.7334        | 0.7140        |     | 2,225.5841        |
| Paving       | 0.0342        |                |                |               |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                   | 0.0000            |               |     | 0.0000            |
| <b>Total</b> | <b>1.3907</b> | <b>14.0656</b> | <b>14.6521</b> | <b>0.0228</b> |               | <b>0.7528</b> | <b>0.7528</b> |                | <b>0.6926</b> | <b>0.6926</b> | <b>0.0000</b> | <b>2,207.7334</b> | <b>2,207.7334</b> | <b>0.7140</b> |     | <b>2,225.5841</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|-----------------|-----------------|--------------------|-----|-----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                 |                 |                    |     |                 |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             |     | 0.0000          |
| Worker       | 0.0647        | 0.0343        | 0.4898        | 1.2000e-003        | 0.1141        | 7.9000e-004        | 0.1149        | 0.0303         | 7.3000e-004        | 0.0310        |          | 119.0269        | 119.0269        | 3.4100e-003        |     | 119.1122        |
| <b>Total</b> | <b>0.0647</b> | <b>0.0343</b> | <b>0.4898</b> | <b>1.2000e-003</b> | <b>0.1141</b> | <b>7.9000e-004</b> | <b>0.1149</b> | <b>0.0303</b>  | <b>7.3000e-004</b> | <b>0.0310</b> |          | <b>119.0269</b> | <b>119.0269</b> | <b>3.4100e-003</b> |     | <b>119.1122</b> |



Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.6 Architectural Coating - 2019**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Archit. Coating | 3.2874        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2664        | 1.8354        | 1.8413        | 2.9700e-003        |               | 0.1288        | 0.1288        |                | 0.1288        | 0.1288        |          | 281.4481        | 281.4481        | 0.0238        |     | 282.0423        |
| <b>Total</b>    | <b>3.5538</b> | <b>1.8354</b> | <b>1.8413</b> | <b>2.9700e-003</b> |               | <b>0.1288</b> | <b>0.1288</b> |                | <b>0.1288</b> | <b>0.1288</b> |          | <b>281.4481</b> | <b>281.4481</b> | <b>0.0238</b> |     | <b>282.0423</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0469        | 0.0257        | 0.3611        | 8.2000e-004        | 0.0761        | 5.4000e-004        | 0.0766        | 0.0202         | 5.0000e-004        | 0.0207        |          | 81.8642        | 81.8642        | 2.5800e-003        |     | 81.9286        |
| <b>Total</b> | <b>0.0469</b> | <b>0.0257</b> | <b>0.3611</b> | <b>8.2000e-004</b> | <b>0.0761</b> | <b>5.4000e-004</b> | <b>0.0766</b> | <b>0.0202</b>  | <b>5.0000e-004</b> | <b>0.0207</b> |          | <b>81.8642</b> | <b>81.8642</b> | <b>2.5800e-003</b> |     | <b>81.9286</b> |

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**3.6 Architectural Coating - 2019**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day        |                 |                 |               |     |                 |
| Archit. Coating | 3.2874        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2664        | 1.8354        | 1.8413        | 2.9700e-003        |               | 0.1288        | 0.1288        |                | 0.1288        | 0.1288        | 0.0000        | 281.4481        | 281.4481        | 0.0238        |     | 282.0423        |
| <b>Total</b>    | <b>3.5538</b> | <b>1.8354</b> | <b>1.8413</b> | <b>2.9700e-003</b> |               | <b>0.1288</b> | <b>0.1288</b> |                | <b>0.1288</b> | <b>0.1288</b> | <b>0.0000</b> | <b>281.4481</b> | <b>281.4481</b> | <b>0.0238</b> |     | <b>282.0423</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0469        | 0.0257        | 0.3611        | 8.2000e-004        | 0.0761        | 5.4000e-004        | 0.0766        | 0.0202         | 5.0000e-004        | 0.0207        |          | 81.8642        | 81.8642        | 2.5800e-003        |     | 81.9286        |
| <b>Total</b> | <b>0.0469</b> | <b>0.0257</b> | <b>0.3611</b> | <b>8.2000e-004</b> | <b>0.0761</b> | <b>5.4000e-004</b> | <b>0.0766</b> | <b>0.0202</b>  | <b>5.0000e-004</b> | <b>0.0207</b> |          | <b>81.8642</b> | <b>81.8642</b> | <b>2.5800e-003</b> |     | <b>81.9286</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.6 Architectural Coating - 2020**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |               |     |                 |
| Archit. Coating | 3.2874        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2422        | 1.6838        | 1.8314        | 2.9700e-003        |               | 0.1109        | 0.1109        |                | 0.1109        | 0.1109        |          | 281.4481        | 281.4481        | 0.0218        |     | 281.9928        |
| <b>Total</b>    | <b>3.5295</b> | <b>1.6838</b> | <b>1.8314</b> | <b>2.9700e-003</b> |               | <b>0.1109</b> | <b>0.1109</b> |                | <b>0.1109</b> | <b>0.1109</b> |          | <b>281.4481</b> | <b>281.4481</b> | <b>0.0218</b> |     | <b>281.9928</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0431        | 0.0229        | 0.3265        | 8.0000e-004        | 0.0761        | 5.3000e-004        | 0.0766        | 0.0202         | 4.9000e-004        | 0.0207        |          | 79.3513        | 79.3513        | 2.2700e-003        |     | 79.4081        |
| <b>Total</b> | <b>0.0431</b> | <b>0.0229</b> | <b>0.3265</b> | <b>8.0000e-004</b> | <b>0.0761</b> | <b>5.3000e-004</b> | <b>0.0766</b> | <b>0.0202</b>  | <b>4.9000e-004</b> | <b>0.0207</b> |          | <b>79.3513</b> | <b>79.3513</b> | <b>2.2700e-003</b> |     | <b>79.4081</b> |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**3.6 Architectural Coating - 2020**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O | CO2e            |
|-----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|-----|-----------------|
| Category        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day        |                 |                 |               |     |                 |
| Archit. Coating | 3.2874        |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |               |                 | 0.0000          |               |     | 0.0000          |
| Off-Road        | 0.2422        | 1.6838        | 1.8314        | 2.9700e-003        |               | 0.1109        | 0.1109        |                | 0.1109        | 0.1109        | 0.0000        | 281.4481        | 281.4481        | 0.0218        |     | 281.9928        |
| <b>Total</b>    | <b>3.5295</b> | <b>1.6838</b> | <b>1.8314</b> | <b>2.9700e-003</b> |               | <b>0.1109</b> | <b>0.1109</b> |                | <b>0.1109</b> | <b>0.1109</b> | <b>0.0000</b> | <b>281.4481</b> | <b>281.4481</b> | <b>0.0218</b> |     | <b>281.9928</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2 | NBio- CO2      | Total CO2      | CH4                | N2O | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------|----------------|----------------|--------------------|-----|----------------|
| Category     | lb/day        |               |               |                    |               |                    |               |                |                    |               | lb/day   |                |                |                    |     |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Vendor       | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000         | 0.0000             | 0.0000        |          | 0.0000         | 0.0000         | 0.0000             |     | 0.0000         |
| Worker       | 0.0431        | 0.0229        | 0.3265        | 8.0000e-004        | 0.0761        | 5.3000e-004        | 0.0766        | 0.0202         | 4.9000e-004        | 0.0207        |          | 79.3513        | 79.3513        | 2.2700e-003        |     | 79.4081        |
| <b>Total</b> | <b>0.0431</b> | <b>0.0229</b> | <b>0.3265</b> | <b>8.0000e-004</b> | <b>0.0761</b> | <b>5.3000e-004</b> | <b>0.0766</b> | <b>0.0202</b>  | <b>4.9000e-004</b> | <b>0.0207</b> |          | <b>79.3513</b> | <b>79.3513</b> | <b>2.2700e-003</b> |     | <b>79.4081</b> |

**4.0 Operational Detail - Mobile**

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Mack Road Storage Facility- Original Project - Sacramento County, Summer

**4.1 Mitigation Measures Mobile**

|             | ROG    | NOx    | CO      | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2  | Total CO2  | CH4    | N2O | CO2e       |
|-------------|--------|--------|---------|--------|---------------|--------------|------------|----------------|---------------|-------------|----------|------------|------------|--------|-----|------------|
| Category    | lb/day |        |         |        |               |              |            |                |               |             | lb/day   |            |            |        |     |            |
| Mitigated   | 1.8110 | 5.6171 | 17.0874 | 0.0485 | 3.8622        | 0.0408       | 3.9030     | 1.0327         | 0.0382        | 1.0708      |          | 4,909.7817 | 4,909.7817 | 0.2364 |     | 4,915.6907 |
| Unmitigated | 1.8110 | 5.6171 | 17.0874 | 0.0485 | 3.8622        | 0.0408       | 3.9030     | 1.0327         | 0.0382        | 1.0708      |          | 4,909.7817 | 4,909.7817 | 0.2364 |     | 4,915.6907 |

**4.2 Trip Summary Information**

| Land Use                | Average Daily Trip Rate |          |        | Unmitigated | Mitigated  |
|-------------------------|-------------------------|----------|--------|-------------|------------|
|                         | Weekday                 | Saturday | Sunday | Annual VMT  | Annual VMT |
| General Office Building | 881.51                  | 196.60   | 83.91  | 1,383,044   | 1,383,044  |
| Parking Lot             | 0.00                    | 0.00     | 0.00   |             |            |
| Total                   | 881.51                  | 196.60   | 83.91  | 1,383,044   | 1,383,044  |

**4.3 Trip Type Information**

| Land Use                | Miles      |            |             | Trip %     |            |             | Trip Purpose % |          |         |
|-------------------------|------------|------------|-------------|------------|------------|-------------|----------------|----------|---------|
|                         | H-W or C-W | H-S or C-C | H-O or C-NW | H-W or C-W | H-S or C-C | H-O or C-NW | Primary        | Diverted | Pass-by |
| General Office Building | 10.00      | 5.00       | 6.50        | 33.00      | 48.00      | 19.00       | 77             | 19       | 4       |
| Parking Lot             | 10.00      | 5.00       | 6.50        | 0.00       | 0.00       | 0.00        | 0              | 0        | 0       |

**4.4 Fleet Mix**

Mack Road Storage Facility- Original Project - Sacramento County, Summer

| Land Use                | LDA      | LDT1     | LDT2     | MDV      | LHD1     | LHD2     | MHD      | HHD      | OBUS     | UBUS     | MCY      | SBUS     | MH       |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| General Office Building | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |
| Parking Lot             | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |

**5.0 Energy Detail**

Historical Energy Use: N

**5.1 Mitigation Measures Energy**

|                        | ROG    | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O         | CO2e     |
|------------------------|--------|--------|--------|-------------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-------------|----------|
| Category               | lb/day |        |        |             |               |              |            |                |               |             | lb/day   |           |           |             |             |          |
| NaturalGas Mitigated   | 0.0309 | 0.2812 | 0.2362 | 1.6900e-003 |               | 0.0214       | 0.0214     |                | 0.0214        | 0.0214      |          | 337.4501  | 337.4501  | 6.4700e-003 | 6.1900e-003 | 339.4554 |
| NaturalGas Unmitigated | 0.0309 | 0.2812 | 0.2362 | 1.6900e-003 |               | 0.0214       | 0.0214     |                | 0.0214        | 0.0214      |          | 337.4501  | 337.4501  | 6.4700e-003 | 6.1900e-003 | 339.4554 |

Mack Road Storage Facility- Original Project - Sacramento County, Summer

**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

|                         | NaturalGas Use | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O                | CO2e            |
|-------------------------|----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|--------------------|--------------------|-----------------|
| Land Use                | kBTU/yr        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |                    |                    |                 |
| General Office Building | 2868.33        | 0.0309        | 0.2812        | 0.2362        | 1.6900e-003        |               | 0.0214        | 0.0214        |                | 0.0214        | 0.0214        |          | 337.4501        | 337.4501        | 6.4700e-003        | 6.1900e-003        | 339.4554        |
| Parking Lot             | 0              | 0.0000        | 0.0000        | 0.0000        | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             | 0.0000             | 0.0000          |
| <b>Total</b>            |                | <b>0.0309</b> | <b>0.2812</b> | <b>0.2362</b> | <b>1.6900e-003</b> |               | <b>0.0214</b> | <b>0.0214</b> |                | <b>0.0214</b> | <b>0.0214</b> |          | <b>337.4501</b> | <b>337.4501</b> | <b>6.4700e-003</b> | <b>6.1900e-003</b> | <b>339.4554</b> |

**Mitigated**

|                         | NaturalGas Use | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2 | NBio- CO2       | Total CO2       | CH4                | N2O                | CO2e            |
|-------------------------|----------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------|-----------------|-----------------|--------------------|--------------------|-----------------|
| Land Use                | kBTU/yr        | lb/day        |               |               |                    |               |               |               |                |               |               | lb/day   |                 |                 |                    |                    |                 |
| General Office Building | 2.86833        | 0.0309        | 0.2812        | 0.2362        | 1.6900e-003        |               | 0.0214        | 0.0214        |                | 0.0214        | 0.0214        |          | 337.4501        | 337.4501        | 6.4700e-003        | 6.1900e-003        | 339.4554        |
| Parking Lot             | 0              | 0.0000        | 0.0000        | 0.0000        | 0.0000             |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        |          | 0.0000          | 0.0000          | 0.0000             | 0.0000             | 0.0000          |
| <b>Total</b>            |                | <b>0.0309</b> | <b>0.2812</b> | <b>0.2362</b> | <b>1.6900e-003</b> |               | <b>0.0214</b> | <b>0.0214</b> |                | <b>0.0214</b> | <b>0.0214</b> |          | <b>337.4501</b> | <b>337.4501</b> | <b>6.4700e-003</b> | <b>6.1900e-003</b> | <b>339.4554</b> |

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

Mack Road Storage Facility- Original Project - Sacramento County, Summer

|             | ROG    | NOx         | CO     | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O | CO2e   |
|-------------|--------|-------------|--------|--------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-----|--------|
| Category    | lb/day |             |        |        |               |              |             |                |               |             | lb/day   |           |           |             |     |        |
| Mitigated   | 1.9389 | 2.0000e-004 | 0.0222 | 0.0000 |               | 8.0000e-005  | 8.0000e-005 |                | 8.0000e-005   | 8.0000e-005 |          | 0.0475    | 0.0475    | 1.3000e-004 |     | 0.0506 |
| Unmitigated | 1.9389 | 2.0000e-004 | 0.0222 | 0.0000 |               | 8.0000e-005  | 8.0000e-005 |                | 8.0000e-005   | 8.0000e-005 |          | 0.0475    | 0.0475    | 1.3000e-004 |     | 0.0506 |

6.2 Area by SubCategory

Unmitigated

|                       | ROG           | NOx                | CO            | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2 | NBio- CO2     | Total CO2     | CH4                | N2O | CO2e          |
|-----------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|----------|---------------|---------------|--------------------|-----|---------------|
| SubCategory           | lb/day        |                    |               |               |               |                    |                    |                |                    |                    | lb/day   |               |               |                    |     |               |
| Architectural Coating | 0.2072        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Consumer Products     | 1.7297        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Landscaping           | 2.0800e-003   | 2.0000e-004        | 0.0222        | 0.0000        |               | 8.0000e-005        | 8.0000e-005        |                | 8.0000e-005        | 8.0000e-005        |          | 0.0475        | 0.0475        | 1.3000e-004        |     | 0.0506        |
| <b>Total</b>          | <b>1.9389</b> | <b>2.0000e-004</b> | <b>0.0222</b> | <b>0.0000</b> |               | <b>8.0000e-005</b> | <b>8.0000e-005</b> |                | <b>8.0000e-005</b> | <b>8.0000e-005</b> |          | <b>0.0475</b> | <b>0.0475</b> | <b>1.3000e-004</b> |     | <b>0.0506</b> |



Mack Road Storage Facility- Original Project - Sacramento County, Summer

**6.2 Area by SubCategory**

Mitigated

|                       | ROG           | NOx                | CO            | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2 | NBio- CO2     | Total CO2     | CH4                | N2O | CO2e          |
|-----------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|----------|---------------|---------------|--------------------|-----|---------------|
| SubCategory           | lb/day        |                    |               |               |               |                    |                    |                |                    |                    | lb/day   |               |               |                    |     |               |
| Architectural Coating | 0.2072        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Consumer Products     | 1.7297        |                    |               |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             |          |               | 0.0000        |                    |     | 0.0000        |
| Landscaping           | 2.0800e-003   | 2.0000e-004        | 0.0222        | 0.0000        |               | 8.0000e-005        | 8.0000e-005        |                | 8.0000e-005        | 8.0000e-005        |          | 0.0475        | 0.0475        | 1.3000e-004        |     | 0.0506        |
| <b>Total</b>          | <b>1.9389</b> | <b>2.0000e-004</b> | <b>0.0222</b> | <b>0.0000</b> |               | <b>8.0000e-005</b> | <b>8.0000e-005</b> |                | <b>8.0000e-005</b> | <b>8.0000e-005</b> |          | <b>0.0475</b> | <b>0.0475</b> | <b>1.3000e-004</b> |     | <b>0.0506</b> |

**7.0 Water Detail**

**7.1 Mitigation Measures Water**

**8.0 Waste Detail**

**8.1 Mitigation Measures Waste**

**9.0 Operational Offroad**

| Equipment Type | Number | Hours/Day | Days/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|-----------|-------------|-------------|-----------|
|----------------|--------|-----------|-----------|-------------|-------------|-----------|

**10.0 Stationary Equipment**

Fire Pumps and Emergency Generators

Mack Road Storage Facility- Original Project - Sacramento County, Summer

| Equipment Type | Number | Hours/Day | Hours/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|------------|-------------|-------------|-----------|
|----------------|--------|-----------|------------|-------------|-------------|-----------|

**Boilers**

| Equipment Type | Number | Heat Input/Day | Heat Input/Year | Boiler Rating | Fuel Type |
|----------------|--------|----------------|-----------------|---------------|-----------|
|----------------|--------|----------------|-----------------|---------------|-----------|

**User Defined Equipment**

| Equipment Type | Number |
|----------------|--------|
|----------------|--------|

**11.0 Vegetation**

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**Attachment B – CalEEMod Output File for Greenhouse Gas Emissions**

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Mack Road Storage Facility- Modified Project - Sacramento County, Annual

**Mack Road Storage Facility- Modified Project  
Sacramento County, Annual**

**1.0 Project Characteristics**

**1.1 Land Usage**

| Land Uses               | Size   | Metric   | Lot Acreage | Floor Surface Area | Population |
|-------------------------|--------|----------|-------------|--------------------|------------|
| General Office Building | 90.00  | 1000sqft | 5.00        | 90,000.00          | 0          |
| Parking Lot             | 100.00 | Space    | 2.52        | 40,000.00          | 0          |

**1.2 Other Project Characteristics**

|                                |                                |                                |       |                                  |       |
|--------------------------------|--------------------------------|--------------------------------|-------|----------------------------------|-------|
| <b>Urbanization</b>            | Urban                          | <b>Wind Speed (m/s)</b>        | 3.5   | <b>Precipitation Freq (Days)</b> | 58    |
| <b>Climate Zone</b>            | 6                              |                                |       | <b>Operational Year</b>          | 2021  |
| <b>Utility Company</b>         | Pacific Gas & Electric Company |                                |       |                                  |       |
| <b>CO2 Intensity (lb/MWhr)</b> | 290                            | <b>CH4 Intensity (lb/MWhr)</b> | 0.029 | <b>N2O Intensity (lb/MWhr)</b>   | 0.006 |

**1.3 User Entered Comments & Non-Default Data**

Project Characteristics - PG&E 2020 CO2 Intensity Factor

Land Use - Lot acreage updated to match that of the project.

Construction Phase - Building, paving and coating assumed to occur at the same time.

## Mack Road Storage Facility- Modified Project - Sacramento County, Annual

| Table Name                | Column Name        | Default Value | New Value |
|---------------------------|--------------------|---------------|-----------|
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblConstructionPhase      | PhaseEndDate       | 5/5/2020      | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 3/10/2020     | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 4/23/2019     | 7/12/2019 |
| tblConstructionPhase      | PhaseEndDate       | 4/7/2020      | 5/29/2020 |
| tblConstructionPhase      | PhaseEndDate       | 3/26/2019     | 6/14/2019 |
| tblConstructionPhase      | PhaseStartDate     | 4/8/2020      | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 4/24/2019     | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/27/2019     | 6/15/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/11/2020     | 7/13/2019 |
| tblConstructionPhase      | PhaseStartDate     | 3/13/2019     | 6/1/2019  |
| tblLandUse                | LotAcreage         | 2.07          | 5.00      |
| tblLandUse                | LotAcreage         | 0.90          | 2.52      |
| tblProjectCharacteristics | CO2IntensityFactor | 641.35        | 290       |

## 2.0 Emissions Summary

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Mack Road Storage Facility- Modified Project - Sacramento County, Annual

| Quarter | Start Date | End Date   | Maximum Unmitigated ROG + NOX (tons/quarter) | Maximum Mitigated ROG + NOX (tons/quarter) |
|---------|------------|------------|--|--|
| 1       | 3-13-2019  | 6-12-2019  | 0.2145                                       | 0.2145                                     |
| 2       | 6-13-2019  | 9-12-2019  | 1.4326                                       | 1.4326                                     |
| 3       | 9-13-2019  | 12-12-2019 | 1.5969                                       | 1.5969                                     |
| 4       | 12-13-2019 | 3-12-2020  | 1.4976                                       | 1.4976                                     |
| 5       | 3-13-2020  | 6-12-2020  | 1.2597                                       | 1.2597                                     |
|         |            | Highest    | 1.5969                                       | 1.5969                                     |

2.2 Overall Operational

Unmitigated Operational

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2       | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr          |                 |                 |               |               |                 |
| Area         | 0.3966        | 2.0000e-005   | 2.4300e-003   | 0.0000             |               | 1.0000e-005   | 1.0000e-005   | 1.0000e-005    | 1.0000e-005   | 1.0000e-005   | 0.0000         | 4.7200e-003     | 4.7200e-003     | 1.0000e-005   | 0.0000        | 5.0300e-003     |
| Energy       | 6.3600e-003   | 0.0578        | 0.0486        | 3.5000e-004        |               | 4.3900e-003   | 4.3900e-003   |                | 4.3900e-003   | 4.3900e-003   | 0.0000         | 235.7092        | 235.7092        | 0.0185        | 4.7300e-003   | 237.5804        |
| Mobile       | 0.2214        | 0.9088        | 2.4011        | 6.9800e-003        | 0.5810        | 6.3800e-003   | 0.5873        | 0.1558         | 5.9800e-003   | 0.1618        | 0.0000         | 641.3708        | 641.3708        | 0.0326        | 0.0000        | 642.1864        |
| Waste        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 16.9903        | 0.0000          | 16.9903         | 1.0041        | 0.0000        | 42.0929         |
| Water        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 5.6594         | 14.9979         | 20.6573         | 0.0210        | 0.0126        | 24.9418         |
| <b>Total</b> | <b>0.6243</b> | <b>0.9666</b> | <b>2.4520</b> | <b>7.3300e-003</b> | <b>0.5810</b> | <b>0.0108</b> | <b>0.5917</b> | <b>0.1558</b>  | <b>0.0104</b> | <b>0.1662</b> | <b>22.6498</b> | <b>892.0826</b> | <b>914.7324</b> | <b>1.0762</b> | <b>0.0174</b> | <b>946.8066</b> |

Mack Road Storage Facility- Modified Project - Sacramento County, Annual

**2.2 Overall Operational**

**Mitigated Operational**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2       | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|----------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr          |                 |                 |               |               |                 |
| Area         | 0.3966        | 2.0000e-005   | 2.4300e-003   | 0.0000             |               | 1.0000e-005   | 1.0000e-005   |                | 1.0000e-005   | 1.0000e-005   | 0.0000         | 4.7200e-003     | 4.7200e-003     | 1.0000e-005   | 0.0000        | 5.0300e-003     |
| Energy       | 6.3600e-003   | 0.0578        | 0.0486        | 3.5000e-004        |               | 4.3900e-003   | 4.3900e-003   |                | 4.3900e-003   | 4.3900e-003   | 0.0000         | 235.7092        | 235.7092        | 0.0185        | 4.7300e-003   | 237.5804        |
| Mobile       | 0.2214        | 0.9088        | 2.4011        | 6.9800e-003        | 0.5810        | 6.3800e-003   | 0.5873        | 0.1558         | 5.9800e-003   | 0.1618        | 0.0000         | 641.3708        | 641.3708        | 0.0326        | 0.0000        | 642.1864        |
| Waste        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 16.9903        | 0.0000          | 16.9903         | 1.0041        | 0.0000        | 42.0929         |
| Water        |               |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 5.6594         | 14.9979         | 20.6573         | 0.0210        | 0.0126        | 24.9418         |
| <b>Total</b> | <b>0.6243</b> | <b>0.9666</b> | <b>2.4520</b> | <b>7.3300e-003</b> | <b>0.5810</b> | <b>0.0108</b> | <b>0.5917</b> | <b>0.1558</b>  | <b>0.0104</b> | <b>0.1662</b> | <b>22.6498</b> | <b>892.0826</b> | <b>914.7324</b> | <b>1.0762</b> | <b>0.0174</b> | <b>946.8066</b> |

|                   | ROG  | NOx  | CO   | SO2  | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4  | N2O  | CO2e |
|-------------------|------|------|------|------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|------|------|------|
| Percent Reduction | 0.00 | 0.00 | 0.00 | 0.00 | 0.00          | 0.00         | 0.00       | 0.00           | 0.00          | 0.00        | 0.00     | 0.00      | 0.00      | 0.00 | 0.00 | 0.00 |

**3.0 Construction Detail**

**Construction Phase**



Mack Road Storage Facility- Modified Project - Sacramento County, Annual

| Phase Number | Phase Name            | Phase Type            | Start Date | End Date  | Num Days Week | Num Days | Phase Description |
|--------------|-----------------------|-----------------------|------------|-----------|---------------|----------|-------------------|
| 1            | Site Preparation      | Site Preparation      | 6/1/2019   | 6/14/2019 | 5             | 10       |                   |
| 2            | Grading               | Grading               | 6/15/2019  | 7/12/2019 | 5             | 20       |                   |
| 3            | Building Construction | Building Construction | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |
| 4            | Paving                | Paving                | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |
| 5            | Architectural Coating | Architectural Coating | 7/13/2019  | 5/29/2020 | 5             | 230      |                   |

**Acres of Grading (Site Preparation Phase): 0**

**Acres of Grading (Grading Phase): 10**

**Acres of Paving: 2.52**

**Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 135,000; Non-Residential Outdoor: 45,000; Striped Parking Area: 2,400 (Architectural Coating – sqft)**

**OffRoad Equipment**

Mack Road Storage Facility- Modified Project - Sacramento County, Annual

| Phase Name            | Offroad Equipment Type    | Amount | Usage Hours | Horse Power | Load Factor |
|-----------------------|---------------------------|--------|-------------|-------------|-------------|
| Site Preparation      | Rubber Tired Dozers       | 3      | 8.00        | 247         | 0.40        |
| Site Preparation      | Tractors/Loaders/Backhoes | 4      | 8.00        | 97          | 0.37        |
| Grading               | Excavators                | 1      | 8.00        | 158         | 0.38        |
| Grading               | Graders                   | 1      | 8.00        | 187         | 0.41        |
| Grading               | Rubber Tired Dozers       | 1      | 8.00        | 247         | 0.40        |
| Grading               | Tractors/Loaders/Backhoes | 3      | 8.00        | 97          | 0.37        |
| Building Construction | Cranes                    | 1      | 7.00        | 231         | 0.29        |
| Building Construction | Forklifts                 | 3      | 8.00        | 89          | 0.20        |
| Building Construction | Generator Sets            | 1      | 8.00        | 84          | 0.74        |
| Building Construction | Tractors/Loaders/Backhoes | 3      | 7.00        | 97          | 0.37        |
| Building Construction | Welders                   | 1      | 8.00        | 46          | 0.45        |
| Paving                | Pavers                    | 2      | 8.00        | 130         | 0.42        |
| Paving                | Paving Equipment          | 2      | 8.00        | 132         | 0.36        |
| Paving                | Rollers                   | 2      | 8.00        | 80          | 0.38        |
| Architectural Coating | Air Compressors           | 1      | 6.00        | 78          | 0.48        |

**Trips and VMT**

| Phase Name            | Offroad Equipment Count | Worker Trip Number | Vendor Trip Number | Hauling Trip Number | Worker Trip Length | Vendor Trip Length | Hauling Trip Length | Worker Vehicle Class | Vendor Vehicle Class | Hauling Vehicle Class |
|-----------------------|-------------------------|--------------------|--------------------|---------------------|--------------------|--------------------|---------------------|----------------------|----------------------|-----------------------|
| Site Preparation      | 7                       | 18.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Grading               | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Building Construction | 9                       | 46.00              | 21.00              | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Paving                | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Architectural Coating | 1                       | 9.00               | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |

**3.1 Mitigation Measures Construction**

Mack Road Storage Facility- Modified Project - Sacramento County, Annual

**3.2 Site Preparation - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0903        | 0.0000        | 0.0903        | 0.0497         | 0.0000        | 0.0497        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0217        | 0.2279        | 0.1103        | 1.9000e-004        |               | 0.0120        | 0.0120        |                | 0.0110        | 0.0110        | 0.0000        | 17.0843        | 17.0843        | 5.4100e-003        | 0.0000        | 17.2195        |
| <b>Total</b>  | <b>0.0217</b> | <b>0.2279</b> | <b>0.1103</b> | <b>1.9000e-004</b> | <b>0.0903</b> | <b>0.0120</b> | <b>0.1023</b> | <b>0.0497</b>  | <b>0.0110</b> | <b>0.0607</b> | <b>0.0000</b> | <b>17.0843</b> | <b>17.0843</b> | <b>5.4100e-003</b> | <b>0.0000</b> | <b>17.2195</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.6000e-004        | 2.6000e-004        | 2.7600e-003        | 1.0000e-005        | 6.6000e-004        | 0.0000        | 6.7000e-004        | 1.8000e-004        | 0.0000        | 1.8000e-004        | 0.0000        | 0.6042        | 0.6042        | 2.0000e-005        | 0.0000        | 0.6047        |
| <b>Total</b> | <b>3.6000e-004</b> | <b>2.6000e-004</b> | <b>2.7600e-003</b> | <b>1.0000e-005</b> | <b>6.6000e-004</b> | <b>0.0000</b> | <b>6.7000e-004</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>0.6042</b> | <b>0.6042</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.6047</b> |

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**3.2 Site Preparation - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0903        | 0.0000        | 0.0903        | 0.0497         | 0.0000        | 0.0497        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0217        | 0.2279        | 0.1103        | 1.9000e-004        |               | 0.0120        | 0.0120        |                | 0.0110        | 0.0110        | 0.0000        | 17.0843        | 17.0843        | 5.4100e-003        | 0.0000        | 17.2195        |
| <b>Total</b>  | <b>0.0217</b> | <b>0.2279</b> | <b>0.1103</b> | <b>1.9000e-004</b> | <b>0.0903</b> | <b>0.0120</b> | <b>0.1023</b> | <b>0.0497</b>  | <b>0.0110</b> | <b>0.0607</b> | <b>0.0000</b> | <b>17.0843</b> | <b>17.0843</b> | <b>5.4100e-003</b> | <b>0.0000</b> | <b>17.2195</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.6000e-004        | 2.6000e-004        | 2.7600e-003        | 1.0000e-005        | 6.6000e-004        | 0.0000        | 6.7000e-004        | 1.8000e-004        | 0.0000        | 1.8000e-004        | 0.0000        | 0.6042        | 0.6042        | 2.0000e-005        | 0.0000        | 0.6047        |
| <b>Total</b> | <b>3.6000e-004</b> | <b>2.6000e-004</b> | <b>2.7600e-003</b> | <b>1.0000e-005</b> | <b>6.6000e-004</b> | <b>0.0000</b> | <b>6.7000e-004</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>0.6042</b> | <b>0.6042</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.6047</b> |

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**3.3 Grading - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0655        | 0.0000        | 0.0655        | 0.0337         | 0.0000        | 0.0337        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0258        | 0.2835        | 0.1629        | 3.0000e-004        |               | 0.0140        | 0.0140        |                | 0.0129        | 0.0129        | 0.0000        | 26.6423        | 26.6423        | 8.4300e-003        | 0.0000        | 26.8530        |
| <b>Total</b>  | <b>0.0258</b> | <b>0.2835</b> | <b>0.1629</b> | <b>3.0000e-004</b> | <b>0.0655</b> | <b>0.0140</b> | <b>0.0795</b> | <b>0.0337</b>  | <b>0.0129</b> | <b>0.0465</b> | <b>0.0000</b> | <b>26.6423</b> | <b>26.6423</b> | <b>8.4300e-003</b> | <b>0.0000</b> | <b>26.8530</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 6.1000e-004        | 4.3000e-004        | 4.6000e-003        | 1.0000e-005        | 1.1000e-003        | 1.0000e-005        | 1.1100e-003        | 2.9000e-004        | 1.0000e-005        | 3.0000e-004        | 0.0000        | 1.0071        | 1.0071        | 3.0000e-005        | 0.0000        | 1.0078        |
| <b>Total</b> | <b>6.1000e-004</b> | <b>4.3000e-004</b> | <b>4.6000e-003</b> | <b>1.0000e-005</b> | <b>1.1000e-003</b> | <b>1.0000e-005</b> | <b>1.1100e-003</b> | <b>2.9000e-004</b> | <b>1.0000e-005</b> | <b>3.0000e-004</b> | <b>0.0000</b> | <b>1.0071</b> | <b>1.0071</b> | <b>3.0000e-005</b> | <b>0.0000</b> | <b>1.0078</b> |

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**3.3 Grading - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0655        | 0.0000        | 0.0655        | 0.0337         | 0.0000        | 0.0337        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0258        | 0.2835        | 0.1629        | 3.0000e-004        |               | 0.0140        | 0.0140        |                | 0.0129        | 0.0129        | 0.0000        | 26.6422        | 26.6422        | 8.4300e-003        | 0.0000        | 26.8530        |
| <b>Total</b>  | <b>0.0258</b> | <b>0.2835</b> | <b>0.1629</b> | <b>3.0000e-004</b> | <b>0.0655</b> | <b>0.0140</b> | <b>0.0795</b> | <b>0.0337</b>  | <b>0.0129</b> | <b>0.0465</b> | <b>0.0000</b> | <b>26.6422</b> | <b>26.6422</b> | <b>8.4300e-003</b> | <b>0.0000</b> | <b>26.8530</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 6.1000e-004        | 4.3000e-004        | 4.6000e-003        | 1.0000e-005        | 1.1000e-003        | 1.0000e-005        | 1.1100e-003        | 2.9000e-004        | 1.0000e-005        | 3.0000e-004        | 0.0000        | 1.0071        | 1.0071        | 3.0000e-005        | 0.0000        | 1.0078        |
| <b>Total</b> | <b>6.1000e-004</b> | <b>4.3000e-004</b> | <b>4.6000e-003</b> | <b>1.0000e-005</b> | <b>1.1000e-003</b> | <b>1.0000e-005</b> | <b>1.1100e-003</b> | <b>2.9000e-004</b> | <b>1.0000e-005</b> | <b>3.0000e-004</b> | <b>0.0000</b> | <b>1.0071</b> | <b>1.0071</b> | <b>3.0000e-005</b> | <b>0.0000</b> | <b>1.0078</b> |

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**3.4 Building Construction - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1440        | 1.2858        | 1.0470        | 1.6400e-003        |               | 0.0787        | 0.0787        |                | 0.0740        | 0.0740        | 0.0000        | 143.4136        | 143.4136        | 0.0349        | 0.0000        | 144.2870        |
| <b>Total</b> | <b>0.1440</b> | <b>1.2858</b> | <b>1.0470</b> | <b>1.6400e-003</b> |               | <b>0.0787</b> | <b>0.0787</b> |                | <b>0.0740</b> | <b>0.0740</b> | <b>0.0000</b> | <b>143.4136</b> | <b>143.4136</b> | <b>0.0349</b> | <b>0.0000</b> | <b>144.2870</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 6.1400e-003   | 0.1571        | 0.0480        | 3.2000e-004        | 7.4900e-003   | 1.1200e-003        | 8.6100e-003   | 2.1700e-003        | 1.0700e-003        | 3.2400e-003        | 0.0000        | 30.4994        | 30.4994        | 1.9100e-003        | 0.0000        | 30.5472        |
| Worker       | 0.0114        | 7.9700e-003   | 0.0861        | 2.1000e-004        | 0.0206        | 1.5000e-004        | 0.0208        | 5.4800e-003        | 1.4000e-004        | 5.6200e-003        | 0.0000        | 18.8388        | 18.8388        | 5.9000e-004        | 0.0000        | 18.8534        |
| <b>Total</b> | <b>0.0175</b> | <b>0.1651</b> | <b>0.1341</b> | <b>5.3000e-004</b> | <b>0.0281</b> | <b>1.2700e-003</b> | <b>0.0294</b> | <b>7.6500e-003</b> | <b>1.2100e-003</b> | <b>8.8600e-003</b> | <b>0.0000</b> | <b>49.3381</b> | <b>49.3381</b> | <b>2.5000e-003</b> | <b>0.0000</b> | <b>49.4006</b> |

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**3.4 Building Construction - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1440        | 1.2858        | 1.0470        | 1.6400e-003        |               | 0.0787        | 0.0787        |                | 0.0740        | 0.0740        | 0.0000        | 143.4134        | 143.4134        | 0.0349        | 0.0000        | 144.2868        |
| <b>Total</b> | <b>0.1440</b> | <b>1.2858</b> | <b>1.0470</b> | <b>1.6400e-003</b> |               | <b>0.0787</b> | <b>0.0787</b> |                | <b>0.0740</b> | <b>0.0740</b> | <b>0.0000</b> | <b>143.4134</b> | <b>143.4134</b> | <b>0.0349</b> | <b>0.0000</b> | <b>144.2868</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 6.1400e-003   | 0.1571        | 0.0480        | 3.2000e-004        | 7.4900e-003   | 1.1200e-003        | 8.6100e-003   | 2.1700e-003        | 1.0700e-003        | 3.2400e-003        | 0.0000        | 30.4994        | 30.4994        | 1.9100e-003        | 0.0000        | 30.5472        |
| Worker       | 0.0114        | 7.9700e-003   | 0.0861        | 2.1000e-004        | 0.0206        | 1.5000e-004        | 0.0208        | 5.4800e-003        | 1.4000e-004        | 5.6200e-003        | 0.0000        | 18.8388        | 18.8388        | 5.9000e-004        | 0.0000        | 18.8534        |
| <b>Total</b> | <b>0.0175</b> | <b>0.1651</b> | <b>0.1341</b> | <b>5.3000e-004</b> | <b>0.0281</b> | <b>1.2700e-003</b> | <b>0.0294</b> | <b>7.6500e-003</b> | <b>1.2100e-003</b> | <b>8.8600e-003</b> | <b>0.0000</b> | <b>49.3381</b> | <b>49.3381</b> | <b>2.5000e-003</b> | <b>0.0000</b> | <b>49.4006</b> |



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**3.4 Building Construction - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1145        | 1.0361        | 0.9098        | 1.4500e-003        |               | 0.0603        | 0.0603        |                | 0.0567        | 0.0567        | 0.0000        | 125.0694        | 125.0694        | 0.0305        | 0.0000        | 125.8322        |
| <b>Total</b> | <b>0.1145</b> | <b>1.0361</b> | <b>0.9098</b> | <b>1.4500e-003</b> |               | <b>0.0603</b> | <b>0.0603</b> |                | <b>0.0567</b> | <b>0.0567</b> | <b>0.0000</b> | <b>125.0694</b> | <b>125.0694</b> | <b>0.0305</b> | <b>0.0000</b> | <b>125.8322</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 4.3400e-003   | 0.1272        | 0.0355        | 2.8000e-004        | 6.6300e-003   | 6.6000e-004        | 7.2900e-003   | 1.9200e-003        | 6.3000e-004        | 2.5500e-003        | 0.0000        | 26.8320        | 26.8320        | 1.5900e-003        | 0.0000        | 26.8717        |
| Worker       | 9.2500e-003   | 6.2700e-003   | 0.0688        | 1.8000e-004        | 0.0182        | 1.3000e-004        | 0.0184        | 4.8500e-003        | 1.2000e-004        | 4.9700e-003        | 0.0000        | 16.1643        | 16.1643        | 4.6000e-004        | 0.0000        | 16.1757        |
| <b>Total</b> | <b>0.0136</b> | <b>0.1334</b> | <b>0.1042</b> | <b>4.6000e-004</b> | <b>0.0249</b> | <b>7.9000e-004</b> | <b>0.0257</b> | <b>6.7700e-003</b> | <b>7.5000e-004</b> | <b>7.5200e-003</b> | <b>0.0000</b> | <b>42.9962</b> | <b>42.9962</b> | <b>2.0500e-003</b> | <b>0.0000</b> | <b>43.0474</b> |

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**3.4 Building Construction - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1145        | 1.0360        | 0.9098        | 1.4500e-003        |               | 0.0603        | 0.0603        |                | 0.0567        | 0.0567        | 0.0000        | 125.0692        | 125.0692        | 0.0305        | 0.0000        | 125.8321        |
| <b>Total</b> | <b>0.1145</b> | <b>1.0360</b> | <b>0.9098</b> | <b>1.4500e-003</b> |               | <b>0.0603</b> | <b>0.0603</b> |                | <b>0.0567</b> | <b>0.0567</b> | <b>0.0000</b> | <b>125.0692</b> | <b>125.0692</b> | <b>0.0305</b> | <b>0.0000</b> | <b>125.8321</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 4.3400e-003   | 0.1272        | 0.0355        | 2.8000e-004        | 6.6300e-003   | 6.6000e-004        | 7.2900e-003   | 1.9200e-003        | 6.3000e-004        | 2.5500e-003        | 0.0000        | 26.8320        | 26.8320        | 1.5900e-003        | 0.0000        | 26.8717        |
| Worker       | 9.2500e-003   | 6.2700e-003   | 0.0688        | 1.8000e-004        | 0.0182        | 1.3000e-004        | 0.0184        | 4.8500e-003        | 1.2000e-004        | 4.9700e-003        | 0.0000        | 16.1643        | 16.1643        | 4.6000e-004        | 0.0000        | 16.1757        |
| <b>Total</b> | <b>0.0136</b> | <b>0.1334</b> | <b>0.1042</b> | <b>4.6000e-004</b> | <b>0.0249</b> | <b>7.9000e-004</b> | <b>0.0257</b> | <b>6.7700e-003</b> | <b>7.5000e-004</b> | <b>7.5200e-003</b> | <b>0.0000</b> | <b>42.9962</b> | <b>42.9962</b> | <b>2.0500e-003</b> | <b>0.0000</b> | <b>43.0474</b> |

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**3.5 Paving - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0887        | 0.9299        | 0.8946        | 1.3900e-003        |               | 0.0503        | 0.0503        |                | 0.0463        | 0.0463        | 0.0000        | 124.8986        | 124.8986        | 0.0395        | 0.0000        | 125.8865        |
| Paving       | 1.7500e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.0905</b> | <b>0.9299</b> | <b>0.8946</b> | <b>1.3900e-003</b> |               | <b>0.0503</b> | <b>0.0503</b> |                | <b>0.0463</b> | <b>0.0463</b> | <b>0.0000</b> | <b>124.8986</b> | <b>124.8986</b> | <b>0.0395</b> | <b>0.0000</b> | <b>125.8865</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.7000e-003        | 2.6000e-003        | 0.0281        | 7.0000e-005        | 6.7200e-003        | 5.0000e-005        | 6.7700e-003        | 1.7900e-003        | 5.0000e-005        | 1.8300e-003        | 0.0000        | 6.1431        | 6.1431        | 1.9000e-004        | 0.0000        | 6.1479        |
| <b>Total</b> | <b>3.7000e-003</b> | <b>2.6000e-003</b> | <b>0.0281</b> | <b>7.0000e-005</b> | <b>6.7200e-003</b> | <b>5.0000e-005</b> | <b>6.7700e-003</b> | <b>1.7900e-003</b> | <b>5.0000e-005</b> | <b>1.8300e-003</b> | <b>0.0000</b> | <b>6.1431</b> | <b>6.1431</b> | <b>1.9000e-004</b> | <b>0.0000</b> | <b>6.1479</b> |

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**3.5 Paving - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0887        | 0.9299        | 0.8946        | 1.3900e-003        |               | 0.0503        | 0.0503        |                | 0.0463        | 0.0463        | 0.0000        | 124.8985        | 124.8985        | 0.0395        | 0.0000        | 125.8864        |
| Paving       | 1.7500e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.0905</b> | <b>0.9299</b> | <b>0.8946</b> | <b>1.3900e-003</b> |               | <b>0.0503</b> | <b>0.0503</b> |                | <b>0.0463</b> | <b>0.0463</b> | <b>0.0000</b> | <b>124.8985</b> | <b>124.8985</b> | <b>0.0395</b> | <b>0.0000</b> | <b>125.8864</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.7000e-003        | 2.6000e-003        | 0.0281        | 7.0000e-005        | 6.7200e-003        | 5.0000e-005        | 6.7700e-003        | 1.7900e-003        | 5.0000e-005        | 1.8300e-003        | 0.0000        | 6.1431        | 6.1431        | 1.9000e-004        | 0.0000        | 6.1479        |
| <b>Total</b> | <b>3.7000e-003</b> | <b>2.6000e-003</b> | <b>0.0281</b> | <b>7.0000e-005</b> | <b>6.7200e-003</b> | <b>5.0000e-005</b> | <b>6.7700e-003</b> | <b>1.7900e-003</b> | <b>5.0000e-005</b> | <b>1.8300e-003</b> | <b>0.0000</b> | <b>6.1431</b> | <b>6.1431</b> | <b>1.9000e-004</b> | <b>0.0000</b> | <b>6.1479</b> |

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**3.5 Paving - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0733        | 0.7595        | 0.7912        | 1.2300e-003        |               | 0.0407        | 0.0407        |                | 0.0374        | 0.0374        | 0.0000        | 108.1524        | 108.1524        | 0.0350        | 0.0000        | 109.0269        |
| Paving       | 1.5500e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.0748</b> | <b>0.7595</b> | <b>0.7912</b> | <b>1.2300e-003</b> |               | <b>0.0407</b> | <b>0.0407</b> |                | <b>0.0374</b> | <b>0.0374</b> | <b>0.0000</b> | <b>108.1524</b> | <b>108.1524</b> | <b>0.0350</b> | <b>0.0000</b> | <b>109.0269</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.0200e-003        | 2.0400e-003        | 0.0224        | 6.0000e-005        | 5.9500e-003        | 4.0000e-005        | 5.9900e-003        | 1.5800e-003        | 4.0000e-005        | 1.6200e-003        | 0.0000        | 5.2710        | 5.2710        | 1.5000e-004        | 0.0000        | 5.2747        |
| <b>Total</b> | <b>3.0200e-003</b> | <b>2.0400e-003</b> | <b>0.0224</b> | <b>6.0000e-005</b> | <b>5.9500e-003</b> | <b>4.0000e-005</b> | <b>5.9900e-003</b> | <b>1.5800e-003</b> | <b>4.0000e-005</b> | <b>1.6200e-003</b> | <b>0.0000</b> | <b>5.2710</b> | <b>5.2710</b> | <b>1.5000e-004</b> | <b>0.0000</b> | <b>5.2747</b> |

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**3.5 Paving - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0733        | 0.7595        | 0.7912        | 1.2300e-003        |               | 0.0407        | 0.0407        |                | 0.0374        | 0.0374        | 0.0000        | 108.1523        | 108.1523        | 0.0350        | 0.0000        | 109.0267        |
| Paving       | 1.5500e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.0748</b> | <b>0.7595</b> | <b>0.7912</b> | <b>1.2300e-003</b> |               | <b>0.0407</b> | <b>0.0407</b> |                | <b>0.0374</b> | <b>0.0374</b> | <b>0.0000</b> | <b>108.1523</b> | <b>108.1523</b> | <b>0.0350</b> | <b>0.0000</b> | <b>109.0267</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.0200e-003        | 2.0400e-003        | 0.0224        | 6.0000e-005        | 5.9500e-003        | 4.0000e-005        | 5.9900e-003        | 1.5800e-003        | 4.0000e-005        | 1.6200e-003        | 0.0000        | 5.2710        | 5.2710        | 1.5000e-004        | 0.0000        | 5.2747        |
| <b>Total</b> | <b>3.0200e-003</b> | <b>2.0400e-003</b> | <b>0.0224</b> | <b>6.0000e-005</b> | <b>5.9500e-003</b> | <b>4.0000e-005</b> | <b>5.9900e-003</b> | <b>1.5800e-003</b> | <b>4.0000e-005</b> | <b>1.6200e-003</b> | <b>0.0000</b> | <b>5.2710</b> | <b>5.2710</b> | <b>1.5000e-004</b> | <b>0.0000</b> | <b>5.2747</b> |

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**3.6 Architectural Coating - 2019**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.2242        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0163        | 0.1120        | 0.1123        | 1.8000e-004        |               | 7.8500e-003        | 7.8500e-003        |                | 7.8500e-003        | 7.8500e-003        | 0.0000        | 15.5749        | 15.5749        | 1.3200e-003        | 0.0000        | 15.6077        |
| <b>Total</b>    | <b>0.2405</b> | <b>0.1120</b> | <b>0.1123</b> | <b>1.8000e-004</b> |               | <b>7.8500e-003</b> | <b>7.8500e-003</b> |                | <b>7.8500e-003</b> | <b>7.8500e-003</b> | <b>0.0000</b> | <b>15.5749</b> | <b>15.5749</b> | <b>1.3200e-003</b> | <b>0.0000</b> | <b>15.6077</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2200e-003        | 1.5600e-003        | 0.0169        | 4.0000e-005        | 4.0300e-003        | 3.0000e-005        | 4.0600e-003        | 1.0700e-003        | 3.0000e-005        | 1.1000e-003        | 0.0000        | 3.6858        | 3.6858        | 1.1000e-004        | 0.0000        | 3.6887        |
| <b>Total</b> | <b>2.2200e-003</b> | <b>1.5600e-003</b> | <b>0.0169</b> | <b>4.0000e-005</b> | <b>4.0300e-003</b> | <b>3.0000e-005</b> | <b>4.0600e-003</b> | <b>1.0700e-003</b> | <b>3.0000e-005</b> | <b>1.1000e-003</b> | <b>0.0000</b> | <b>3.6858</b> | <b>3.6858</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>3.6887</b> |

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**3.6 Architectural Coating - 2019**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.2242        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0163        | 0.1120        | 0.1123        | 1.8000e-004        |               | 7.8500e-003        | 7.8500e-003        |                | 7.8500e-003        | 7.8500e-003        | 0.0000        | 15.5748        | 15.5748        | 1.3200e-003        | 0.0000        | 15.6077        |
| <b>Total</b>    | <b>0.2405</b> | <b>0.1120</b> | <b>0.1123</b> | <b>1.8000e-004</b> |               | <b>7.8500e-003</b> | <b>7.8500e-003</b> |                | <b>7.8500e-003</b> | <b>7.8500e-003</b> | <b>0.0000</b> | <b>15.5748</b> | <b>15.5748</b> | <b>1.3200e-003</b> | <b>0.0000</b> | <b>15.6077</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.2200e-003        | 1.5600e-003        | 0.0169        | 4.0000e-005        | 4.0300e-003        | 3.0000e-005        | 4.0600e-003        | 1.0700e-003        | 3.0000e-005        | 1.1000e-003        | 0.0000        | 3.6858        | 3.6858        | 1.1000e-004        | 0.0000        | 3.6887        |
| <b>Total</b> | <b>2.2200e-003</b> | <b>1.5600e-003</b> | <b>0.0169</b> | <b>4.0000e-005</b> | <b>4.0300e-003</b> | <b>3.0000e-005</b> | <b>4.0600e-003</b> | <b>1.0700e-003</b> | <b>3.0000e-005</b> | <b>1.1000e-003</b> | <b>0.0000</b> | <b>3.6858</b> | <b>3.6858</b> | <b>1.1000e-004</b> | <b>0.0000</b> | <b>3.6887</b> |



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**3.6 Architectural Coating - 2020**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.1985        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0131        | 0.0909        | 0.0989        | 1.6000e-004        |               | 5.9900e-003        | 5.9900e-003        |                | 5.9900e-003        | 5.9900e-003        | 0.0000        | 13.7876        | 13.7876        | 1.0700e-003        | 0.0000        | 13.8143        |
| <b>Total</b>    | <b>0.2116</b> | <b>0.0909</b> | <b>0.0989</b> | <b>1.6000e-004</b> |               | <b>5.9900e-003</b> | <b>5.9900e-003</b> |                | <b>5.9900e-003</b> | <b>5.9900e-003</b> | <b>0.0000</b> | <b>13.7876</b> | <b>13.7876</b> | <b>1.0700e-003</b> | <b>0.0000</b> | <b>13.8143</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.8100e-003        | 1.2300e-003        | 0.0135        | 4.0000e-005        | 3.5700e-003        | 3.0000e-005        | 3.6000e-003        | 9.5000e-004        | 2.0000e-005        | 9.7000e-004        | 0.0000        | 3.1626        | 3.1626        | 9.0000e-005        | 0.0000        | 3.1648        |
| <b>Total</b> | <b>1.8100e-003</b> | <b>1.2300e-003</b> | <b>0.0135</b> | <b>4.0000e-005</b> | <b>3.5700e-003</b> | <b>3.0000e-005</b> | <b>3.6000e-003</b> | <b>9.5000e-004</b> | <b>2.0000e-005</b> | <b>9.7000e-004</b> | <b>0.0000</b> | <b>3.1626</b> | <b>3.1626</b> | <b>9.0000e-005</b> | <b>0.0000</b> | <b>3.1648</b> |

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**3.6 Architectural Coating - 2020**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.1985        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0131        | 0.0909        | 0.0989        | 1.6000e-004        |               | 5.9900e-003        | 5.9900e-003        |                | 5.9900e-003        | 5.9900e-003        | 0.0000        | 13.7876        | 13.7876        | 1.0700e-003        | 0.0000        | 13.8142        |
| <b>Total</b>    | <b>0.2116</b> | <b>0.0909</b> | <b>0.0989</b> | <b>1.6000e-004</b> |               | <b>5.9900e-003</b> | <b>5.9900e-003</b> |                | <b>5.9900e-003</b> | <b>5.9900e-003</b> | <b>0.0000</b> | <b>13.7876</b> | <b>13.7876</b> | <b>1.0700e-003</b> | <b>0.0000</b> | <b>13.8142</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.8100e-003        | 1.2300e-003        | 0.0135        | 4.0000e-005        | 3.5700e-003        | 3.0000e-005        | 3.6000e-003        | 9.5000e-004        | 2.0000e-005        | 9.7000e-004        | 0.0000        | 3.1626        | 3.1626        | 9.0000e-005        | 0.0000        | 3.1648        |
| <b>Total</b> | <b>1.8100e-003</b> | <b>1.2300e-003</b> | <b>0.0135</b> | <b>4.0000e-005</b> | <b>3.5700e-003</b> | <b>3.0000e-005</b> | <b>3.6000e-003</b> | <b>9.5000e-004</b> | <b>2.0000e-005</b> | <b>9.7000e-004</b> | <b>0.0000</b> | <b>3.1626</b> | <b>3.1626</b> | <b>9.0000e-005</b> | <b>0.0000</b> | <b>3.1648</b> |

**4.0 Operational Detail - Mobile**

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**4.1 Mitigation Measures Mobile**

|             | ROG     | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4    | N2O    | CO2e     |
|-------------|---------|--------|--------|-------------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|--------|--------|----------|
| Category    | tons/yr |        |        |             |               |              |            |                |               |             | MT/yr    |           |           |        |        |          |
| Mitigated   | 0.2214  | 0.9088 | 2.4011 | 6.9800e-003 | 0.5810        | 6.3800e-003  | 0.5873     | 0.1558         | 5.9800e-003   | 0.1618      | 0.0000   | 641.3708  | 641.3708  | 0.0326 | 0.0000 | 642.1864 |
| Unmitigated | 0.2214  | 0.9088 | 2.4011 | 6.9800e-003 | 0.5810        | 6.3800e-003  | 0.5873     | 0.1558         | 5.9800e-003   | 0.1618      | 0.0000   | 641.3708  | 641.3708  | 0.0326 | 0.0000 | 642.1864 |

**4.2 Trip Summary Information**

| Land Use                | Average Daily Trip Rate |               |              | Unmitigated      | Mitigated        |
|-------------------------|-------------------------|---------------|--------------|------------------|------------------|
|                         | Weekday                 | Saturday      | Sunday       | Annual VMT       | Annual VMT       |
| General Office Building | 992.70                  | 221.40        | 94.50        | 1,557,501        | 1,557,501        |
| Parking Lot             | 0.00                    | 0.00          | 0.00         |                  |                  |
| <b>Total</b>            | <b>992.70</b>           | <b>221.40</b> | <b>94.50</b> | <b>1,557,501</b> | <b>1,557,501</b> |

**4.3 Trip Type Information**

| Land Use                | Miles      |            |             | Trip %     |            |             | Trip Purpose % |          |         |
|-------------------------|------------|------------|-------------|------------|------------|-------------|----------------|----------|---------|
|                         | H-W or C-W | H-S or C-C | H-O or C-NW | H-W or C-W | H-S or C-C | H-O or C-NW | Primary        | Diverted | Pass-by |
| General Office Building | 10.00      | 5.00       | 6.50        | 33.00      | 48.00      | 19.00       | 77             | 19       | 4       |
| Parking Lot             | 10.00      | 5.00       | 6.50        | 0.00       | 0.00       | 0.00        | 0              | 0        | 0       |

**4.4 Fleet Mix**

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| Land Use                | LDA      | LDT1     | LDT2     | MDV      | LHD1     | LHD2     | MHD      | HHD      | OBUS     | UBUS     | MCY      | SBUS     | MH       |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| General Office Building | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |
| Parking Lot             | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |

**5.0 Energy Detail**

Historical Energy Use: N

**5.1 Mitigation Measures Energy**

| Category                | ROG         | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O         | CO2e     |
|-------------------------|-------------|--------|--------|-------------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-------------|----------|
|                         | tons/yr     |        |        |             |               |              |             |                |               |             | MT/yr    |           |           |             |             |          |
| Electricity Mitigated   |             |        |        |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 172.7933  | 172.7933  | 0.0173      | 3.5800e-003 | 174.2906 |
| Electricity Unmitigated |             |        |        |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 172.7933  | 172.7933  | 0.0173      | 3.5800e-003 | 174.2906 |
| NaturalGas Mitigated    | 6.3600e-003 | 0.0578 | 0.0486 | 3.5000e-004 |               | 4.3900e-003  | 4.3900e-003 |                | 4.3900e-003   | 4.3900e-003 | 0.0000   | 62.9159   | 62.9159   | 1.2100e-003 | 1.1500e-003 | 63.2898  |
| NaturalGas Unmitigated  | 6.3600e-003 | 0.0578 | 0.0486 | 3.5000e-004 |               | 4.3900e-003  | 4.3900e-003 |                | 4.3900e-003   | 4.3900e-003 | 0.0000   | 62.9159   | 62.9159   | 1.2100e-003 | 1.1500e-003 | 63.2898  |

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**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

|                         | NaturalGas Use | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|-------------------------|----------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                | kBTU/yr        | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| General Office Building | 1.179e+006     | 6.3600e-003        | 0.0578        | 0.0486        | 3.5000e-004        |               | 4.3900e-003        | 4.3900e-003        |                | 4.3900e-003        | 4.3900e-003        | 0.0000        | 62.9159        | 62.9159        | 1.2100e-003        | 1.1500e-003        | 63.2898        |
| Parking Lot             | 0              | 0.0000             | 0.0000        | 0.0000        | 0.0000             |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000             | 0.0000         |
| <b>Total</b>            |                | <b>6.3600e-003</b> | <b>0.0578</b> | <b>0.0486</b> | <b>3.5000e-004</b> |               | <b>4.3900e-003</b> | <b>4.3900e-003</b> |                | <b>4.3900e-003</b> | <b>4.3900e-003</b> | <b>0.0000</b> | <b>62.9159</b> | <b>62.9159</b> | <b>1.2100e-003</b> | <b>1.1500e-003</b> | <b>63.2898</b> |

**Mitigated**

|                         | NaturalGas Use | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|-------------------------|----------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                | kBTU/yr        | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| General Office Building | 1.179e+006     | 6.3600e-003        | 0.0578        | 0.0486        | 3.5000e-004        |               | 4.3900e-003        | 4.3900e-003        |                | 4.3900e-003        | 4.3900e-003        | 0.0000        | 62.9159        | 62.9159        | 1.2100e-003        | 1.1500e-003        | 63.2898        |
| Parking Lot             | 0              | 0.0000             | 0.0000        | 0.0000        | 0.0000             |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000             | 0.0000         |
| <b>Total</b>            |                | <b>6.3600e-003</b> | <b>0.0578</b> | <b>0.0486</b> | <b>3.5000e-004</b> |               | <b>4.3900e-003</b> | <b>4.3900e-003</b> |                | <b>4.3900e-003</b> | <b>4.3900e-003</b> | <b>0.0000</b> | <b>62.9159</b> | <b>62.9159</b> | <b>1.2100e-003</b> | <b>1.1500e-003</b> | <b>63.2898</b> |

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**5.3 Energy by Land Use - Electricity**

**Unmitigated**

|                         | Electricity Use | Total CO2       | CH4           | N2O                | CO2e            |
|-------------------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Land Use                | kWh/yr          | MT/yr           |               |                    |                 |
| General Office Building | 1.2996e+006     | 170.9517        | 0.0171        | 3.5400e-003        | 172.4331        |
| Parking Lot             | 14000           | 1.8416          | 1.8000e-004   | 4.0000e-005        | 1.8575          |
| <b>Total</b>            |                 | <b>172.7933</b> | <b>0.0173</b> | <b>3.5800e-003</b> | <b>174.2906</b> |

**Mitigated**

|                         | Electricity Use | Total CO2       | CH4           | N2O                | CO2e            |
|-------------------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Land Use                | kWh/yr          | MT/yr           |               |                    |                 |
| General Office Building | 1.2996e+006     | 170.9517        | 0.0171        | 3.5400e-003        | 172.4331        |
| Parking Lot             | 14000           | 1.8416          | 1.8000e-004   | 4.0000e-005        | 1.8575          |
| <b>Total</b>            |                 | <b>172.7933</b> | <b>0.0173</b> | <b>3.5800e-003</b> | <b>174.2906</b> |

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

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|             | ROG     | NOx         | CO          | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2   | Total CO2   | CH4         | N2O    | CO2e        |
|-------------|---------|-------------|-------------|--------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-------------|-------------|-------------|--------|-------------|
| Category    | tons/yr |             |             |        |               |              |             |                |               |             | MT/yr    |             |             |             |        |             |
| Mitigated   | 0.3966  | 2.0000e-005 | 2.4300e-003 | 0.0000 |               | 1.0000e-005  | 1.0000e-005 |                | 1.0000e-005   | 1.0000e-005 | 0.0000   | 4.7200e-003 | 4.7200e-003 | 1.0000e-005 | 0.0000 | 5.0300e-003 |
| Unmitigated | 0.3966  | 2.0000e-005 | 2.4300e-003 | 0.0000 |               | 1.0000e-005  | 1.0000e-005 |                | 1.0000e-005   | 1.0000e-005 | 0.0000   | 4.7200e-003 | 4.7200e-003 | 1.0000e-005 | 0.0000 | 5.0300e-003 |

6.2 Area by SubCategory

Unmitigated

|                       | ROG           | NOx                | CO                 | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2          | Total CO2          | CH4                | N2O           | CO2e               |
|-----------------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|
| SubCategory           | tons/yr       |                    |                    |               |               |                    |                    |                |                    |                    | MT/yr         |                    |                    |                    |               |                    |
| Architectural Coating | 0.0423        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Consumer Products     | 0.3541        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Landscaping           | 2.3000e-004   | 2.0000e-005        | 2.4300e-003        | 0.0000        |               | 1.0000e-005        | 1.0000e-005        |                | 1.0000e-005        | 1.0000e-005        | 0.0000        | 4.7200e-003        | 4.7200e-003        | 1.0000e-005        | 0.0000        | 5.0300e-003        |
| <b>Total</b>          | <b>0.3966</b> | <b>2.0000e-005</b> | <b>2.4300e-003</b> | <b>0.0000</b> |               | <b>1.0000e-005</b> | <b>1.0000e-005</b> |                | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>4.7200e-003</b> | <b>4.7200e-003</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.0300e-003</b> |

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**6.2 Area by SubCategory**

**Mitigated**

|                       | ROG           | NOx                | CO                 | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2          | Total CO2          | CH4                | N2O           | CO2e               |
|-----------------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|
| SubCategory           | tons/yr       |                    |                    |               |               |                    |                    |                |                    |                    | MT/yr         |                    |                    |                    |               |                    |
| Architectural Coating | 0.0423        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Consumer Products     | 0.3541        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Landscaping           | 2.3000e-004   | 2.0000e-005        | 2.4300e-003        | 0.0000        |               | 1.0000e-005        | 1.0000e-005        |                | 1.0000e-005        | 1.0000e-005        | 0.0000        | 4.7200e-003        | 4.7200e-003        | 1.0000e-005        | 0.0000        | 5.0300e-003        |
| <b>Total</b>          | <b>0.3966</b> | <b>2.0000e-005</b> | <b>2.4300e-003</b> | <b>0.0000</b> |               | <b>1.0000e-005</b> | <b>1.0000e-005</b> |                | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>4.7200e-003</b> | <b>4.7200e-003</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.0300e-003</b> |

**7.0 Water Detail**

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**7.1 Mitigation Measures Water**



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|             | Total CO2 | CH4    | N2O    | CO2e    |
|-------------|-----------|--------|--------|---------|
| Category    | MT/yr     |        |        |         |
| Mitigated   | 20.6573   | 0.0210 | 0.0126 | 24.9418 |
| Unmitigated | 20.6573   | 0.0210 | 0.0126 | 24.9418 |

**7.2 Water by Land Use**

**Unmitigated**

|                         | Indoor/Outdoor Use | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|--------------------|----------------|---------------|---------------|----------------|
| Land Use                | Mgal               | MT/yr          |               |               |                |
| General Office Building | 15.996 / 9.80402   | 20.6573        | 0.0210        | 0.0126        | 24.9418        |
| Parking Lot             | 0 / 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                    | <b>20.6573</b> | <b>0.0210</b> | <b>0.0126</b> | <b>24.9418</b> |

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**7.2 Water by Land Use**

**Mitigated**

|                         | Indoor/Outdoor Use | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|--------------------|----------------|---------------|---------------|----------------|
| Land Use                | Mgal               | MT/yr          |               |               |                |
| General Office Building | 15.996 / 9.80402   | 20.6573        | 0.0210        | 0.0126        | 24.9418        |
| Parking Lot             | 0 / 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                    | <b>20.6573</b> | <b>0.0210</b> | <b>0.0126</b> | <b>24.9418</b> |

**8.0 Waste Detail**

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**8.1 Mitigation Measures Waste**

**Category/Year**

|             | Total CO2 | CH4    | N2O    | CO2e    |
|-------------|-----------|--------|--------|---------|
|             | MT/yr     |        |        |         |
| Mitigated   | 16.9903   | 1.0041 | 0.0000 | 42.0929 |
| Unmitigated | 16.9903   | 1.0041 | 0.0000 | 42.0929 |

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**8.2 Waste by Land Use**

**Unmitigated**

|                         | Waste Disposed | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|----------------|----------------|---------------|---------------|----------------|
| Land Use                | tons           | MT/yr          |               |               |                |
| General Office Building | 83.7           | 16.9903        | 1.0041        | 0.0000        | 42.0929        |
| Parking Lot             | 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                | <b>16.9903</b> | <b>1.0041</b> | <b>0.0000</b> | <b>42.0929</b> |

**Mitigated**

|                         | Waste Disposed | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|----------------|----------------|---------------|---------------|----------------|
| Land Use                | tons           | MT/yr          |               |               |                |
| General Office Building | 83.7           | 16.9903        | 1.0041        | 0.0000        | 42.0929        |
| Parking Lot             | 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                | <b>16.9903</b> | <b>1.0041</b> | <b>0.0000</b> | <b>42.0929</b> |

**9.0 Operational Offroad**

| Equipment Type | Number | Hours/Day | Days/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|-----------|-------------|-------------|-----------|
|----------------|--------|-----------|-----------|-------------|-------------|-----------|

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**10.0 Stationary Equipment**

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**Fire Pumps and Emergency Generators**

| Equipment Type | Number | Hours/Day | Hours/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|------------|-------------|-------------|-----------|
|----------------|--------|-----------|------------|-------------|-------------|-----------|

**Boilers**

| Equipment Type | Number | Heat Input/Day | Heat Input/Year | Boiler Rating | Fuel Type |
|----------------|--------|----------------|-----------------|---------------|-----------|
|----------------|--------|----------------|-----------------|---------------|-----------|

**User Defined Equipment**

| Equipment Type | Number |
|----------------|--------|
|----------------|--------|

**11.0 Vegetation**

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Mack Road Storage Facility- Original Project - Sacramento County, Annual

**Mack Road Storage Facility- Original Project  
Sacramento County, Annual**

**1.0 Project Characteristics**

**1.1 Land Usage**

| Land Uses               | Size   | Metric   | Lot Acreage | Floor Surface Area | Population |
|-------------------------|--------|----------|-------------|--------------------|------------|
| General Office Building | 79.92  | 1000sqft | 4.52        | 79,919.00          | 0          |
| Parking Lot             | 137.00 | Space    | 3.00        | 54,800.00          | 0          |

**1.2 Other Project Characteristics**

|                                |                                |                                |       |                                  |       |
|--------------------------------|--------------------------------|--------------------------------|-------|----------------------------------|-------|
| <b>Urbanization</b>            | Urban                          | <b>Wind Speed (m/s)</b>        | 3.5   | <b>Precipitation Freq (Days)</b> | 58    |
| <b>Climate Zone</b>            | 6                              |                                |       | <b>Operational Year</b>          | 2021  |
| <b>Utility Company</b>         | Pacific Gas & Electric Company |                                |       |                                  |       |
| <b>CO2 Intensity (lb/MWhr)</b> | 290                            | <b>CH4 Intensity (lb/MWhr)</b> | 0.029 | <b>N2O Intensity (lb/MWhr)</b>   | 0.006 |

**1.3 User Entered Comments & Non-Default Data**

Project Characteristics - PG&E 2020 C02 Intensity Factor

Land Use - Lot acreage update to match that of the project.

Construction Phase - Building, paving and coating assumed to occur at the same time.

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| Table Name                | Column Name        | Default Value | New Value |
|---------------------------|--------------------|---------------|-----------|
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblConstructionPhase      | NumDays            | 20.00         | 230.00    |
| tblLandUse                | LandUseSquareFeet  | 79,920.00     | 79,919.00 |
| tblLandUse                | LotAcreage         | 1.83          | 4.52      |
| tblLandUse                | LotAcreage         | 1.23          | 3.00      |
| tblProjectCharacteristics | CO2IntensityFactor | 641.35        | 290       |

## 2.0 Emissions Summary

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| Quarter | Start Date | End Date   | Maximum Unmitigated ROG + NOX (tons/quarter) | Maximum Mitigated ROG + NOX (tons/quarter) |
|---------|------------|------------|--|--|
| 1       | 3-13-2019  | 6-12-2019  | 0.4719                                       | 0.4719                                     |
| 2       | 6-13-2019  | 9-12-2019  | 1.5538                                       | 1.5538                                     |
| 3       | 9-13-2019  | 12-12-2019 | 1.5895                                       | 1.5895                                     |
| 4       | 12-13-2019 | 3-12-2020  | 1.4899                                       | 1.4899                                     |
| 5       | 3-13-2020  | 6-12-2020  | 0.8997                                       | 0.8997                                     |
|         |            | Highest    | 1.5895                                       | 1.5895                                     |

2.2 Overall Operational

Unmitigated Operational

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2       | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                |                    |               | MT/yr          |                 |                 |               |               |                 |
| Area         | 0.3537        | 3.0000e-005   | 2.7800e-003   | 0.0000             |               | 1.0000e-005        | 1.0000e-005   |                | 1.0000e-005        | 1.0000e-005   | 0.0000         | 5.3800e-003     | 5.3800e-003     | 1.0000e-005   | 0.0000        | 5.7400e-003     |
| Energy       | 5.6500e-003   | 0.0513        | 0.0431        | 3.1000e-004        |               | 3.9000e-003        | 3.9000e-003   |                | 3.9000e-003        | 3.9000e-003   | 0.0000         | 210.1948        | 210.1948        | 0.0165        | 4.2200e-003   | 211.8642        |
| Mobile       | 0.1966        | 0.8070        | 2.1321        | 6.2000e-003        | 0.5159        | 5.6700e-003        | 0.5216        | 0.1383         | 5.3100e-003        | 0.1436        | 0.0000         | 569.5373        | 569.5373        | 0.0290        | 0.0000        | 570.2616        |
| Waste        |               |               |               |                    |               | 0.0000             | 0.0000        |                | 0.0000             | 0.0000        | 15.0883        | 0.0000          | 15.0883         | 0.8917        | 0.0000        | 37.3807         |
| Water        |               |               |               |                    |               | 0.0000             | 0.0000        |                | 0.0000             | 0.0000        | 5.0256         | 13.3181         | 18.3437         | 0.0186        | 0.0112        | 22.1483         |
| <b>Total</b> | <b>0.5560</b> | <b>0.8583</b> | <b>2.1780</b> | <b>6.5100e-003</b> | <b>0.5159</b> | <b>9.5800e-003</b> | <b>0.5255</b> | <b>0.1383</b>  | <b>9.2200e-003</b> | <b>0.1476</b> | <b>20.1139</b> | <b>793.0556</b> | <b>813.1695</b> | <b>0.9558</b> | <b>0.0154</b> | <b>841.6605</b> |



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**2.2 Overall Operational**

**Mitigated Operational**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2       | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|----------------|--------------------|---------------|----------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                |                    |               | MT/yr          |                 |                 |               |               |                 |
| Area         | 0.3537        | 3.0000e-005   | 2.7800e-003   | 0.0000             |               | 1.0000e-005        | 1.0000e-005   |                | 1.0000e-005        | 1.0000e-005   | 0.0000         | 5.3800e-003     | 5.3800e-003     | 1.0000e-005   | 0.0000        | 5.7400e-003     |
| Energy       | 5.6500e-003   | 0.0513        | 0.0431        | 3.1000e-004        |               | 3.9000e-003        | 3.9000e-003   |                | 3.9000e-003        | 3.9000e-003   | 0.0000         | 210.1948        | 210.1948        | 0.0165        | 4.2200e-003   | 211.8642        |
| Mobile       | 0.1966        | 0.8070        | 2.1321        | 6.2000e-003        | 0.5159        | 5.6700e-003        | 0.5216        | 0.1383         | 5.3100e-003        | 0.1436        | 0.0000         | 569.5373        | 569.5373        | 0.0290        | 0.0000        | 570.2616        |
| Waste        |               |               |               |                    |               | 0.0000             | 0.0000        |                | 0.0000             | 0.0000        | 15.0883        | 0.0000          | 15.0883         | 0.8917        | 0.0000        | 37.3807         |
| Water        |               |               |               |                    |               | 0.0000             | 0.0000        |                | 0.0000             | 0.0000        | 5.0256         | 13.3181         | 18.3437         | 0.0186        | 0.0112        | 22.1483         |
| <b>Total</b> | <b>0.5560</b> | <b>0.8583</b> | <b>2.1780</b> | <b>6.5100e-003</b> | <b>0.5159</b> | <b>9.5800e-003</b> | <b>0.5255</b> | <b>0.1383</b>  | <b>9.2200e-003</b> | <b>0.1476</b> | <b>20.1139</b> | <b>793.0556</b> | <b>813.1695</b> | <b>0.9558</b> | <b>0.0154</b> | <b>841.6605</b> |

|                   | ROG  | NOx  | CO   | SO2  | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4  | N2O  | CO2e |
|-------------------|------|------|------|------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|------|------|------|
| Percent Reduction | 0.00 | 0.00 | 0.00 | 0.00 | 0.00          | 0.00         | 0.00       | 0.00           | 0.00          | 0.00        | 0.00     | 0.00      | 0.00      | 0.00 | 0.00 | 0.00 |

**3.0 Construction Detail**

**Construction Phase**

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| Phase Number | Phase Name            | Phase Type            | Start Date | End Date  | Num Days Week | Num Days | Phase Description |
|--------------|-----------------------|-----------------------|------------|-----------|---------------|----------|-------------------|
| 1            | Site Preparation      | Site Preparation      | 5/10/2019  | 5/23/2019 | 5             | 10       |                   |
| 2            | Grading               | Grading               | 5/24/2019  | 6/20/2019 | 5             | 20       |                   |
| 3            | Building Construction | Building Construction | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |
| 4            | Paving                | Paving                | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |
| 5            | Architectural Coating | Architectural Coating | 6/21/2019  | 5/7/2020  | 5             | 230      |                   |

**Acres of Grading (Site Preparation Phase): 0**

**Acres of Grading (Grading Phase): 10**

**Acres of Paving: 3**

**Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 119,879; Non-Residential Outdoor: 39,960; Striped Parking Area: 3,288 (Architectural Coating – sqft)**

**OffRoad Equipment**

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| Phase Name            | Offroad Equipment Type    | Amount | Usage Hours | Horse Power | Load Factor |
|-----------------------|---------------------------|--------|-------------|-------------|-------------|
| Site Preparation      | Rubber Tired Dozers       | 3      | 8.00        | 247         | 0.40        |
| Site Preparation      | Tractors/Loaders/Backhoes | 4      | 8.00        | 97          | 0.37        |
| Grading               | Excavators                | 1      | 8.00        | 158         | 0.38        |
| Grading               | Graders                   | 1      | 8.00        | 187         | 0.41        |
| Grading               | Rubber Tired Dozers       | 1      | 8.00        | 247         | 0.40        |
| Grading               | Tractors/Loaders/Backhoes | 3      | 8.00        | 97          | 0.37        |
| Building Construction | Cranes                    | 1      | 7.00        | 231         | 0.29        |
| Building Construction | Forklifts                 | 3      | 8.00        | 89          | 0.20        |
| Building Construction | Generator Sets            | 1      | 8.00        | 84          | 0.74        |
| Building Construction | Tractors/Loaders/Backhoes | 3      | 7.00        | 97          | 0.37        |
| Building Construction | Welders                   | 1      | 8.00        | 46          | 0.45        |
| Paving                | Pavers                    | 2      | 8.00        | 130         | 0.42        |
| Paving                | Paving Equipment          | 2      | 8.00        | 132         | 0.36        |
| Paving                | Rollers                   | 2      | 8.00        | 80          | 0.38        |
| Architectural Coating | Air Compressors           | 1      | 6.00        | 78          | 0.48        |

**Trips and VMT**

| Phase Name            | Offroad Equipment Count | Worker Trip Number | Vendor Trip Number | Hauling Trip Number | Worker Trip Length | Vendor Trip Length | Hauling Trip Length | Worker Vehicle Class | Vendor Vehicle Class | Hauling Vehicle Class |
|-----------------------|-------------------------|--------------------|--------------------|---------------------|--------------------|--------------------|---------------------|----------------------|----------------------|-----------------------|
| Site Preparation      | 7                       | 18.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Grading               | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Building Construction | 9                       | 49.00              | 22.00              | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Paving                | 6                       | 15.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |
| Architectural Coating | 1                       | 10.00              | 0.00               | 0.00                | 10.00              | 6.50               | 20.00               | LD_Mix               | HDT_Mix              | HHDT                  |

**3.1 Mitigation Measures Construction**

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**3.2 Site Preparation - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0903        | 0.0000        | 0.0903        | 0.0497         | 0.0000        | 0.0497        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0217        | 0.2279        | 0.1103        | 1.9000e-004        |               | 0.0120        | 0.0120        |                | 0.0110        | 0.0110        | 0.0000        | 17.0843        | 17.0843        | 5.4100e-003        | 0.0000        | 17.2195        |
| <b>Total</b>  | <b>0.0217</b> | <b>0.2279</b> | <b>0.1103</b> | <b>1.9000e-004</b> | <b>0.0903</b> | <b>0.0120</b> | <b>0.1023</b> | <b>0.0497</b>  | <b>0.0110</b> | <b>0.0607</b> | <b>0.0000</b> | <b>17.0843</b> | <b>17.0843</b> | <b>5.4100e-003</b> | <b>0.0000</b> | <b>17.2195</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.6000e-004        | 2.6000e-004        | 2.7600e-003        | 1.0000e-005        | 6.6000e-004        | 0.0000        | 6.7000e-004        | 1.8000e-004        | 0.0000        | 1.8000e-004        | 0.0000        | 0.6042        | 0.6042        | 2.0000e-005        | 0.0000        | 0.6047        |
| <b>Total</b> | <b>3.6000e-004</b> | <b>2.6000e-004</b> | <b>2.7600e-003</b> | <b>1.0000e-005</b> | <b>6.6000e-004</b> | <b>0.0000</b> | <b>6.7000e-004</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>0.6042</b> | <b>0.6042</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.6047</b> |

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**3.2 Site Preparation - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0903        | 0.0000        | 0.0903        | 0.0497         | 0.0000        | 0.0497        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0217        | 0.2279        | 0.1103        | 1.9000e-004        |               | 0.0120        | 0.0120        |                | 0.0110        | 0.0110        | 0.0000        | 17.0843        | 17.0843        | 5.4100e-003        | 0.0000        | 17.2195        |
| <b>Total</b>  | <b>0.0217</b> | <b>0.2279</b> | <b>0.1103</b> | <b>1.9000e-004</b> | <b>0.0903</b> | <b>0.0120</b> | <b>0.1023</b> | <b>0.0497</b>  | <b>0.0110</b> | <b>0.0607</b> | <b>0.0000</b> | <b>17.0843</b> | <b>17.0843</b> | <b>5.4100e-003</b> | <b>0.0000</b> | <b>17.2195</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10  | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5 | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|--------------------|--------------------|---------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |               |                    |                    |               |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 3.6000e-004        | 2.6000e-004        | 2.7600e-003        | 1.0000e-005        | 6.6000e-004        | 0.0000        | 6.7000e-004        | 1.8000e-004        | 0.0000        | 1.8000e-004        | 0.0000        | 0.6042        | 0.6042        | 2.0000e-005        | 0.0000        | 0.6047        |
| <b>Total</b> | <b>3.6000e-004</b> | <b>2.6000e-004</b> | <b>2.7600e-003</b> | <b>1.0000e-005</b> | <b>6.6000e-004</b> | <b>0.0000</b> | <b>6.7000e-004</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>1.8000e-004</b> | <b>0.0000</b> | <b>0.6042</b> | <b>0.6042</b> | <b>2.0000e-005</b> | <b>0.0000</b> | <b>0.6047</b> |

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**3.3 Grading - 2019**

**Unmitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0655        | 0.0000        | 0.0655        | 0.0337         | 0.0000        | 0.0337        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0258        | 0.2835        | 0.1629        | 3.0000e-004        |               | 0.0140        | 0.0140        |                | 0.0129        | 0.0129        | 0.0000        | 26.6423        | 26.6423        | 8.4300e-003        | 0.0000        | 26.8530        |
| <b>Total</b>  | <b>0.0258</b> | <b>0.2835</b> | <b>0.1629</b> | <b>3.0000e-004</b> | <b>0.0655</b> | <b>0.0140</b> | <b>0.0795</b> | <b>0.0337</b>  | <b>0.0129</b> | <b>0.0465</b> | <b>0.0000</b> | <b>26.6423</b> | <b>26.6423</b> | <b>8.4300e-003</b> | <b>0.0000</b> | <b>26.8530</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 6.1000e-004        | 4.3000e-004        | 4.6000e-003        | 1.0000e-005        | 1.1000e-003        | 1.0000e-005        | 1.1100e-003        | 2.9000e-004        | 1.0000e-005        | 3.0000e-004        | 0.0000        | 1.0071        | 1.0071        | 3.0000e-005        | 0.0000        | 1.0078        |
| <b>Total</b> | <b>6.1000e-004</b> | <b>4.3000e-004</b> | <b>4.6000e-003</b> | <b>1.0000e-005</b> | <b>1.1000e-003</b> | <b>1.0000e-005</b> | <b>1.1100e-003</b> | <b>2.9000e-004</b> | <b>1.0000e-005</b> | <b>3.0000e-004</b> | <b>0.0000</b> | <b>1.0071</b> | <b>1.0071</b> | <b>3.0000e-005</b> | <b>0.0000</b> | <b>1.0078</b> |

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**3.3 Grading - 2019**

**Mitigated Construction On-Site**

|               | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|---------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category      | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |                    |               |                |
| Fugitive Dust |               |               |               |                    | 0.0655        | 0.0000        | 0.0655        | 0.0337         | 0.0000        | 0.0337        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road      | 0.0258        | 0.2835        | 0.1629        | 3.0000e-004        |               | 0.0140        | 0.0140        |                | 0.0129        | 0.0129        | 0.0000        | 26.6422        | 26.6422        | 8.4300e-003        | 0.0000        | 26.8530        |
| <b>Total</b>  | <b>0.0258</b> | <b>0.2835</b> | <b>0.1629</b> | <b>3.0000e-004</b> | <b>0.0655</b> | <b>0.0140</b> | <b>0.0795</b> | <b>0.0337</b>  | <b>0.0129</b> | <b>0.0465</b> | <b>0.0000</b> | <b>26.6422</b> | <b>26.6422</b> | <b>8.4300e-003</b> | <b>0.0000</b> | <b>26.8530</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO                 | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |                    |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 6.1000e-004        | 4.3000e-004        | 4.6000e-003        | 1.0000e-005        | 1.1000e-003        | 1.0000e-005        | 1.1100e-003        | 2.9000e-004        | 1.0000e-005        | 3.0000e-004        | 0.0000        | 1.0071        | 1.0071        | 3.0000e-005        | 0.0000        | 1.0078        |
| <b>Total</b> | <b>6.1000e-004</b> | <b>4.3000e-004</b> | <b>4.6000e-003</b> | <b>1.0000e-005</b> | <b>1.1000e-003</b> | <b>1.0000e-005</b> | <b>1.1100e-003</b> | <b>2.9000e-004</b> | <b>1.0000e-005</b> | <b>3.0000e-004</b> | <b>0.0000</b> | <b>1.0071</b> | <b>1.0071</b> | <b>3.0000e-005</b> | <b>0.0000</b> | <b>1.0078</b> |

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**3.4 Building Construction - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1629        | 1.4544        | 1.1843        | 1.8600e-003        |               | 0.0890        | 0.0890        |                | 0.0837        | 0.0837        | 0.0000        | 162.2219        | 162.2219        | 0.0395        | 0.0000        | 163.2099        |
| <b>Total</b> | <b>0.1629</b> | <b>1.4544</b> | <b>1.1843</b> | <b>1.8600e-003</b> |               | <b>0.0890</b> | <b>0.0890</b> |                | <b>0.0837</b> | <b>0.0837</b> | <b>0.0000</b> | <b>162.2219</b> | <b>162.2219</b> | <b>0.0395</b> | <b>0.0000</b> | <b>163.2099</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |               | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 7.2800e-003   | 0.1862        | 0.0569        | 3.8000e-004        | 8.8800e-003   | 1.3300e-003        | 0.0102        | 2.5700e-003        | 1.2700e-003        | 3.8400e-003   | 0.0000        | 36.1421        | 36.1421        | 2.2700e-003        | 0.0000        | 36.1988        |
| Worker       | 0.0137        | 9.6100e-003   | 0.1037        | 2.5000e-004        | 0.0248        | 1.8000e-004        | 0.0250        | 6.6000e-003        | 1.7000e-004        | 6.7700e-003   | 0.0000        | 22.6992        | 22.6992        | 7.1000e-004        | 0.0000        | 22.7168        |
| <b>Total</b> | <b>0.0210</b> | <b>0.1958</b> | <b>0.1607</b> | <b>6.3000e-004</b> | <b>0.0337</b> | <b>1.5100e-003</b> | <b>0.0352</b> | <b>9.1700e-003</b> | <b>1.4400e-003</b> | <b>0.0106</b> | <b>0.0000</b> | <b>58.8413</b> | <b>58.8413</b> | <b>2.9800e-003</b> | <b>0.0000</b> | <b>58.9156</b> |



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**3.4 Building Construction - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1629        | 1.4544        | 1.1843        | 1.8600e-003        |               | 0.0890        | 0.0890        |                | 0.0837        | 0.0837        | 0.0000        | 162.2217        | 162.2217        | 0.0395        | 0.0000        | 163.2097        |
| <b>Total</b> | <b>0.1629</b> | <b>1.4544</b> | <b>1.1843</b> | <b>1.8600e-003</b> |               | <b>0.0890</b> | <b>0.0890</b> |                | <b>0.0837</b> | <b>0.0837</b> | <b>0.0000</b> | <b>162.2217</b> | <b>162.2217</b> | <b>0.0395</b> | <b>0.0000</b> | <b>163.2097</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|---------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |               | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 7.2800e-003   | 0.1862        | 0.0569        | 3.8000e-004        | 8.8800e-003   | 1.3300e-003        | 0.0102        | 2.5700e-003        | 1.2700e-003        | 3.8400e-003   | 0.0000        | 36.1421        | 36.1421        | 2.2700e-003        | 0.0000        | 36.1988        |
| Worker       | 0.0137        | 9.6100e-003   | 0.1037        | 2.5000e-004        | 0.0248        | 1.8000e-004        | 0.0250        | 6.6000e-003        | 1.7000e-004        | 6.7700e-003   | 0.0000        | 22.6992        | 22.6992        | 7.1000e-004        | 0.0000        | 22.7168        |
| <b>Total</b> | <b>0.0210</b> | <b>0.1958</b> | <b>0.1607</b> | <b>6.3000e-004</b> | <b>0.0337</b> | <b>1.5100e-003</b> | <b>0.0352</b> | <b>9.1700e-003</b> | <b>1.4400e-003</b> | <b>0.0106</b> | <b>0.0000</b> | <b>58.8413</b> | <b>58.8413</b> | <b>2.9800e-003</b> | <b>0.0000</b> | <b>58.9156</b> |

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**3.4 Building Construction - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0975        | 0.8826        | 0.7750        | 1.2400e-003        |               | 0.0514        | 0.0514        |                | 0.0483        | 0.0483        | 0.0000        | 106.5406        | 106.5406        | 0.0260        | 0.0000        | 107.1904        |
| <b>Total</b> | <b>0.0975</b> | <b>0.8826</b> | <b>0.7750</b> | <b>1.2400e-003</b> |               | <b>0.0514</b> | <b>0.0514</b> |                | <b>0.0483</b> | <b>0.0483</b> | <b>0.0000</b> | <b>106.5406</b> | <b>106.5406</b> | <b>0.0260</b> | <b>0.0000</b> | <b>107.1904</b> |

**Unmitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 3.8700e-003   | 0.1135        | 0.0317        | 2.5000e-004        | 5.9200e-003   | 5.9000e-004        | 6.5000e-003   | 1.7100e-003        | 5.6000e-004        | 2.2700e-003        | 0.0000        | 23.9453        | 23.9453        | 1.4200e-003        | 0.0000        | 23.9807        |
| Worker       | 8.3900e-003   | 5.6900e-003   | 0.0624        | 1.6000e-004        | 0.0166        | 1.2000e-004        | 0.0167        | 4.4000e-003        | 1.1000e-004        | 4.5100e-003        | 0.0000        | 14.6676        | 14.6676        | 4.1000e-004        | 0.0000        | 14.6780        |
| <b>Total</b> | <b>0.0123</b> | <b>0.1192</b> | <b>0.0940</b> | <b>4.1000e-004</b> | <b>0.0225</b> | <b>7.1000e-004</b> | <b>0.0232</b> | <b>6.1100e-003</b> | <b>6.7000e-004</b> | <b>6.7800e-003</b> | <b>0.0000</b> | <b>38.6129</b> | <b>38.6129</b> | <b>1.8300e-003</b> | <b>0.0000</b> | <b>38.6587</b> |

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**3.4 Building Construction - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.0975        | 0.8826        | 0.7750        | 1.2400e-003        |               | 0.0514        | 0.0514        |                | 0.0483        | 0.0483        | 0.0000        | 106.5405        | 106.5405        | 0.0260        | 0.0000        | 107.1903        |
| <b>Total</b> | <b>0.0975</b> | <b>0.8826</b> | <b>0.7750</b> | <b>1.2400e-003</b> |               | <b>0.0514</b> | <b>0.0514</b> |                | <b>0.0483</b> | <b>0.0483</b> | <b>0.0000</b> | <b>106.5405</b> | <b>106.5405</b> | <b>0.0260</b> | <b>0.0000</b> | <b>107.1903</b> |

**Mitigated Construction Off-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total    | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |                    |               |                    |                    |                    | MT/yr         |                |                |                    |               |                |
| Hauling      | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Vendor       | 3.8700e-003   | 0.1135        | 0.0317        | 2.5000e-004        | 5.9200e-003   | 5.9000e-004        | 6.5000e-003   | 1.7100e-003        | 5.6000e-004        | 2.2700e-003        | 0.0000        | 23.9453        | 23.9453        | 1.4200e-003        | 0.0000        | 23.9807        |
| Worker       | 8.3900e-003   | 5.6900e-003   | 0.0624        | 1.6000e-004        | 0.0166        | 1.2000e-004        | 0.0167        | 4.4000e-003        | 1.1000e-004        | 4.5100e-003        | 0.0000        | 14.6676        | 14.6676        | 4.1000e-004        | 0.0000        | 14.6780        |
| <b>Total</b> | <b>0.0123</b> | <b>0.1192</b> | <b>0.0940</b> | <b>4.1000e-004</b> | <b>0.0225</b> | <b>7.1000e-004</b> | <b>0.0232</b> | <b>6.1100e-003</b> | <b>6.7000e-004</b> | <b>6.7800e-003</b> | <b>0.0000</b> | <b>38.6129</b> | <b>38.6129</b> | <b>1.8300e-003</b> | <b>0.0000</b> | <b>38.6587</b> |

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**3.5 Paving - 2019**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1004        | 1.0518        | 1.0119        | 1.5700e-003        |               | 0.0569        | 0.0569        |                | 0.0523        | 0.0523        | 0.0000        | 141.2788        | 141.2788        | 0.0447        | 0.0000        | 142.3962        |
| Paving       | 2.3600e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.1027</b> | <b>1.0518</b> | <b>1.0119</b> | <b>1.5700e-003</b> |               | <b>0.0569</b> | <b>0.0569</b> |                | <b>0.0523</b> | <b>0.0523</b> | <b>0.0000</b> | <b>141.2788</b> | <b>141.2788</b> | <b>0.0447</b> | <b>0.0000</b> | <b>142.3962</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 4.1900e-003        | 2.9400e-003        | 0.0318        | 8.0000e-005        | 7.6000e-003        | 6.0000e-005        | 7.6600e-003        | 2.0200e-003        | 5.0000e-005        | 2.0700e-003        | 0.0000        | 6.9487        | 6.9487        | 2.2000e-004        | 0.0000        | 6.9541        |
| <b>Total</b> | <b>4.1900e-003</b> | <b>2.9400e-003</b> | <b>0.0318</b> | <b>8.0000e-005</b> | <b>7.6000e-003</b> | <b>6.0000e-005</b> | <b>7.6600e-003</b> | <b>2.0200e-003</b> | <b>5.0000e-005</b> | <b>2.0700e-003</b> | <b>0.0000</b> | <b>6.9487</b> | <b>6.9487</b> | <b>2.2000e-004</b> | <b>0.0000</b> | <b>6.9541</b> |

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**3.5 Paving - 2019**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2       | Total CO2       | CH4           | N2O           | CO2e            |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|-----------------|-----------------|---------------|---------------|-----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                 |                 |               |               |                 |
| Off-Road     | 0.1004        | 1.0518        | 1.0119        | 1.5700e-003        |               | 0.0569        | 0.0569        |                | 0.0523        | 0.0523        | 0.0000        | 141.2786        | 141.2786        | 0.0447        | 0.0000        | 142.3961        |
| Paving       | 2.3600e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000          | 0.0000          | 0.0000        | 0.0000        | 0.0000          |
| <b>Total</b> | <b>0.1027</b> | <b>1.0518</b> | <b>1.0119</b> | <b>1.5700e-003</b> |               | <b>0.0569</b> | <b>0.0569</b> |                | <b>0.0523</b> | <b>0.0523</b> | <b>0.0000</b> | <b>141.2786</b> | <b>141.2786</b> | <b>0.0447</b> | <b>0.0000</b> | <b>142.3961</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 4.1900e-003        | 2.9400e-003        | 0.0318        | 8.0000e-005        | 7.6000e-003        | 6.0000e-005        | 7.6600e-003        | 2.0200e-003        | 5.0000e-005        | 2.0700e-003        | 0.0000        | 6.9487        | 6.9487        | 2.2000e-004        | 0.0000        | 6.9541        |
| <b>Total</b> | <b>4.1900e-003</b> | <b>2.9400e-003</b> | <b>0.0318</b> | <b>8.0000e-005</b> | <b>7.6000e-003</b> | <b>6.0000e-005</b> | <b>7.6600e-003</b> | <b>2.0200e-003</b> | <b>5.0000e-005</b> | <b>2.0700e-003</b> | <b>0.0000</b> | <b>6.9487</b> | <b>6.9487</b> | <b>2.2000e-004</b> | <b>0.0000</b> | <b>6.9541</b> |

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**3.5 Paving - 2020**

**Unmitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |               |               |                |
| Off-Road     | 0.0624        | 0.6470        | 0.6740        | 1.0500e-003        |               | 0.0346        | 0.0346        |                | 0.0319        | 0.0319        | 0.0000        | 92.1298        | 92.1298        | 0.0298        | 0.0000        | 92.8747        |
| Paving       | 1.5700e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b> | <b>0.0640</b> | <b>0.6470</b> | <b>0.6740</b> | <b>1.0500e-003</b> |               | <b>0.0346</b> | <b>0.0346</b> |                | <b>0.0319</b> | <b>0.0319</b> | <b>0.0000</b> | <b>92.1298</b> | <b>92.1298</b> | <b>0.0298</b> | <b>0.0000</b> | <b>92.8747</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.5700e-003        | 1.7400e-003        | 0.0191        | 5.0000e-005        | 5.0700e-003        | 4.0000e-005        | 5.1000e-003        | 1.3500e-003        | 3.0000e-005        | 1.3800e-003        | 0.0000        | 4.4901        | 4.4901        | 1.3000e-004        | 0.0000        | 4.4933        |
| <b>Total</b> | <b>2.5700e-003</b> | <b>1.7400e-003</b> | <b>0.0191</b> | <b>5.0000e-005</b> | <b>5.0700e-003</b> | <b>4.0000e-005</b> | <b>5.1000e-003</b> | <b>1.3500e-003</b> | <b>3.0000e-005</b> | <b>1.3800e-003</b> | <b>0.0000</b> | <b>4.4901</b> | <b>4.4901</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>4.4933</b> |

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**3.5 Paving - 2020**

**Mitigated Construction On-Site**

|              | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10  | PM10 Total    | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total   | Bio- CO2      | NBio- CO2      | Total CO2      | CH4           | N2O           | CO2e           |
|--------------|---------------|---------------|---------------|--------------------|---------------|---------------|---------------|----------------|---------------|---------------|---------------|----------------|----------------|---------------|---------------|----------------|
| Category     | tons/yr       |               |               |                    |               |               |               |                |               |               | MT/yr         |                |                |               |               |                |
| Off-Road     | 0.0624        | 0.6470        | 0.6740        | 1.0500e-003        |               | 0.0346        | 0.0346        |                | 0.0319        | 0.0319        | 0.0000        | 92.1297        | 92.1297        | 0.0298        | 0.0000        | 92.8746        |
| Paving       | 1.5700e-003   |               |               |                    |               | 0.0000        | 0.0000        |                | 0.0000        | 0.0000        | 0.0000        | 0.0000         | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b> | <b>0.0640</b> | <b>0.6470</b> | <b>0.6740</b> | <b>1.0500e-003</b> |               | <b>0.0346</b> | <b>0.0346</b> |                | <b>0.0319</b> | <b>0.0319</b> | <b>0.0000</b> | <b>92.1297</b> | <b>92.1297</b> | <b>0.0298</b> | <b>0.0000</b> | <b>92.8746</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.5700e-003        | 1.7400e-003        | 0.0191        | 5.0000e-005        | 5.0700e-003        | 4.0000e-005        | 5.1000e-003        | 1.3500e-003        | 3.0000e-005        | 1.3800e-003        | 0.0000        | 4.4901        | 4.4901        | 1.3000e-004        | 0.0000        | 4.4933        |
| <b>Total</b> | <b>2.5700e-003</b> | <b>1.7400e-003</b> | <b>0.0191</b> | <b>5.0000e-005</b> | <b>5.0700e-003</b> | <b>4.0000e-005</b> | <b>5.1000e-003</b> | <b>1.3500e-003</b> | <b>3.0000e-005</b> | <b>1.3800e-003</b> | <b>0.0000</b> | <b>4.4901</b> | <b>4.4901</b> | <b>1.3000e-004</b> | <b>0.0000</b> | <b>4.4933</b> |

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**3.6 Architectural Coating - 2019**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.2268        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0184        | 0.1266        | 0.1271        | 2.1000e-004        |               | 8.8800e-003        | 8.8800e-003        |                | 8.8800e-003        | 8.8800e-003        | 0.0000        | 17.6175        | 17.6175        | 1.4900e-003        | 0.0000        | 17.6547        |
| <b>Total</b>    | <b>0.2452</b> | <b>0.1266</b> | <b>0.1271</b> | <b>2.1000e-004</b> |               | <b>8.8800e-003</b> | <b>8.8800e-003</b> |                | <b>8.8800e-003</b> | <b>8.8800e-003</b> | <b>0.0000</b> | <b>17.6175</b> | <b>17.6175</b> | <b>1.4900e-003</b> | <b>0.0000</b> | <b>17.6547</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.7900e-003        | 1.9600e-003        | 0.0212        | 5.0000e-005        | 5.0700e-003        | 4.0000e-005        | 5.1100e-003        | 1.3500e-003        | 3.0000e-005        | 1.3800e-003        | 0.0000        | 4.6325        | 4.6325        | 1.4000e-004        | 0.0000        | 4.6361        |
| <b>Total</b> | <b>2.7900e-003</b> | <b>1.9600e-003</b> | <b>0.0212</b> | <b>5.0000e-005</b> | <b>5.0700e-003</b> | <b>4.0000e-005</b> | <b>5.1100e-003</b> | <b>1.3500e-003</b> | <b>3.0000e-005</b> | <b>1.3800e-003</b> | <b>0.0000</b> | <b>4.6325</b> | <b>4.6325</b> | <b>1.4000e-004</b> | <b>0.0000</b> | <b>4.6361</b> |



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**3.6 Architectural Coating - 2019**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.2268        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0184        | 0.1266        | 0.1271        | 2.1000e-004        |               | 8.8800e-003        | 8.8800e-003        |                | 8.8800e-003        | 8.8800e-003        | 0.0000        | 17.6174        | 17.6174        | 1.4900e-003        | 0.0000        | 17.6546        |
| <b>Total</b>    | <b>0.2452</b> | <b>0.1266</b> | <b>0.1271</b> | <b>2.1000e-004</b> |               | <b>8.8800e-003</b> | <b>8.8800e-003</b> |                | <b>8.8800e-003</b> | <b>8.8800e-003</b> | <b>0.0000</b> | <b>17.6174</b> | <b>17.6174</b> | <b>1.4900e-003</b> | <b>0.0000</b> | <b>17.6546</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 2.7900e-003        | 1.9600e-003        | 0.0212        | 5.0000e-005        | 5.0700e-003        | 4.0000e-005        | 5.1100e-003        | 1.3500e-003        | 3.0000e-005        | 1.3800e-003        | 0.0000        | 4.6325        | 4.6325        | 1.4000e-004        | 0.0000        | 4.6361        |
| <b>Total</b> | <b>2.7900e-003</b> | <b>1.9600e-003</b> | <b>0.0212</b> | <b>5.0000e-005</b> | <b>5.0700e-003</b> | <b>4.0000e-005</b> | <b>5.1100e-003</b> | <b>1.3500e-003</b> | <b>3.0000e-005</b> | <b>1.3800e-003</b> | <b>0.0000</b> | <b>4.6325</b> | <b>4.6325</b> | <b>1.4000e-004</b> | <b>0.0000</b> | <b>4.6361</b> |

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**3.6 Architectural Coating - 2020**

**Unmitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.1512        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0111        | 0.0775        | 0.0843        | 1.4000e-004        |               | 5.1000e-003        | 5.1000e-003        |                | 5.1000e-003        | 5.1000e-003        | 0.0000        | 11.7450        | 11.7450        | 9.1000e-004        | 0.0000        | 11.7677        |
| <b>Total</b>    | <b>0.1624</b> | <b>0.0775</b> | <b>0.0843</b> | <b>1.4000e-004</b> |               | <b>5.1000e-003</b> | <b>5.1000e-003</b> |                | <b>5.1000e-003</b> | <b>5.1000e-003</b> | <b>0.0000</b> | <b>11.7450</b> | <b>11.7450</b> | <b>9.1000e-004</b> | <b>0.0000</b> | <b>11.7677</b> |

**Unmitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.7100e-003        | 1.1600e-003        | 0.0127        | 3.0000e-005        | 3.3800e-003        | 2.0000e-005        | 3.4000e-003        | 9.0000e-004        | 2.0000e-005        | 9.2000e-004        | 0.0000        | 2.9934        | 2.9934        | 8.0000e-005        | 0.0000        | 2.9955        |
| <b>Total</b> | <b>1.7100e-003</b> | <b>1.1600e-003</b> | <b>0.0127</b> | <b>3.0000e-005</b> | <b>3.3800e-003</b> | <b>2.0000e-005</b> | <b>3.4000e-003</b> | <b>9.0000e-004</b> | <b>2.0000e-005</b> | <b>9.2000e-004</b> | <b>0.0000</b> | <b>2.9934</b> | <b>2.9934</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>2.9955</b> |

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**3.6 Architectural Coating - 2020**

**Mitigated Construction On-Site**

|                 | ROG           | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O           | CO2e           |
|-----------------|---------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|---------------|----------------|
| Category        | tons/yr       |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |               |                |
| Archit. Coating | 0.1512        |               |               |                    |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000        | 0.0000         |
| Off-Road        | 0.0111        | 0.0775        | 0.0843        | 1.4000e-004        |               | 5.1000e-003        | 5.1000e-003        |                | 5.1000e-003        | 5.1000e-003        | 0.0000        | 11.7450        | 11.7450        | 9.1000e-004        | 0.0000        | 11.7677        |
| <b>Total</b>    | <b>0.1624</b> | <b>0.0775</b> | <b>0.0843</b> | <b>1.4000e-004</b> |               | <b>5.1000e-003</b> | <b>5.1000e-003</b> |                | <b>5.1000e-003</b> | <b>5.1000e-003</b> | <b>0.0000</b> | <b>11.7450</b> | <b>11.7450</b> | <b>9.1000e-004</b> | <b>0.0000</b> | <b>11.7677</b> |

**Mitigated Construction Off-Site**

|              | ROG                | NOx                | CO            | SO2                | Fugitive PM10      | Exhaust PM10       | PM10 Total         | Fugitive PM2.5     | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2     | Total CO2     | CH4                | N2O           | CO2e          |
|--------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|---------------|---------------|---------------|--------------------|---------------|---------------|
| Category     | tons/yr            |                    |               |                    |                    |                    |                    |                    |                    |                    | MT/yr         |               |               |                    |               |               |
| Hauling      | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Vendor       | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000        | 0.0000        | 0.0000             | 0.0000        | 0.0000        |
| Worker       | 1.7100e-003        | 1.1600e-003        | 0.0127        | 3.0000e-005        | 3.3800e-003        | 2.0000e-005        | 3.4000e-003        | 9.0000e-004        | 2.0000e-005        | 9.2000e-004        | 0.0000        | 2.9934        | 2.9934        | 8.0000e-005        | 0.0000        | 2.9955        |
| <b>Total</b> | <b>1.7100e-003</b> | <b>1.1600e-003</b> | <b>0.0127</b> | <b>3.0000e-005</b> | <b>3.3800e-003</b> | <b>2.0000e-005</b> | <b>3.4000e-003</b> | <b>9.0000e-004</b> | <b>2.0000e-005</b> | <b>9.2000e-004</b> | <b>0.0000</b> | <b>2.9934</b> | <b>2.9934</b> | <b>8.0000e-005</b> | <b>0.0000</b> | <b>2.9955</b> |

**4.0 Operational Detail - Mobile**

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**4.1 Mitigation Measures Mobile**

|             | ROG     | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4    | N2O    | CO2e     |
|-------------|---------|--------|--------|-------------|---------------|--------------|------------|----------------|---------------|-------------|----------|-----------|-----------|--------|--------|----------|
| Category    | tons/yr |        |        |             |               |              |            |                |               |             | MT/yr    |           |           |        |        |          |
| Mitigated   | 0.1966  | 0.8070 | 2.1321 | 6.2000e-003 | 0.5159        | 5.6700e-003  | 0.5216     | 0.1383         | 5.3100e-003   | 0.1436      | 0.0000   | 569.5373  | 569.5373  | 0.0290 | 0.0000 | 570.2616 |
| Unmitigated | 0.1966  | 0.8070 | 2.1321 | 6.2000e-003 | 0.5159        | 5.6700e-003  | 0.5216     | 0.1383         | 5.3100e-003   | 0.1436      | 0.0000   | 569.5373  | 569.5373  | 0.0290 | 0.0000 | 570.2616 |

**4.2 Trip Summary Information**

| Land Use                | Average Daily Trip Rate |          |        | Unmitigated | Mitigated  |
|-------------------------|-------------------------|----------|--------|-------------|------------|
|                         | Weekday                 | Saturday | Sunday | Annual VMT  | Annual VMT |
| General Office Building | 881.52                  | 196.60   | 83.92  | 1,383,061   | 1,383,061  |
| Parking Lot             | 0.00                    | 0.00     | 0.00   |             |            |
| Total                   | 881.52                  | 196.60   | 83.92  | 1,383,061   | 1,383,061  |

**4.3 Trip Type Information**

| Land Use                | Miles      |            |             | Trip %     |            |             | Trip Purpose % |          |         |
|-------------------------|------------|------------|-------------|------------|------------|-------------|----------------|----------|---------|
|                         | H-W or C-W | H-S or C-C | H-O or C-NW | H-W or C-W | H-S or C-C | H-O or C-NW | Primary        | Diverted | Pass-by |
| General Office Building | 10.00      | 5.00       | 6.50        | 33.00      | 48.00      | 19.00       | 77             | 19       | 4       |
| Parking Lot             | 10.00      | 5.00       | 6.50        | 0.00       | 0.00       | 0.00        | 0              | 0        | 0       |

**4.4 Fleet Mix**

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| Land Use                | LDA      | LDT1     | LDT2     | MDV      | LHD1     | LHD2     | MHD      | HHD      | OBUS     | UBUS     | MCY      | SBUS     | MH       |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| General Office Building | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |
| Parking Lot             | 0.555851 | 0.039752 | 0.205040 | 0.120748 | 0.020349 | 0.005402 | 0.018507 | 0.022668 | 0.002052 | 0.002157 | 0.005939 | 0.000618 | 0.000915 |

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

| Category                | ROG         | NOx    | CO     | SO2         | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2 | Total CO2 | CH4         | N2O         | CO2e     |
|-------------------------|-------------|--------|--------|-------------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-----------|-----------|-------------|-------------|----------|
|                         | tons/yr     |        |        |             |               |              |             |                |               |             | MT/yr    |           |           |             |             |          |
| Electricity Mitigated   |             |        |        |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 154.3262  | 154.3262  | 0.0154      | 3.1900e-003 | 155.6635 |
| Electricity Unmitigated |             |        |        |             |               | 0.0000       | 0.0000      |                | 0.0000        | 0.0000      | 0.0000   | 154.3262  | 154.3262  | 0.0154      | 3.1900e-003 | 155.6635 |
| NaturalGas Mitigated    | 5.6500e-003 | 0.0513 | 0.0431 | 3.1000e-004 |               | 3.9000e-003  | 3.9000e-003 |                | 3.9000e-003   | 3.9000e-003 | 0.0000   | 55.8687   | 55.8687   | 1.0700e-003 | 1.0200e-003 | 56.2007  |
| NaturalGas Unmitigated  | 5.6500e-003 | 0.0513 | 0.0431 | 3.1000e-004 |               | 3.9000e-003  | 3.9000e-003 |                | 3.9000e-003   | 3.9000e-003 | 0.0000   | 55.8687   | 55.8687   | 1.0700e-003 | 1.0200e-003 | 56.2007  |

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**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

|                         | NaturalGas Use | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|-------------------------|----------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                | kBTU/yr        | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| General Office Building | 1.04694e+006   | 5.6500e-003        | 0.0513        | 0.0431        | 3.1000e-004        |               | 3.9000e-003        | 3.9000e-003        |                | 3.9000e-003        | 3.9000e-003        | 0.0000        | 55.8687        | 55.8687        | 1.0700e-003        | 1.0200e-003        | 56.2007        |
| Parking Lot             | 0              | 0.0000             | 0.0000        | 0.0000        | 0.0000             |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000             | 0.0000         |
| <b>Total</b>            |                | <b>5.6500e-003</b> | <b>0.0513</b> | <b>0.0431</b> | <b>3.1000e-004</b> |               | <b>3.9000e-003</b> | <b>3.9000e-003</b> |                | <b>3.9000e-003</b> | <b>3.9000e-003</b> | <b>0.0000</b> | <b>55.8687</b> | <b>55.8687</b> | <b>1.0700e-003</b> | <b>1.0200e-003</b> | <b>56.2007</b> |

**Mitigated**

|                         | NaturalGas Use | ROG                | NOx           | CO            | SO2                | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2      | Total CO2      | CH4                | N2O                | CO2e           |
|-------------------------|----------------|--------------------|---------------|---------------|--------------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|----------------|----------------|--------------------|--------------------|----------------|
| Land Use                | kBTU/yr        | tons/yr            |               |               |                    |               |                    |                    |                |                    |                    | MT/yr         |                |                |                    |                    |                |
| General Office Building | 1.04694e+006   | 5.6500e-003        | 0.0513        | 0.0431        | 3.1000e-004        |               | 3.9000e-003        | 3.9000e-003        |                | 3.9000e-003        | 3.9000e-003        | 0.0000        | 55.8687        | 55.8687        | 1.0700e-003        | 1.0200e-003        | 56.2007        |
| Parking Lot             | 0              | 0.0000             | 0.0000        | 0.0000        | 0.0000             |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000         | 0.0000         | 0.0000             | 0.0000             | 0.0000         |
| <b>Total</b>            |                | <b>5.6500e-003</b> | <b>0.0513</b> | <b>0.0431</b> | <b>3.1000e-004</b> |               | <b>3.9000e-003</b> | <b>3.9000e-003</b> |                | <b>3.9000e-003</b> | <b>3.9000e-003</b> | <b>0.0000</b> | <b>55.8687</b> | <b>55.8687</b> | <b>1.0700e-003</b> | <b>1.0200e-003</b> | <b>56.2007</b> |

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**5.3 Energy by Land Use - Electricity**

**Unmitigated**

|                         | Electricity Use | Total CO2       | CH4           | N2O                | CO2e            |
|-------------------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Land Use                | kWh/yr          | MT/yr           |               |                    |                 |
| General Office Building | 1.15403e+006    | 151.8032        | 0.0152        | 3.1400e-003        | 153.1187        |
| Parking Lot             | 19180           | 2.5230          | 2.5000e-004   | 5.0000e-005        | 2.5448          |
| <b>Total</b>            |                 | <b>154.3262</b> | <b>0.0154</b> | <b>3.1900e-003</b> | <b>155.6635</b> |

**Mitigated**

|                         | Electricity Use | Total CO2       | CH4           | N2O                | CO2e            |
|-------------------------|-----------------|-----------------|---------------|--------------------|-----------------|
| Land Use                | kWh/yr          | MT/yr           |               |                    |                 |
| General Office Building | 1.15403e+006    | 151.8032        | 0.0152        | 3.1400e-003        | 153.1187        |
| Parking Lot             | 19180           | 2.5230          | 2.5000e-004   | 5.0000e-005        | 2.5448          |
| <b>Total</b>            |                 | <b>154.3262</b> | <b>0.0154</b> | <b>3.1900e-003</b> | <b>155.6635</b> |

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

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|             | ROG     | NOx         | CO          | SO2    | Fugitive PM10 | Exhaust PM10 | PM10 Total  | Fugitive PM2.5 | Exhaust PM2.5 | PM2.5 Total | Bio- CO2 | NBio- CO2   | Total CO2   | CH4         | N2O    | CO2e        |
|-------------|---------|-------------|-------------|--------|---------------|--------------|-------------|----------------|---------------|-------------|----------|-------------|-------------|-------------|--------|-------------|
| Category    | tons/yr |             |             |        |               |              |             |                |               |             | MT/yr    |             |             |             |        |             |
| Mitigated   | 0.3537  | 3.0000e-005 | 2.7800e-003 | 0.0000 |               | 1.0000e-005  | 1.0000e-005 |                | 1.0000e-005   | 1.0000e-005 | 0.0000   | 5.3800e-003 | 5.3800e-003 | 1.0000e-005 | 0.0000 | 5.7400e-003 |
| Unmitigated | 0.3537  | 3.0000e-005 | 2.7800e-003 | 0.0000 |               | 1.0000e-005  | 1.0000e-005 |                | 1.0000e-005   | 1.0000e-005 | 0.0000   | 5.3800e-003 | 5.3800e-003 | 1.0000e-005 | 0.0000 | 5.7400e-003 |

6.2 Area by SubCategory

Unmitigated

|                       | ROG           | NOx                | CO                 | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2          | Total CO2          | CH4                | N2O           | CO2e               |
|-----------------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|
| SubCategory           | tons/yr       |                    |                    |               |               |                    |                    |                |                    |                    | MT/yr         |                    |                    |                    |               |                    |
| Architectural Coating | 0.0378        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Consumer Products     | 0.3157        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Landscaping           | 2.6000e-004   | 3.0000e-005        | 2.7800e-003        | 0.0000        |               | 1.0000e-005        | 1.0000e-005        |                | 1.0000e-005        | 1.0000e-005        | 0.0000        | 5.3800e-003        | 5.3800e-003        | 1.0000e-005        | 0.0000        | 5.7400e-003        |
| <b>Total</b>          | <b>0.3537</b> | <b>3.0000e-005</b> | <b>2.7800e-003</b> | <b>0.0000</b> |               | <b>1.0000e-005</b> | <b>1.0000e-005</b> |                | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.3800e-003</b> | <b>5.3800e-003</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.7400e-003</b> |



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**6.2 Area by SubCategory**

**Mitigated**

|                       | ROG           | NOx                | CO                 | SO2           | Fugitive PM10 | Exhaust PM10       | PM10 Total         | Fugitive PM2.5 | Exhaust PM2.5      | PM2.5 Total        | Bio- CO2      | NBio- CO2          | Total CO2          | CH4                | N2O           | CO2e               |
|-----------------------|---------------|--------------------|--------------------|---------------|---------------|--------------------|--------------------|----------------|--------------------|--------------------|---------------|--------------------|--------------------|--------------------|---------------|--------------------|
| SubCategory           | tons/yr       |                    |                    |               |               |                    |                    |                |                    |                    | MT/yr         |                    |                    |                    |               |                    |
| Architectural Coating | 0.0378        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Consumer Products     | 0.3157        |                    |                    |               |               | 0.0000             | 0.0000             |                | 0.0000             | 0.0000             | 0.0000        | 0.0000             | 0.0000             | 0.0000             | 0.0000        | 0.0000             |
| Landscaping           | 2.6000e-004   | 3.0000e-005        | 2.7800e-003        | 0.0000        |               | 1.0000e-005        | 1.0000e-005        |                | 1.0000e-005        | 1.0000e-005        | 0.0000        | 5.3800e-003        | 5.3800e-003        | 1.0000e-005        | 0.0000        | 5.7400e-003        |
| <b>Total</b>          | <b>0.3537</b> | <b>3.0000e-005</b> | <b>2.7800e-003</b> | <b>0.0000</b> |               | <b>1.0000e-005</b> | <b>1.0000e-005</b> |                | <b>1.0000e-005</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.3800e-003</b> | <b>5.3800e-003</b> | <b>1.0000e-005</b> | <b>0.0000</b> | <b>5.7400e-003</b> |

**7.0 Water Detail**

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**7.1 Mitigation Measures Water**

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|             | Total CO2 | CH4    | N2O    | CO2e    |
|-------------|-----------|--------|--------|---------|
| Category    | MT/yr     |        |        |         |
| Mitigated   | 18.3437   | 0.0186 | 0.0112 | 22.1483 |
| Unmitigated | 18.3437   | 0.0186 | 0.0112 | 22.1483 |

**7.2 Water by Land Use**

**Unmitigated**

|                         | Indoor/Outdoor Use | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|--------------------|----------------|---------------|---------------|----------------|
| Land Use                | Mgal               | MT/yr          |               |               |                |
| General Office Building | 14.2045 / 8.70597  | 18.3437        | 0.0186        | 0.0112        | 22.1483        |
| Parking Lot             | 0 / 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                    | <b>18.3437</b> | <b>0.0186</b> | <b>0.0112</b> | <b>22.1483</b> |

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**7.2 Water by Land Use**

**Mitigated**

|                         | Indoor/Outdoor Use | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|--------------------|----------------|---------------|---------------|----------------|
| Land Use                | Mgal               | MT/yr          |               |               |                |
| General Office Building | 14.2045 / 8.70597  | 18.3437        | 0.0186        | 0.0112        | 22.1483        |
| Parking Lot             | 0 / 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                    | <b>18.3437</b> | <b>0.0186</b> | <b>0.0112</b> | <b>22.1483</b> |

**8.0 Waste Detail**

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**8.1 Mitigation Measures Waste**

**Category/Year**

|             | Total CO2 | CH4    | N2O    | CO2e    |
|-------------|-----------|--------|--------|---------|
|             | MT/yr     |        |        |         |
| Mitigated   | 15.0883   | 0.8917 | 0.0000 | 37.3807 |
| Unmitigated | 15.0883   | 0.8917 | 0.0000 | 37.3807 |

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**8.2 Waste by Land Use**

**Unmitigated**

|                         | Waste Disposed | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|----------------|----------------|---------------|---------------|----------------|
| Land Use                | tons           | MT/yr          |               |               |                |
| General Office Building | 74.33          | 15.0883        | 0.8917        | 0.0000        | 37.3807        |
| Parking Lot             | 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                | <b>15.0883</b> | <b>0.8917</b> | <b>0.0000</b> | <b>37.3807</b> |

**Mitigated**

|                         | Waste Disposed | Total CO2      | CH4           | N2O           | CO2e           |
|-------------------------|----------------|----------------|---------------|---------------|----------------|
| Land Use                | tons           | MT/yr          |               |               |                |
| General Office Building | 74.33          | 15.0883        | 0.8917        | 0.0000        | 37.3807        |
| Parking Lot             | 0              | 0.0000         | 0.0000        | 0.0000        | 0.0000         |
| <b>Total</b>            |                | <b>15.0883</b> | <b>0.8917</b> | <b>0.0000</b> | <b>37.3807</b> |

**9.0 Operational Offroad**

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| Equipment Type | Number | Hours/Day | Days/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|-----------|-------------|-------------|-----------|
|----------------|--------|-----------|-----------|-------------|-------------|-----------|

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**10.0 Stationary Equipment**

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**Fire Pumps and Emergency Generators**

| Equipment Type | Number | Hours/Day | Hours/Year | Horse Power | Load Factor | Fuel Type |
|----------------|--------|-----------|------------|-------------|-------------|-----------|
|----------------|--------|-----------|------------|-------------|-------------|-----------|

**Boilers**

| Equipment Type | Number | Heat Input/Day | Heat Input/Year | Boiler Rating | Fuel Type |
|----------------|--------|----------------|-----------------|---------------|-----------|
|----------------|--------|----------------|-----------------|---------------|-----------|

**User Defined Equipment**

| Equipment Type | Number |
|----------------|--------|
|----------------|--------|

**11.0 Vegetation**

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**ATTACHMENT D**

**Notice of Decision and Findings of Fact for Mack Road Mini Storage  
Located at 5701 Mack Road, Sacramento, California  
in the General Commercial Review (C-2-R) Zone (P04-146)**

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**Attachment 1**

***AMENDED BY STAFF MARCH 10, 2005***  
**NOTICE OF DECISION AND FINDINGS OF FACT FOR**  
**MACK ROAD MINI STORAGE LOCATED AT 5701 MACK ROAD SACRAMENTO,**  
**CALIFORNIA IN THE GENERAL COMMERCIAL REVIEW (C-2-R) ZONE(P04-146)**

At the regular meeting March 10, 2005, the City Planning Commission heard and considered evidence in the above-entitled matter. Based on verbal and documentary evidence at said hearing, the Planning Commission took the following actions for the location listed above:

- A. Ratified the **Environmental Determination:** Mitigated Negative Declaration;
- B. Approved the **Mitigation Monitoring Plan**
- C. Approved the **Special Permit** to construct a 79,919 square foot mini storage with 137 RV parking spaces in the General Commercial Review (C-2-R) Zone;
- D. Approved the **Parcel Merger** to merge 4 lots totalling 7.52± gross acres into 2 lots.
- E. Approved the **Section 65402(a) Review** For General Plan consistency for the summary vacation of existing access and utilities easements located at 5701 Mack Road.

These actions were made based upon the following findings of fact and subject to the following conditions:

**FINDINGS OF FACT**

- A. **Environmental Determination:** The City Planning Commission approved the Mitigated Negative Declaration (the Mitigated Negative Declaration is approved) based on the following findings.
  - 1. The Negative Declaration was prepared and circulated for the above-identified project pursuant to the requirements of CEQA;

- 
2. The proposed Negative Declaration and comments received during the public review process were considered prior to action being taken on the project;
  3. Based upon the whole record before it, the Initial Study and the comments received during the public review process, it has been determined that there is no substantial evidence that the project will have a significant effect on the environment.
  4. The Negative Declaration reflects the lead agency's independent judgment and analysis
- B. The Mitigation Monitoring Plan** is approved based upon the following findings of fact:
1. One or more mitigation measures have been added to the above-identified project;
  2. A Mitigation Monitoring Plan has been prepared to ensure compliance and implementation of the mitigation measures for the above-identified project, a copy of which is attached as **(Exhibit 1A)**;
  3. The Mitigation Monitoring Plan meets the requirements of Public Resources Code Sec. 21081.6.
  4. The Mitigation Monitoring Plan is approved, and the mitigation measures shall be implemented and monitored as set forth in the Plan.
- C. Special Permit:** The Special Permit to construct a 79,919 square foot mini storage with 137 RV parking spaces in the General Commercial Review (C-2-R) Zone is approved based on the following findings of fact:
1. The project is based upon sound principles of land use in that the proposed mini-storage land use is consistent with the General and the South Sacramento Community plans land use designations which designate the site for Community/Neighborhood Commercial & Offices and General Commercial, respectively and is also consistent with the proposed General Commercial Review (C-2-R) zoning in that mini-storage use is allowed with the approval of a Special Permit;
  2. The proposed use will not adversely affect the public health, safety or general welfare of the surrounding residential neighborhood in that:
    - a. The mini-storage use is considered to be a low intensity use therefore



the use should have minimal impact on the adjacent uses;

- b. The perimeter façades as well as the office/residence have been enhanced to blend with the adjacent uses and;
- c. The facility will have a manager in residence and is surrounded by a 7 to 12-foot tall masonry block walls and is gated limiting facility access to renters;
- d. The proposed project will provide the required on-site parking that will minimize parking impacts on the surrounding neighborhood.

3. The project is consistent with General Plan policies in that:

- a. The project will minimize potential adverse impacts from development because the project has implemented façade enhancements as well as site improvements that will blend with and enhance the Mack Road frontage
- b. The project will minimize potential impacts because the mini-storage use is a low intensity use.

**D.** The **Parcel Merger** to merge 4 lots totalling 7.52± gross acres into 2 lots is approved based on the following findings of fact:

- a. That all existing streets and/or utility easements of record are reserved;
- b. That the resulting parcels conform to the requirements of this title, the city's General Plan, the city's comprehensive zoning ordinance, and the city's building code

### **CONDITIONS OF APPROVAL**

**C.** The **Special Permit** to construct a 79,919 square foot mini storage with 137 RV parking spaces in the General Commercial Review (C-2-R) Zone is hereby approved subject to the following conditions of approval:

#### **Planning**

- C1. The applicant shall obtain all necessary building permits prior to commencing construction;

- 
- C2. Development of this site shall be in compliance with the attached plans and landscape exhibits as conditioned. Any substantial modification to the project shall be subject to review and approval by Planning Staff prior to the issuance of building permits;
- C3. The parking areas shall meet the Sacramento City Code regulations regarding tree shading (Chapter 17.68);
- C4. All mechanical equipment shall be screened. All rooftop mechanical and communications equipment shall be completely screened from view from public streets by the building parapet, screen wall, and architectural projections which are integral to the building design;
- C5. Prior to issuance of any building permits, the applicant shall provide appropriate documentation indicating compliance with all mitigation measures as prescribed in the adopted Mitigation Monitoring Plan (Exhibit 1A);
- C6. The applicant shall paint electrical meters/cabinets, telephone connection boxes, and other utility appurtenances to match the building to which they are attached;
- C7. The electronic access keyboard(s) shall be well illuminated from dusk to dawn;
- C8. All roofing shall be concrete tile or other similar tile roofing. The perimeter concrete masonry wall shall be 12 feet high as noted on the project elevations. Any substantial changes to the roofing or the perimeter wall shall be subject to Planning Staff review and may require the approval of additional entitlements.

**Signage:**

- C9. A sign permit shall be obtained prior to construction or installation of any attached or detached signs;

**Trash Enclosures:**

- C10. Recycling and Solid Waste disposal facilities shall comply with the City of Sacramento Recycling and Solid Waste Disposal Regulations as specified in Title 17, Chapter 17.72;
- C11. Trash enclosures shall be constructed of concrete block or similar masonry material and be finished with stucco or other material which is comparable in color and texture to the building elevations;

**Landscaping:**

- C12. In order to provide adequate surveillance opportunities, all plants and shrubs are to be maintained at a maximum height of thirty inches (30"); the lowest tree branches shall be at least six feet (6'). Decorative planting shall be maintained so as not to obstruct or diminish lighting levels throughout the project;
- C13. A 25' deep landscape area shall run the length of the Mack Road frontage, except where ingress/egress driveways exist. The landscape area shall have automatic irrigation installed and a drought tolerant groundcover shall be planted, to cover the entire landscape area within 2 years of installation;
- C14. Section 17.64.030(H) of the Zoning Ordinance requires trees to be planted and maintained throughout any surface parking lot to ensure that, within fifteen (15) years after establishment of the parking lot, at least fifty (50) percent of the parking area will be shaded. The project will be conditioned to comply with the fifty percent shading requirement for all parking and maneuvering areas;

**Lighting:**

- C15. Lighting shall be designed so as not to produce hazardous and annoying glare to motorists, adjacent properties, or the general public. All fixtures should be placed in a manner that avoids glare when observed from street or other public areas;
- C16. Lighting levels shall be as follows: 1.5 foot-candles of minimum maintained illumination per square foot of parking space between the hours of dusk and one hour after sunrise. A minimum of 0.25 foot-candles of illumination shall be provided at the surface of any walkway, alcove, or passageway related to the building project during the same hours;
- C17. The height of pole mounted light fixtures shall not be more than 30 feet;

**Development Engineering**

- C18. Repair or replace/reconstruct any existing deteriorated curb, gutter and sidewalk fronting the property along Mack road per City standards and to the satisfaction of the Development Engineering and Finance Division;
- C19. Dedicate sufficient right-of-way and construct full frontage improvements along Mack Road. Mack Road frontage shall be constructed based on a 4-lane arterial standard with separated sidewalks;

- 
- C20. Construct standard frontage improvements (i.e. Curb, gutter and sidewalk) along the private street (F street) at a minimum from Mack Road to the project main entrance. This will include the round corner and handicapped ramps at the intersection of Mack Road and the private street;
- C21. Construct standard improvements as noted in these conditions pursuant to section 16.48.110 of the City Code. Improvements shall be designed and constructed to City standards in place at the time that the Building Permit is issued. All improvements shall be designed and constructed to the satisfaction of the Development Engineering and Finance Division. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards;
- C22. All new driveways shall be designed and constructed to City Standards to the satisfaction of the Development Engineering and Finance Division;
- C23. The site plan shall conform to A.D.A. requirements in all respects;
- C24. The site plan shall conform to the parking requirements set forth in chapter 17 of City Code (Zoning Ordinance);
- C25. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Development Engineering and Finance Division;

### Utilities

- C26. All on-site utilities shall be abandoned according to the conditions of abandonment required by the Department of Utilities. The applicant shall comply with all conditions of the abandonment to the satisfaction of the Department of Utilities.
- C27. Properly abandon all excess utility easements and services to the satisfaction of the Department of Utilities and the appropriate utility companies (PG&E, SMUD, Telephone, Cable, etc.).
- C28. Only one domestic water service per parcel is allowed. Any excess domestic water services must be abandoned to the satisfaction of the Department of Utilities. The applicant should be advised that the tap record research and verification of tap locations by the field crews involved prior to sign-off of this

condition may take a considerable amount of time, therefore, all requests should be submitted in a timely manner.

- C29. The proposed development is located within County Sanitation District No. 1 (CSD1). Satisfy all CSD1 requirements.
- C30. An on-site surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All on-site systems shall be designed to the standard for private storm drainage systems (per Section 11.12 of the Design and Procedures Manual). Finished floor elevations shall be a minimum of 1.5' above the 100-year HGL and 1.7' above the controlling overland release.
- C31. If required by the Department of Utilities, provide a Public Access Easement for maintenance access along the private drive ('F' Street) from Mack Road to the northern terminus. Provide a cyclone fence access gate (2 – 6' wide gates) to Elder Creek at the terminus of 'F' Street. The preferred location for the gate is on the east side of 'F' Street. If the adjacent property owner is not agreeable, the gate may be installed on the west side of 'F' Street.
- C32. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the Department of Utilities.
- C33. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to prepare erosion and sediment control plans for both during and after construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction.
- C34. This project will disturb greater than 1 acre of property, therefore the project is required to comply with the State "NPDES General Permit for Stormwater Discharges Associated with Construction Activity" (State Permit). To comply with the State Permit, the applicant will need to file a Notice of Intent (NOI) with the State Water Resources Control Board (SWRCB) and prepare a Stormwater Pollution Prevention Plan (SWPPP) prior to construction. A copy of the State Permit and NOI may be obtained at [www.swrcb.ca.gov/stormwtr/construction.html](http://www.swrcb.ca.gov/stormwtr/construction.html). The SWPPP will be reviewed by the Department of Utilities prior to issuing a grading permit or approval of improvement plans to assure that the following items are included: 1) vicinity map, 2) site map, 3) list of potential pollutant sources, 4) type and location of erosion and sediment BMPs, 5) name and phone number of person

responsible for SWPPP, 6) signed certification page by property owner or authorized representative.

- C35. Post construction, stormwater quality control measures shall be incorporated into the development to minimize the increase of urban runoff pollution caused by development of the area. Since the project is not served by a regional water quality control facility and is greater than 1 acre, both source controls *and on-site treatment control measures* are required. On-site treatment control measures may affect site design and site configuration and therefore, should be considered during the early planning stages. Specific source controls are required for (1) commercial/industrial material storage, (2) commercial/industrial outdoor loading/unloading of materials, (3) commercial/industrial vehicle and equipment fueling, (4) commercial/industrial vehicle and equipment maintenance, repair and washing, (5) commercial/industrial outdoor process equipment operations and maintenance and (6) commercial/industrial waste handling. Storm drain message is required at all drain inlets. Improvement plans must include the source controls and on-site treatment control measures selected for the site. Refer to the "Guidance Manual for On-Site Stormwater Quality Control Measures", dated January 2000, for appropriate source control measures and on-site treatment control measures.

## Fire

- C36. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction.
- C37. Provide a water flow test. (Contact Department of Utilities at 916-264-5371)
- C38. Provide the required fire hydrants in accordance with CFC 903.4.2 and Appendix III-B, Section 5.
- C39. Provide appropriate Knox access for site.
- C40. Roads used for Fire Department access that are less than 28 feet in width shall be marked "No Parking Fire Lane" on both sides; roads less than 36 feet in width shall be marked on one side.
- C41. Locate and identify Fire Department Connections (FDCs) on address side of building within 40 feet of a fire hydrant.

## Advisory Notes:

### Utilities

- C42. Prior to design of the subject project, the Department of Utilities suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the fire suppression systems.
- C43. City Code 13.04.570 requires that no fire service shall be installed across any parcel other than the parcel to which the service is being furnished, provided that the fire chief may, in his or her discretion, authorize a fire service line that serves more than one parcel, upon the recording of an agreement, in a form approved by the city, that fully provides for the operation, maintenance and repair of the line, and grants a permanent easement for these purposes, at no cost or liability to the city.

- D. The **Parcel Merger** to merge 4 lots totalling 7.52± gross acres into 2 lots is approved based on the following findings of fact:

#### **Development Engineering**

- D1. The applicant shall file an application for a Certificate of Compliance with the development engineering and Finance Division. The applicant must submit all documents required by the submittal requirements checklist and pay necessary fees;
- D2. The applicant must file for a Waiver of Parcel map;
- D3. The applicant shall pay off or segregate existing assessments.

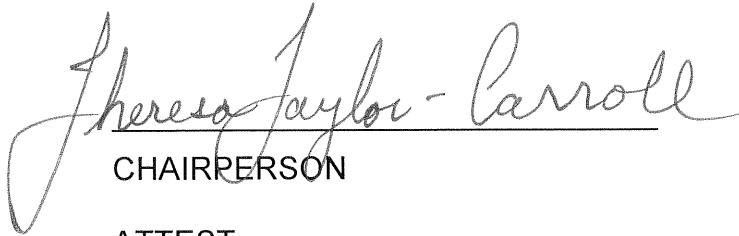
#### **Utilities**

- D4. Only one domestic water service per parcel is allowed. Any excess domestic water services must be abandoned to the satisfaction of the Department of Utilities. The applicant should be advised that the tap record research and verification of tap locations by the field crews involved prior to sign-off of this condition may take a considerable amount of time, therefore, all requests should be submitted in a timely manner.

#### **Fire**

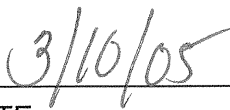
- D5. All turning radii for fire access shall be designed as 35' inside and 55' outside.

- D6. Roads used for Fire Department access shall have an unobstructed width of not less than 20' and unobstructed vertical clearance of 13'6" or more.
- D7. Fire service mains shall not cross property lines unless a reciprocal easement agreement is provided. (see Fire Marshal for assistance)
- D8. Shared driveways used for Fire Department access will require a reciprocal Ingress/egress agreement, and shall be reviewed by the City Attorney.
- D9. Multiple fire services are allowed per parcel and may be required.
- D10. The proposed project is located in the Flood zone designated as an X zone on the Federal Emergency Management Agency (FEMA) Federal Insurance Rate Maps (FIRMs) dated July 6, 1998. Within the X zone, there are no requirements to elevate or flood proof.

  
\_\_\_\_\_  
CHAIRPERSON

ATTEST:

  
\_\_\_\_\_  
SECRETARY TO CITY PLANNING COMMISSION

  
\_\_\_\_\_  
DATE (P04-146)



**MITIGATION MONITORING PLAN**

FOR  
MACK ROAD MINI STORAGE (P04-146)

**TYPE OF ENVIRONMENTAL DOCUMENT:**  
INITIAL STUDY/ NEGATIVE DECLARATION

**PREPARED FOR:**  
CITY OF SACRAMENTO, DEVELOPMENT SERVICES DEPARTMENT

**DATE:**  
DECEMBER 2004

**ADOPTED BY:**  
CITY OF SACRAMENTO  
CITY PLANNING COMMISSION

**DATE:**  
3/10/05

**ATTEST:**  
*Theresa Taylor-Carroll*

**MACK ROAD MINI STORAGE (P04-146)  
MITIGATION MONITORING PLAN**

This Mitigation Monitoring Plan (MMP) has been required by and prepared for the City of Sacramento Development Services Department, Environmental Planning Services, 1231 I Street, Room 300, Sacramento, CA 95814, pursuant to CEQA Guidelines Section 21081.6.

**SECTION 1: PROJECT IDENTIFICATION**

**Project Name / File Number:** Mack Road Mini Storage (P04-146)  
**Owner/Developer- Name:** Bob Nolasco  
**Address:** 1610 Arden Way, #195  
Sacramento, CA 95815

**Project Location / Legal Description of Property (if recorded):**

The project is located east of Center Parkway at 5701 Mack Road in the South Sacramento Community Plan area of the City of Sacramento (APNs: 118-0111-062, -063, -064, -065).

**Project Description:**

The proposed project consists of entitlements to develop a 79,919± square foot mini storage on 7.52± gross acres in the General Commercial Review (C-2-R) Zone in the South Sacramento Community Plan area. Specific entitlements include:

- A. **Special Permit** to construct a 79,919 square foot mini storage with 137 RV parking spaces in General Commercial Review (C-2-R) zone;
- B. **Parcel Merger** to merge 4 lots totaling 7.52± gross acres into 2 lots.

**SECTION 2: GENERAL INFORMATION**

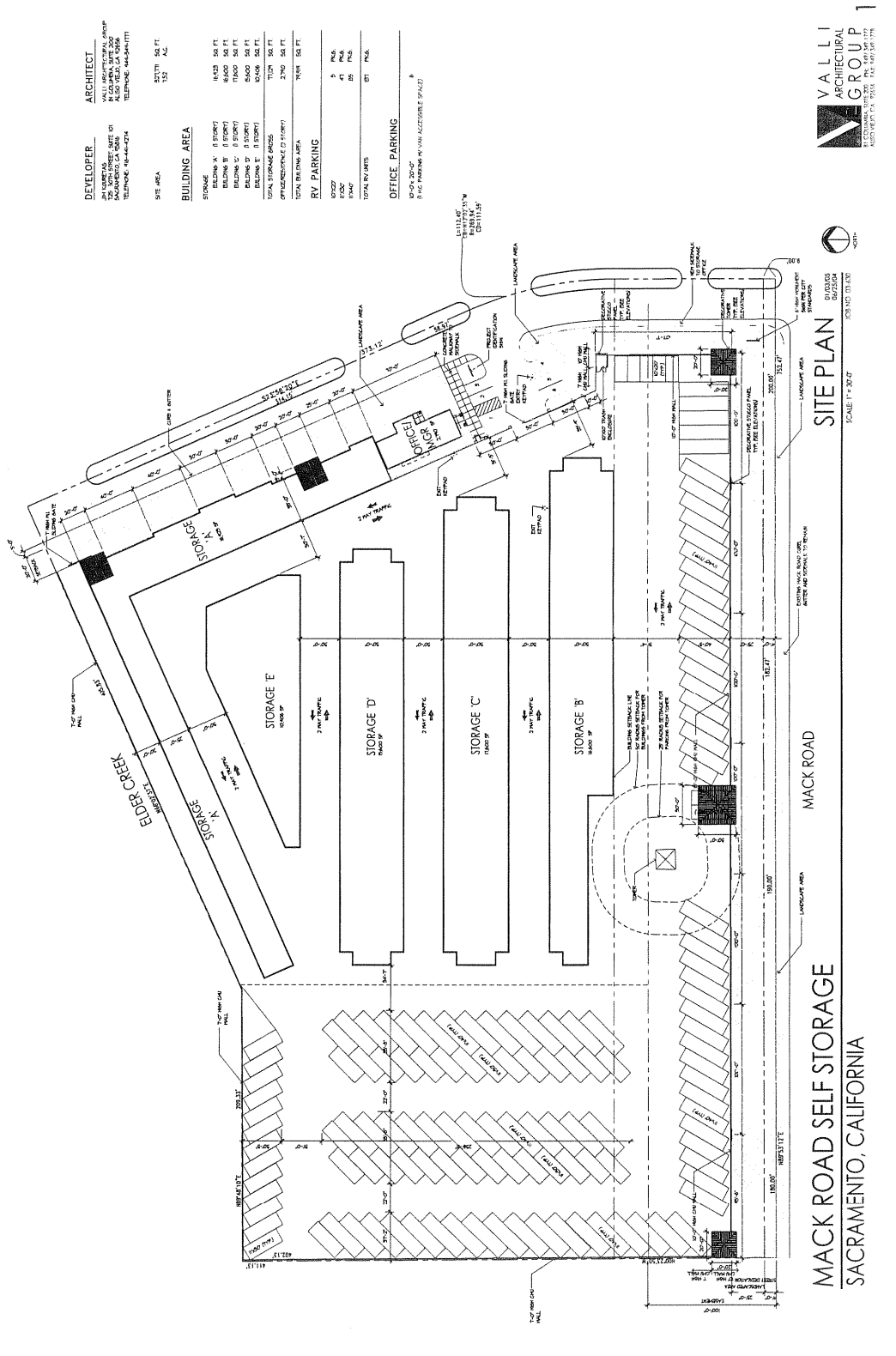
The Plan includes mitigation for Cultural Resources. The intent of the Plan is to prescribe and enforce a means for properly and successfully implementing the mitigation measures as identified within the Initial Study for this project. Unless otherwise noted, the cost of implementing the mitigation measures as prescribed by this Plan shall be funded by the owner/developer identified above. This Mitigation Monitoring Plan (MMP) is designed to aid the City of Sacramento in its implementation and monitoring of mitigation measures adopted for the proposed project.

The mitigation measures have been taken from the Initial Study and are assigned the same number they have in the document. The MMP describes the actions that must take place to implement each mitigation measure, the timing of those actions, and the entities responsible for implementing and monitoring the actions. The developer will be responsible for fully understanding and effectively implementing the mitigation measures contained with the MMP. The City of Sacramento will be responsible for ensuring compliance.

MACK ROAD MINI STORAGE (P04-146)  
MITIGATION MONITORING PLAN

| Mitigation Measure  | VERIFICATION OF COMPLIANCE  |                                      |  |  |
|---|-----------------------------|--------------------------------------|--|--|
|   | Implementing Responsibility | Monitoring Responsibility            | Compliance Standards   | Timing Verification of Compliance (Initials/Date)                                  |
| <p><b>14. Cultural Resources:</b></p> <p><b>CR-1.</b> If subsurface archaeological or historical remains are discovered during construction, work in the area shall stop immediately and a qualified archaeologist and a representative of the Native American Heritage Commission shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less-than-significant level before construction continues.</p> <p><b>CR-2.</b> If human burials are encountered, all work in the area shall stop immediately and the Sacramento County Coroner's office shall be notified immediately. If the remains are determined to be Native American in origin, both the Native American Heritage Commission and any identified descendants must be notified and recommendations for treatment solicited (CEQA Section 15064.5); Health and Safety Code Section 7050.5; Public Resources Code Section 5097.94 and 5097.98.</p> | Applicant / Developer       | City Development Services Department | Mitigation Measures shall be included on the Map and within the Standard Construction Specifications | Measures shall be implemented in field during grading and construction activities. |

Exhibit 1A – Site Plan



**SITE PLAN**  
SCALE: 1" = 20'-0"

**MACK ROAD SELF STORAGE**  
SACRAMENTO, CALIFORNIA

Exhibit 1B - Landscaping Plan

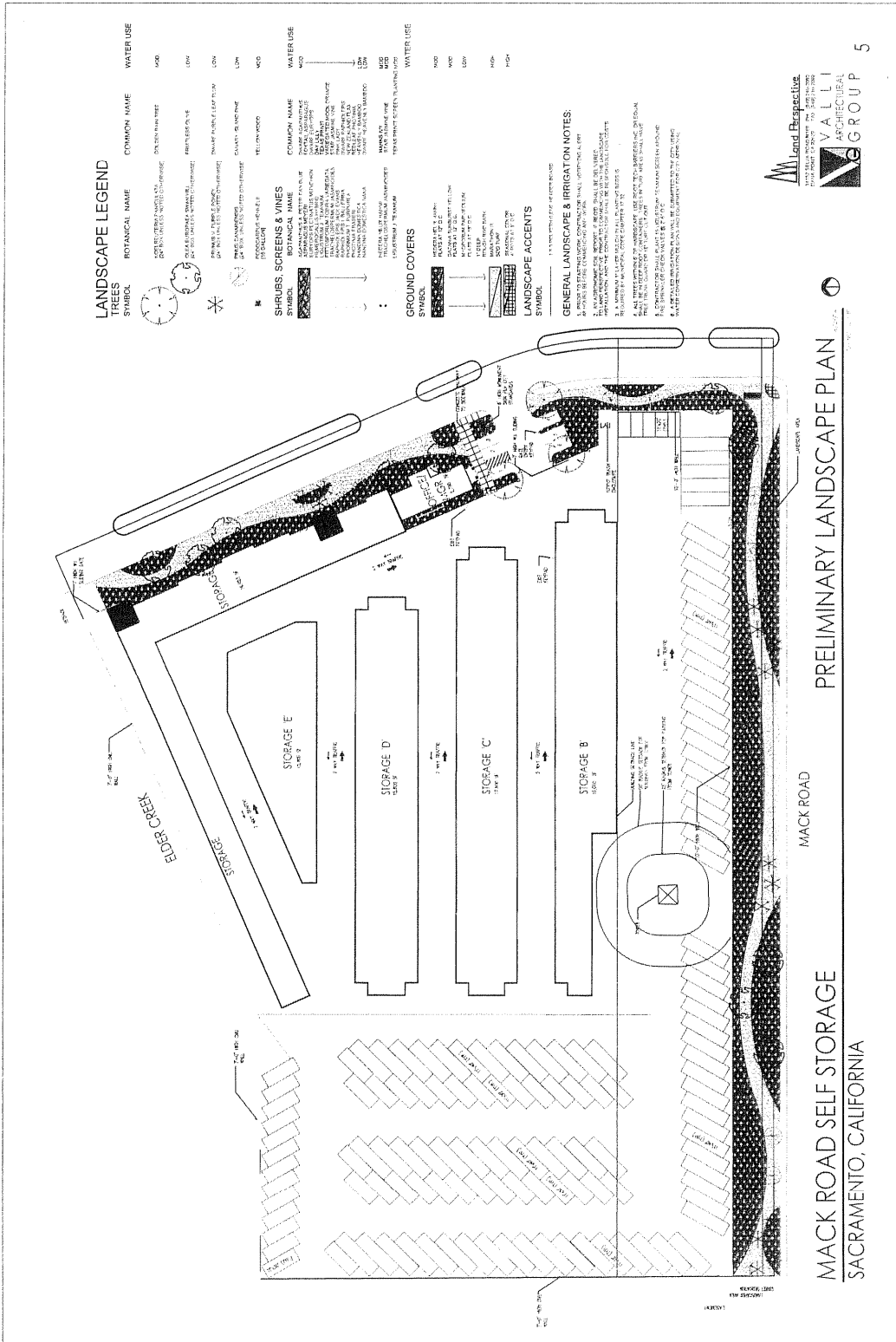
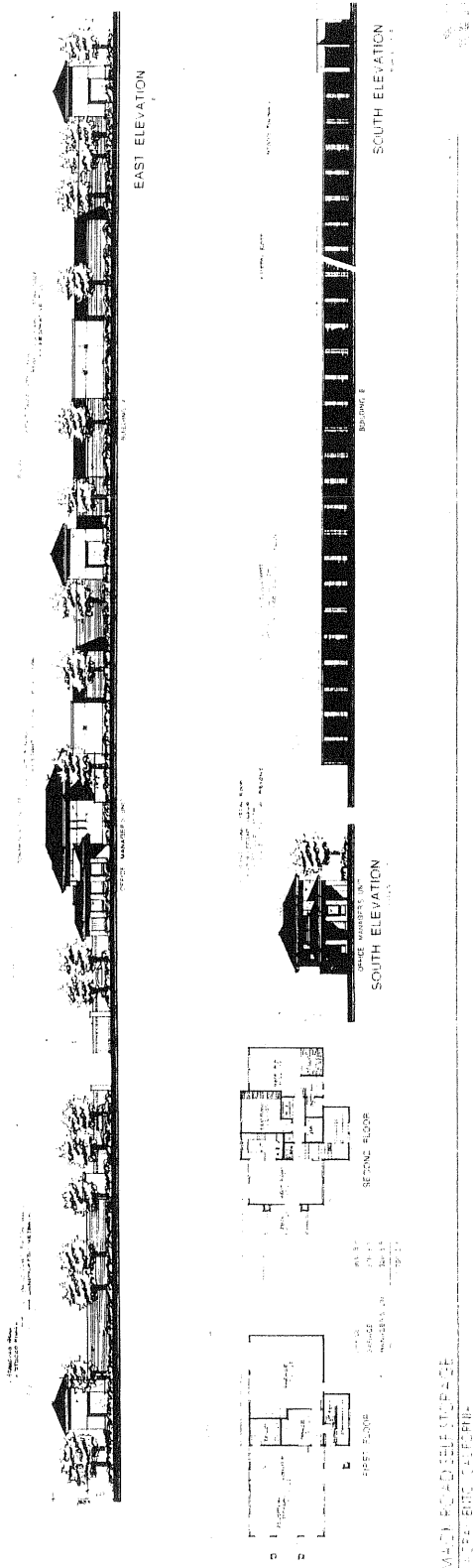
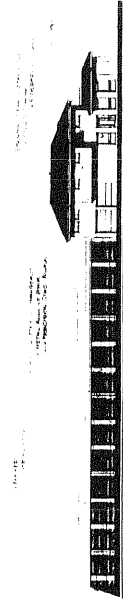


Exhibit 1C – Elevations





MACK ROAD SOUTH ELEVATION



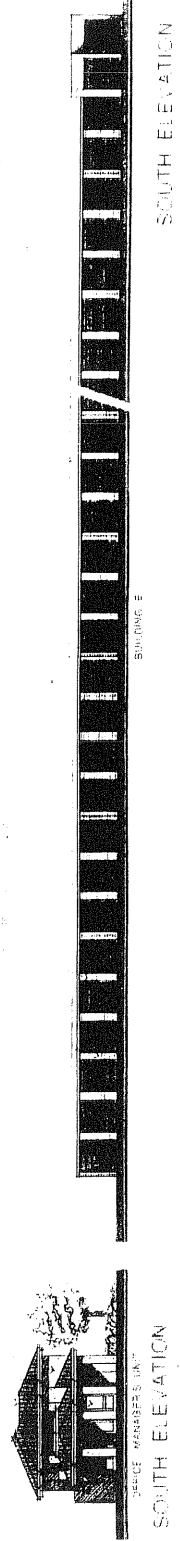
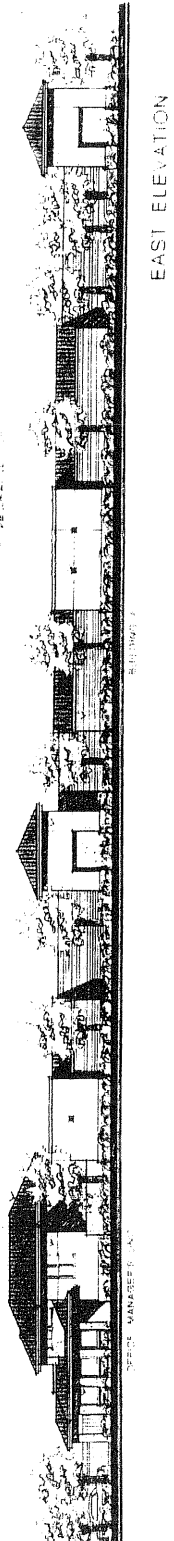
MACK ROAD WEST ELEVATION



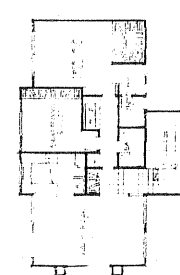
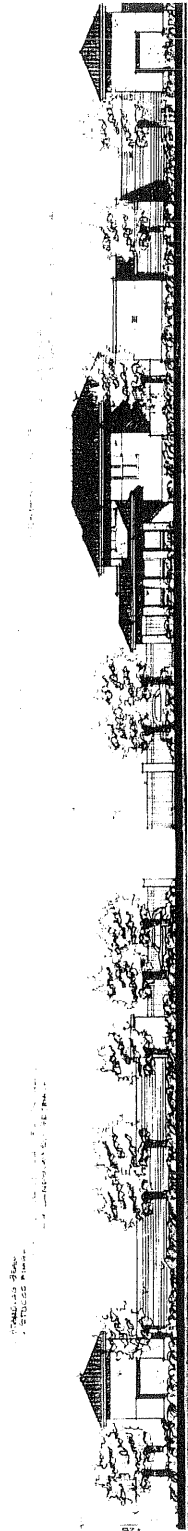
MACK ROAD NORTH ELEVATION

MACK ROAD SELF STORAGE  
PROPERTY CATEGORY

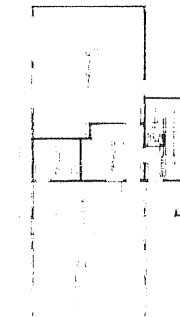




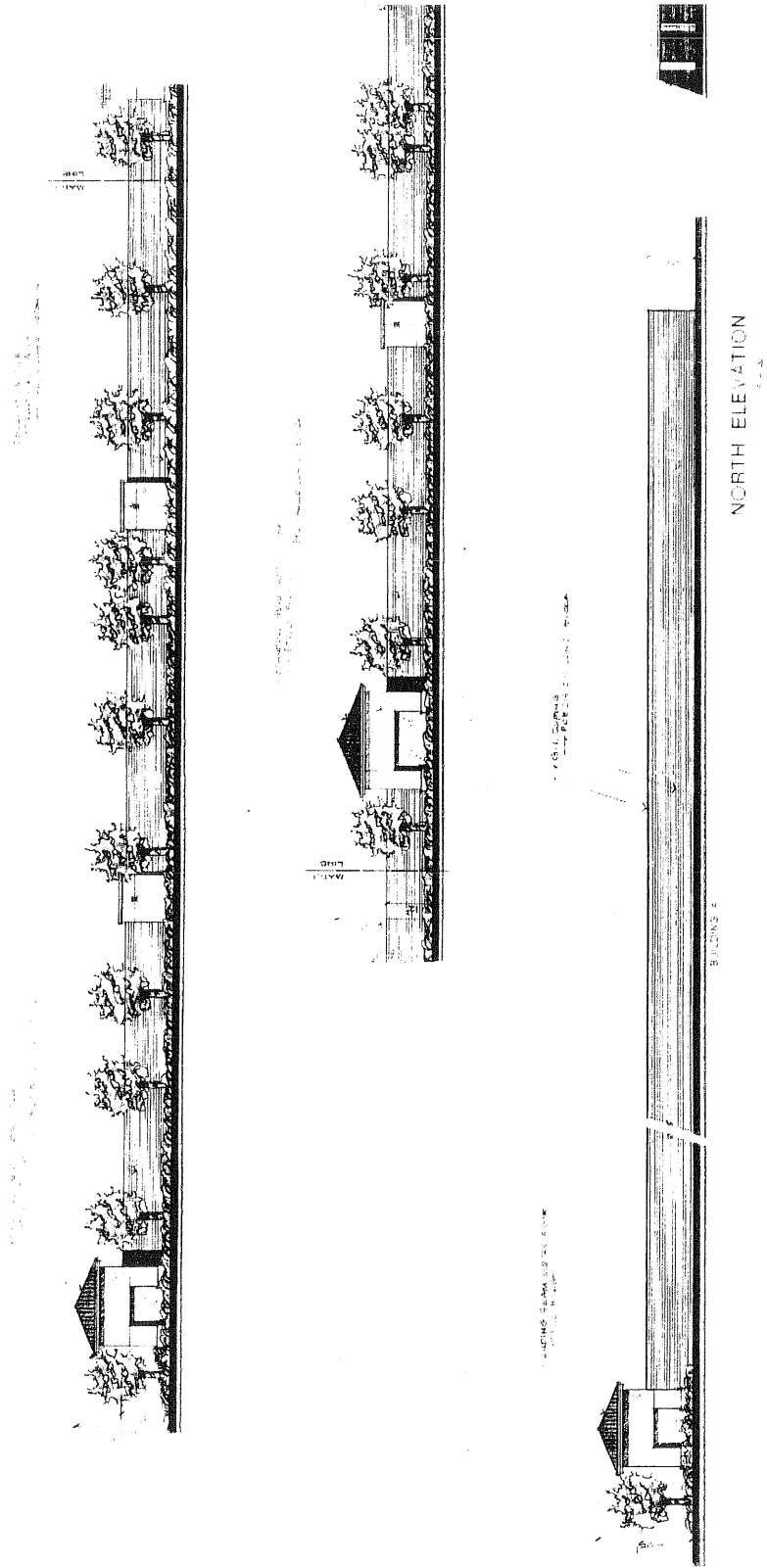




SOUTH ELEVATION



Architectural text or notes at the bottom of the page, oriented vertically.



MACI ROAD SELF STORAGE  
CERRITOS, CALIFORNIA



MAC K ROAD SOUTH ELEVATION

1/4" = 1'-0"  
DATE: 03/10/05  
DRAWN BY: [illegible]  
CHECKED BY: [illegible]



NORTH ELEVATION



WEST ELEVATION





Exhibit 1E - 65402 Exhibit

**PLOTTED EASEMENTS**

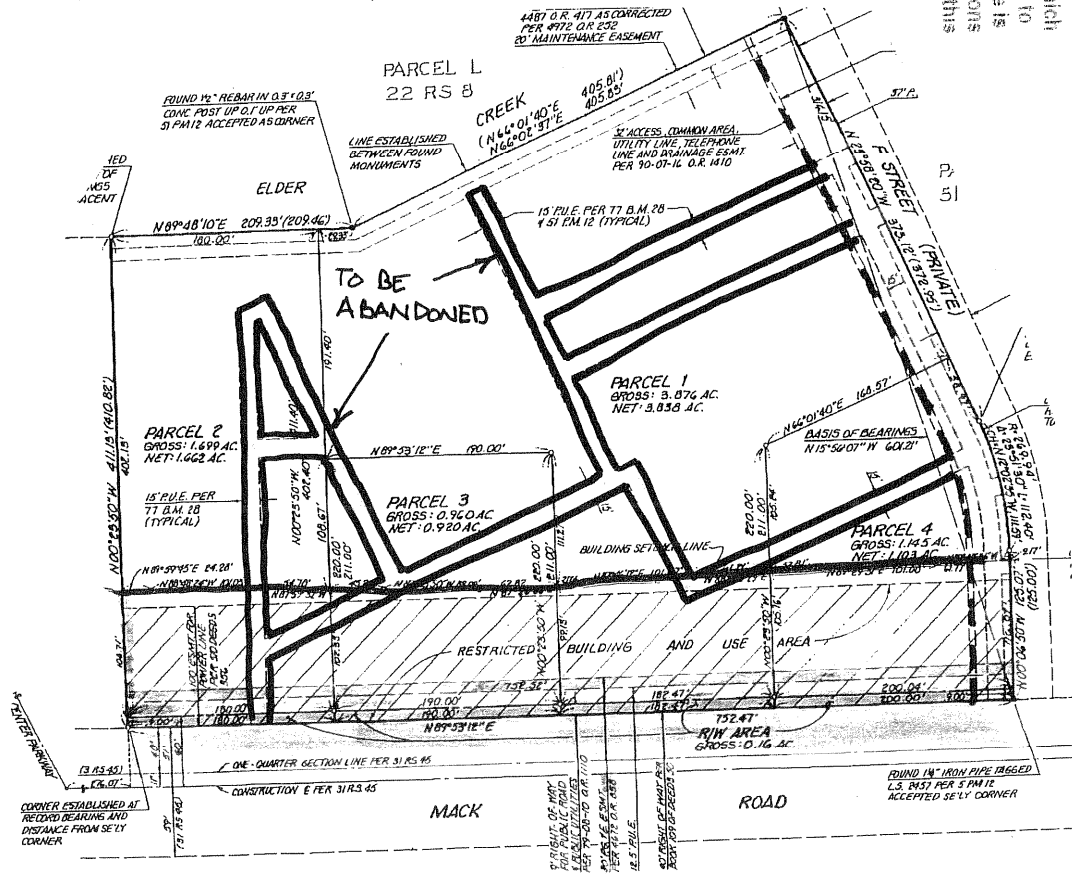
02-11-04

ORDER NO: 71808  
SACRAMENTO COUNTY

A.P.N. 118-0111-62, 63, 64, 65

| ITEM REFERENCE | PURPOSE      | LOCATION                            |
|----------------|--------------|-------------------------------------|
| 8              | 109-50 dds   | (right of way)                      |
| 9              | 510-356 dds  | (electrical facilities)             |
| 10             | 4487-417     | (slope grading)                     |
| 11             | 4672-358     | (electrical facilities) AND         |
| 12             | 5312-246     | (electric facilities)               |
| 13             | 7908-10 1110 | (road and utilities)                |
| 14             | 8310-24 1017 | (ingress and egress)                |
| 16             | MAP 139/1    | (public utilities) and              |
|                |              | (right of way)                      |
|                |              | (20 ft maintenance)                 |
|                |              | (12.50 ft planting and maintenance) |
|                |              | (restricted building and use)       |
| 17             | MAP 139/1    | (building setback)                  |

This map may or may not be a survey of the land depicted hereon. That American expressly disclaims any liability for loss of damage which may result from reliance on this map except to the extent set forth for such loss of damage is expressly provided by the terms and provisions of the title insurance policy, if any, to which this map is attached.



**ATTACHMENT E**  
**Initial Study/Mitigated Negative Declaration**  
**for Mack Road Storage Facility (P04-146)**



DEVELOPMENT SERVICES  
DEPARTMENT

**CITY OF SACRAMENTO**  
CALIFORNIA

1231 I STREET  
ROOM 300  
SACRAMENTO, CA  
95814-2998

PLANNING DIVISION

ENVIRONMENTAL PLANNING  
SERVICES  
916-808-5842  
FAX 916-264-5328

### MITIGATED NEGATIVE DECLARATION

The City of Sacramento, California, a municipal corporation, does hereby prepare, make declare, and publish this Negative Declaration for the following described project:

**P04-146 Mack Road Mini Storage** project consists of entitlements to develop a 79,919± square foot mini storage on 7.52± gross acres in the General Commercial Review (C-2-R) Zone in the South Sacramento Community Plan area. Specific entitlements include:

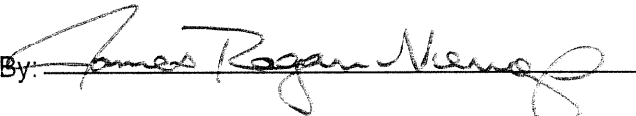
- A. **Special Permit** to construct a 79,919 square foot mini storage with 137 RV parking spaces in General Commercial Review (C-2-R) zone;
- B. **Parcel Merger** to merge 4 lots totaling 7.52± gross acres into 2 lots.

The City of Sacramento, Development Services Department, has reviewed the proposed project and on the basis of the whole record before it, has determined that there is no substantial evidence that the project, with mitigation measures as identified in the attached Initial Study, will have a significant effect on the environment. This Mitigated Negative Declaration reflects the lead agency's independent judgment and analysis. An Environmental Impact Report is not required pursuant to the Environmental Quality Act of 1970 (Sections 21000, et seq., Public Resources Code of the State of California).

This Negative Declaration has been prepared pursuant to Title 14, Section 15070 of the California Code of Regulations; the Sacramento Local Environmental Regulations (Resolution 91-892) adopted by the City of Sacramento; and the Sacramento City Code.

A copy of this document and all supportive documentation may be reviewed or obtained at the City of Sacramento, Development Services Department, Planning Division, 1231 I Street, 3rd Floor, Sacramento, California 95814.

Environmental Services Manager, City of Sacramento,  
California, a municipal corporation

By:  12/14/04

MACK ROAD MINI STRORAGE (P04-146)  
INITIAL STUDY/MITIGATED NEGATIVE DECLARATION

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**MACK ROAD MINI STORAGE (P04-146)  
INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

This Initial Study has been prepared by the Development Services Department, Environmental Planning Services, 1231 I Street, Room 300, Sacramento, CA 95814, pursuant to Title 14, Section 15070 of the California Code of Regulations; the Sacramento Local Environmental Regulations (Resolution 91-892) adopted by the City of Sacramento, and the Sacramento City Code.

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This Initial Study is organized into the following sections:

**SECTION I. - BACKGROUND:** Page 3 - Provides summary background information about the project name, location, sponsor, when the Initial Study was completed, and a project introduction.

**SECTION II. - PROJECT DESCRIPTION:** Page 5 - Includes a detailed description of the Proposed Project.

**SECTION III. - ENVIRONMENTAL CHECKLIST AND DISCUSSION:** Page 6 - Contains the Environmental Checklist form together with a discussion of the checklist questions. The Checklist Form is used to determine the following for the proposed project: 1) "Potentially Significant Impacts" that may not be mitigated with the inclusion of mitigation measures, 2) "Potentially Significant Impacts Unless Mitigated" which could be mitigated with incorporation of mitigation measures, and 3) "Less-than-significant Impacts" which would be less-than-significant and do not require the implementation of mitigation measures.

**SECTION IV. - ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:** Page 43 - Identifies which environmental factors were determined to have either a "Potentially Significant Impact" or "Potentially Significant Impacts Unless Mitigated," as indicated in the Environmental Checklist.

**SECTION V. - DETERMINATION:** Page 44 - Identifies the determination of whether impacts associated with development of the Proposed Project are significant, and what, if any, additional environmental documentation may be required.

**ATTACHMENT**      **A** – Vicinity Map/Location Map  
                             **B** -- Project Plan



## SECTION I. BACKGROUND

File Number, Project Name:

P04-146, Mack Road Mini Storage

Project Location:

The project is located east of Center Parkway at 5701 Mack Road in the South Sacramento Community Plan area of the City of Sacramento (APNs: 118-0111-062, -063, -064, -065)

Project Applicant, Project Planner, and Environmental Planner Contact Information:

Project Applicant

Bob Nolasco  
P.O. Box 15263  
Sacramento, CA 95851-0263

Project Planner

Antonio Ablog, Assistant Planner  
City of Sacramento, Development Services Department  
1231 I Street, Room 300  
Sacramento, CA 95814  
(916) 808-7702

Environmental Planner

Scott Johnson, Assistant Planner  
City of Sacramento, Development Services Department  
1231 I Street, Room 300  
Sacramento, CA 95814  
(916) 808-5842

**Date Initial Study Completed:** November 2004

### Introduction

The proposed project consists of entitlements to develop a 79,919 square foot mini storage with 137 RV parking spaces on 7.52± gross acres in the South Sacramento Community Plan area of the City of Sacramento, Sacramento County.

The City of Sacramento, as lead agency, has determined that the appropriate environmental document for the proposed project is a Mitigated Negative Declaration. This environmental document examines project effects which are identified as potentially significant effects on the

MACK ROAD MINI STORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

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environment or which may be substantially reduced or avoided by the adoption of revisions or conditions to the design of project specific features. It is believed at this time that the project will not result in potentially significant impacts. Therefore, a Mitigated Negative Declaration is the proposed environmental document for this project.

The City is soliciting views of interested persons and agencies on the content of the environmental information presented in this document. Due to the time limits mandated by state law, your response must be sent at the earliest possible date, but no later than the 20-day review period ending January 24, 2005.

Please send written responses to:

Scott Johnson, Environmental Project Manager  
Planning and Building Department  
Environmental Planning Services  
1231 I Street, Ste. 300  
Sacramento, CA 95814  
Fax (916) 264-7185

## SECTION II. PROJECT DESCRIPTION

### Project Location

The project is located east of Center Parkway at 5701 Mack Road in the South Sacramento Community Plan area of the City of Sacramento (APNs: 118-0111-062, -063, -064, -065)

### Project Background

The project site is vacant undeveloped land within the South Sacramento Community Plan area. The site was once approved for a condo project, as part of an additional phase of the development adjacent to the east of the site. This development occurred and subsequently the land remained vacant for a number of years.

### Project Purpose

The Applicant proposes to subdivide the property and develop a mini storage facility on 7.52± acres.

### Project Components

The proposed project consists of entitlements to develop a 79,919± square foot mini storage on 7.52± gross acres in the General Commercial Review (C-2-R) Zone in the South Sacramento Community Plan area. Specific entitlements include:

- A. **Special Permit** to construct a 79,919 square foot mini storage with 137 RV parking spaces in General Commercial Review (C-2-R) zone;
- B. **Parcel Merger** to merge 4 lots totaling 7.52± gross acres into 2 lots.

**SECTION III. ENVIRONMENTAL CHECKLIST AND DISCUSSION**

| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <b>1. LAND USE</b><br><i>Would the proposal:</i>  |                                |   |                              |
| A) Result in a substantial alteration of the present or planned use of an area?   |                                |   | ✓                            |
| B) Affect agricultural resources or operation (e.g., impacts to soils or farmlands, or impact from incompatible land uses?) |                                |   | ✓                            |

**Environmental Setting**

The City of Sacramento General Plan identifies the site as Community/Neighborhood Commercial & Offices. The South Sacramento Community Plan identifies the site as General Commercial. The site is zoned General Commercial Review (C-2-R) zone.

The project site is presently vacant. Immediately north of the site is the channelized portion of Elder Creek, beyond the creek is existing single-family residential units. To the east of the site is existing multi-family residential. South of Mack road there is also existing multi-family residential. To the west of the site is a strip commercial use.

**Standards of Significance**

For the purposes of this analysis, an impact is considered significant if the project would:

- Substantially change land use of the site;
- Be incompatible with long-term uses on adjacent properties; or
- Conflict with applicable land use plans.

**Answers to Checklist Questions**

**Questions A and B**

The proposed project consisting of developing a 79,919 square foot mini storage facility on 7.52 gross acres in the C-2-R Zone, would be consistent with the land use designation of the site and would not result in a substantial alteration of the designated land use of the site. The proposed project is allowed in the C-2-R zone with a Special Permit. The project is consistent with both the General Plan and SSCP designations for the site.

MACK ROAD MINI STORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

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The project site is not currently in agricultural use. Therefore, a less-than-significant impact on land use would occur.

**Mitigation Measures**

No mitigation is required.

**Findings**

The proposed project would not result in impacts to land uses.

| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <b>2. POPULATION AND HOUSING</b>  |                                |   |                              |
| <i>Would the proposal:</i>  |                                |   |                              |
| A) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? |                                |   | ✓                            |
| B) Displace existing housing, especially affordable housing?  |                                |   | ✓                            |

**Environmental Setting**

The City of Sacramento General Plan identifies the site as Community/Neighborhood Commercial & Offices. The South Sacramento Community Plan identifies the site as General Commercial. The site is zoned General Commercial Review (C-2-R) zone.

The project site is presently vacant. Immediately north of the site is the channelized portion of Elder Creek, beyond the creek is existing single-family residential units. To the east of the site is existing multi-family residential. South of Mack road there is also existing multi-family residential. To the west of the site is a strip commercial use.

**Standards of Significance**

Section 15131 of the California Environmental Quality Act (CEQA) Guidelines states that the

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economic or social effects of a project shall not be treated as a significant effect on the environment. However, CEQA indicates that social and economic effects be considered in an EIR only to the extent that they would result in secondary or indirect adverse impacts on the physical environment.

This environmental document does not treat population/housing as an environmental impact, but rather as a social-economic impact. If there are clear secondary impacts created by a population/housing increase generated by the project, those secondary impacts will be addressed in each affected area (e.g., transportation, air quality, etc).

For the purposes of this analysis, an impact is considered significant if the project would induce substantial growth that is inconsistent with the approved land use plan for the area or displace existing affordable housing.

### **Answers to Checklist Questions**

#### **Questions A & B**

The proposed project consisting of developing a 79,919 square foot mini storage facility on 7.52 gross acres in the C-2-R Zone, would be consistent with the land use designation of the site and would not result in a substantial alteration of the designated land use of the site. The proposed project is allowed in the C-2-R zone with a Special Permit. The project is consistent with both the General Plan and SSCP designations for the site. As a result the proposed project would not induce substantial growth to the area, nor would the proposed project displace any housing. Since the project site is presently vacant, no existing housing would be displaced due to the proposed project. Therefore, the proposed project is anticipated to have a less-than-significant impact on population and housing.

#### **Mitigation Measures**

No mitigation is required.

#### **Finding**

The proposed project would result in less-than-significant impacts to population and housing.

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| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>3. SEISMICITY, SOILS, AND GEOLOGY</b>   |                                |   |                              |
| <i>Would the proposal result in or expose people to potential impacts involving:</i> |                                |   |                              |
| A) Seismic hazards?  |                                |   | ✓                            |
| B) Erosion, changes in topography or unstable soil conditions?                       |                                |   | ✓                            |
| C) Subsidence of land (groundwater pumping or dewatering)?                           |                                |   | ✓                            |
| D) Unique geologic or physical features?   |                                |   | ✓                            |

**Environmental Setting**

*Seismicity.* The Sacramento General Plan Update (SGPU) Draft Environmental Impact Report (DEIR) identifies all of the City of Sacramento as being subject to potential damage from earthquake groundshaking at a maximum intensity of VIII of the Modified Mercalli scale (SGPU DEIR, 1987, T-16). No active or potentially active faults are known to cross within close proximity to the project site.

*Topography.* Terrain in the City of Sacramento features very little relief (SGPU, DEIR, 1987, T-3). The potential for slope instability within the City of Sacramento is minor due to the relatively flat topography of the area.

*Regional Geology.* The surface geology of the project site consists of Pleistocene Alluvium (Victor Formation). The Victor Formation forms a broad plain between the Sacramento River and the foothills of the Sierra Nevada mountains (SGPU DEIR, T-1). It is a complex mixture of consolidated, ancient river-borne sediments of all textures (SGPU DEIR, T-1). Weathering subsequent to formation during the Ice Ages has typically caused a hardpan layer to develop near the surface, generally allowing only a moderate-to-low rate of rainwater infiltration (SGPU DEIR, T-1).

The general soils of the area consist of San Joaquin-Galt, which are moderately deep, well-drained soils that are underlain by a cemented hardpan and moderately well-drained soils that have a clayey texture (SGPU DEIR, T-4).

## **Standards of Significance**

For the purposes of this analysis, an impact is considered significant if it allows a project to be built that will either introduce geologic or seismic hazards by allowing the construction of the project on such a site without protection against those hazards.

## **Answers to Checklist Questions**

### **Question A**

Cities in California are required to consider seismic safety as part of the General Plan safety elements. The City of Sacramento also recognizes that it is prudent for the City to prepare for seismic related hazards and has, therefore, adopted policies as a part of the General Plan, Health and Safety Element. These policies require that the City protect lives and property from unacceptable risk due to seismic and geologic activity or unstable soil conditions to the maximum extent feasible, that the City prohibit the construction of structures for permanent occupancy across faults, that soils reports and geologic investigations be required for multiple story buildings, and that the Uniform Building Code requirements that recognize State and Federal earthquake protection standards in construction be used. The policies listed above are implemented through the building permit for new construction projects and reduce the potential significant health and safety impacts. Thus, for the purposes of this environmental analysis, the potential for a significant geologic, soils, or seismic impact created by construction of the project has been substantially lessened by the use of regulatory requirements. Because the project must comply with these regulatory requirements, seismic hazards are considered to be less-than-significant.

### **Question B**

Title 15, Chapter 15.88 of the City's Municipal Code requires a grading permit be obtained prior to construction activities. In accordance with the grading permit requirements, the applicant must submit an Erosion and Sediment Control (ESC) plan to reduce the amount of erosion and retain sediment on the project site. In addition, the Sacramento General Plan Update Draft Environmental Impact Report indicates that there are no highly erodible soils within the City (T-13). For these reasons, the proposed project would not result in substantial soil erosion or loss of topsoil, and geotechnical impacts related to erosion and soil loss would be less than significant.

### **Question C**

According to the SGPU DEIR, no significant subsidence of land had occurred within the City of Sacramento (T-13). State regulations and standards related to geotechnical considerations are reflected in the Sacramento City Code. Construction and design would be required to comply with the latest City-adopted code at the time of construction, including the Uniform Building Code. The code would require construction and design of buildings to meet standards that would reduce risks



associated with subsidence or liquefaction. Since the topography of the area is relatively flat, landslides do not present a hazard in the project site. Therefore, this impact is considered less-than-significant and no mitigation is required.

**Question D**

No unique geologic features exist in close proximity to the project. Therefore, the project would not result in any impacts from or to unique geologic or natural features.

**Mitigation Measures**

No mitigation is required.

**Findings**

The proposed project would not have a significant impact on seismicity, soils, and geology.

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| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <p><b>4. WATER</b><br/><i>Would the proposal result in or expose people to potential impacts involving:</i></p>   |                                |   |                              |
| A) Changes in absorption rates, drainage patterns, or the rate and amount of surface/stormwater runoff (e.g. during or after construction; or from material storage areas, vehicle fueling/maintenance areas, waste handling, hazardous materials handling or storage, delivery areas, etc.)?     |                                |   | ✓                            |
| B) Exposure of people or property to water related hazards such as flooding?  |                                |   | ✓                            |
| C) Discharge into surface waters or other alterations to surface water quality that substantially impact the temperature, dissolved oxygen, turbidity, beneficial uses of receiving waters or areas that provide water quality benefits, or cause harm to the biological integrity of the waters? |                                |   | ✓                            |
| D) Changes in flow velocity or volume of stormwater runoff that cause environmental harm or significant increases in erosion of the project site or surrounding areas?  |                                |   | ✓                            |
| E) Changes in currents, or the course or direction of water movements?  |                                |   | ✓                            |
| F) Change in the quantity of ground waters, either through direct additions or withdrawal, or through interception of an aquifer by cuts or excavations or through substantial loss of recharge capability?   |                                |   | ✓                            |
| G) Altered direction or rate of flow of groundwater?  |                                |   | ✓                            |
| H) Impacts to groundwater quality?  |                                |   | ✓                            |

## Environmental Setting

*Drainage/Surface Water.* There is existing on-site drainage located throughout the site resulting from earlier site improvements. The existing drainage will be required to be abandoned.

There is no surface water on the project site. The nearest surface waters are the channelized Elder Creek, which is located immediately to the north of the site.

*Water Quality.* The City's municipal water is received from the American River and Sacramento River. The water quality of the American River is considered very good. The Sacramento River water is considered to be of good quality, although higher sediment loads and extensive irrigated agriculture upstream of Sacramento tends to degrade the water quality. During the spring and fall, irrigation tailwaters are discharged into drainage canals that flow to the river. In the winter, runoff flows over these same areas. In both instances, flows are highly turbid and introduce large amounts of herbicides and pesticides into the drainage canals, particularly rice field herbicides in May and June. The aesthetic quality of the river is changed from relatively clear to turbid from irrigation discharges.

The Central Valley Regional Water Quality Control Board (RWQCB) has primary responsibility for protecting the quality of surface and groundwaters within the City. The RWQCB's efforts are generally focused on preventing either the introduction of new pollutants or an increase in the discharge of existing pollutants into bodies of water that fall under its jurisdiction.

The RWQCB is concerned with all potential sources of contamination that may reach both these subsurface water supplies and the rivers through direct surface runoff or infiltration. Storm water runoff is collected in City drainage facilities and is sent directly to the Sacramento River. RWQCB implements water quality standards and objectives that are in keeping with the State of California Standards.

The City of Sacramento has obtained a National Pollution Discharge Elimination System (NPDES) permit from the State Water Resources Control Board under the requirements of the Environmental Protection Agency and Section 402 of the Clean Water Act. The goal of the permit is to reduce pollutants found in storm runoff. The general permit requires the permittee to employ BMPs before, during, and after construction. The primary objective of the BMPs is to reduce non-point source pollution into waterways. These practices include structural and source control measures for residential and commercial areas, and BMPs for construction sites. BMP mechanisms minimize erosion and sedimentation, and prevent pollutants such as oil and grease from entering the storm water drains. BMPs are approved by Department of Utilities before beginning construction (the BMP document is available from the Department of Utilities, Engineering Services Division, 1395 35th Avenue, Sacramento, CA). Components of BMPs include:

- Maintenance of structures and roads;
- Flood control management;

- Comprehensive development plans;
- Grading, erosion and sediment control ordinances;
- Inspection and enforcement procedures;
- Educational programs for toxic material management;
- Reduction of pesticide use; and
- Site-specific structural and non-structural control measures.

*Flooding.* The Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) that the project site is within the Flood Zone X. Flood Zone X is defined as: Areas of 500-year flood; areas of 100-year flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 100-year flood.

### **Standards of Significance**

*Surface/Ground Water.* For purposes of this environmental document, an impact is considered significant if the proposed project would substantially degrade water quality and violate any water quality objectives set by the State Water Resources Control Board, due to increased sediments and other contaminants generated by consumption and/or operation activities.

*Flooding.* Substantially increase exposure of people and/or property to the risk of injury and damage in the event of a 100-year flood.

### **Answers to Checklist Questions**

#### **Questions A, C, D and E**

The proposed project consists of developing a 79,919 square foot mini storage on 7.52± gross acres, which would include more paved areas so absorption rates, drainage patterns, and the rate and amount of surface runoff would change. There is an existing drainage system that will be required to be abandoned and the proposed project would be required to connect to the City's storm drain system, to the satisfaction of the Department of Utilities. A Drainage Study and shed map is required to design the drainage system and determine if on-site detention storage is required. The site is located within Drainage Shed 67 with a pumping capacity of 0.19 cfs/acre with all pumps operating.

Additionally, the applicant/developer would be required to comply with the City's Grading, Erosion and Sediment Control Ordinance (Title 15). This ordinance requires the applicant to prepare erosion and sediment control plans for both during and post construction of the proposed project, prepare preliminary and final grading plans, and prepare plans to control urban runoff pollution from the project site during construction. This ordinance also requires that a Post Construction Erosion and Sediment Control Plan be prepared to minimize the increase of urban runoff pollution caused by development of the area. Since the project is not served by a regional water quality control facility and is greater than one acre, both source controls and on-site treatment control measures are required. Specific source controls are required for (1) commercial/industrial material storage, (2) commercial/industrial outdoor loading/unloading of materials, (3) commercial/industrial vehicle and equipment fueling, (4) commercial/industrial vehicle and

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equipment maintenance, repair and washing, (5) commercial/industrial outdoor process equipment operations and maintenance and (6) commercial/industrial waste handling. Storm drain maintenance is required at all drain inlets. On-site treatment control measures are also required.

During construction, sediment may contribute to runoff. However, the proposed project is required to comply with the City's Grading, Erosion and Sediment Control Ordinance (Title 15) as described above.

Improvement plans must include the source control and on-site treatment control measures selected for the site. Because the project is required to comply with the City's ordinances, the project impacts to water quality is anticipated to be less-than-significant.

### **General Stormwater Construction Permit**

Additionally, future developments at the site would be required to comply with regulations involving the control of pollution in stormwater discharges under the National Pollutant Discharge Elimination System (NPDES) program (Section 402(p), Clean Water Act). The City has obtained a NPDES permit from the State Water Resources Control Board (SWRCB) under the requirements of the U.S. Environmental Protection Agency (USEPA) and Section 402 of the Clean Water Act. The regulations, which apply to a new construction projects affecting more than one acre that would not involve dredging and filling of wetlands, are administered by the SWRCB on behalf of the USEPA. Under the program, the developer would file a Notice of Intent with the SWRCB to obtain a General Construction Activity Storm Water Permit prior to construction of the proposed project.

The developer would be required to prepare a Stormwater Pollution Prevention Plan (SWPPP), which would include information on runoff, erosion control measures to be employed, and any toxic substances to be used during construction activities. Surface runoff and drainage would be handled on site. Potential for erosion due to surface water flow would be primarily limited to embankment slopes and areas disturbed by grading during construction. Short-term, construction-related, erosion control would be readily available by means of Best Management Practices (BMPs) (e.g., use of erosion control barriers, synthetic slope covers, hydroseeding, etc.). Long-term erosion control, particularly for embankment slopes, would be available by means of establishing vegetation and controlling surface water flow (e.g., use of crown ditches, paved downdrains, vegetated swales, detention basins, etc.).

The SWRCB requires that the best available technology that is economically achievable, and best conventional pollutant control technology be used to reduce pollutants. These features would be discussed in the SWPPP. A monitoring program would be implemented to evaluate the effectiveness of the measures included in the SWPPP. The RWQCB may review the final drainage plans for the project components.

With compliance of all applicable regulatory requirements, designed to maintain and improve water quality from development activities, the proposed project is anticipated to have a less-than-significant impact on drainage and water quality.

**Question B**

The project site is within the Flood Zone X. Flood Zone X is defined as: Areas of 500-year flood; areas of 100-year flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 100-year flood. Therefore, impacts from 100-year flood events are anticipated to be less-than-significant.

**Questions F – H**

Dewatering or groundwater pumping is not proposed as a part of the project. However, during construction of future development, groundwater may be encountered and may need to be withdrawn. Any groundwater pumping would need to comply with the California Regional Water Quality Control Board (CRWQCB) Standards. Therefore, impacts to groundwater are anticipated to be less-than-significant.

**Findings**

This project would result in less-than-significant impacts to water resources.

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| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>5. AIR QUALITY</b>  |                                |   |                              |
| <i>Would the proposal:</i>   |                                |   |                              |
| A) Violate any air quality standard or contribute to an existing or projected air quality violation? |                                |   | ✓                            |
| B) Exposure of sensitive receptors to pollutants?  |                                |   | ✓                            |
| C) Alter air movement, moisture, or temperature, or cause any change in climate?                     |                                |   | ✓                            |
| D) Create objectionable odors?   |                                |   | ✓                            |

**Environmental Setting**

The project area lies within the Sacramento Valley Air Basin (SVAB). The climate of the SVAB is Mediterranean in character, with mild, rainy winter weather from November through March, and warm to hot, dry weather from May through September. The SVAB is subject to eight unique wind patterns. The predominant annual and summer wind pattern is the full sea breeze, commonly referred to as Delta breezes. Wind direction in the SVAB is influenced by the predominant wind flow pattern associated with the season.

The SVAB is subject to federal, state, and local regulations. Both the federal Environmental Protection Agency and the California Air Resources Board classifies the SVAB as non-attainment for ozone and PM<sub>10</sub> (particulate matter less than 10 microns in diameter). Carbon monoxide (CO) is designated as unclassified/attainment (California Air Resources Board, 1998). The project site is in Sacramento County, under the jurisdiction of the Sacramento Metropolitan Air Quality Management District (SMAQMD). The SMAQMD is responsible for implementing emissions standards and other requirements of federal and state laws.

Air quality within the project area and surrounding region is largely influenced by urban emission sources. As there are minimal industrial emissions, these sources originate primarily from automobiles. Home fireplaces also contribute a significant portion of the air pollutants, particularly during the winter months. Air quality hazards are caused primarily by carbon monoxide (CO), particulate matter (PM<sub>10</sub>), and ozone, primarily as a result of motor vehicles. In 1998, the Sacramento area was within California Environmental Protection Agency attainment

standards for all pollutants except ozone, which exceeded state standards on 42 days of the year. Although the Sacramento Metropolitan Air Quality Management District (SMAQMD) is a non-attainment area for PM<sub>10</sub>, it has not exceeded state or federal standards since 1991 (California Air Resources Board, 1999).

### **Standards of Significance**

*Ozone and Particulate Matter.* An increase of nitrogen oxides (NO<sub>x</sub>) during the construction of the project (short-term effects) above 85 pounds per day would result in a significant impact. An increase of reactive organic gases (ROG) and/or NO<sub>x</sub> during the operation of the project (long-term effects) above 65 pounds per day would result in a significant impact.

*Carbon Monoxide.* The pollutant of concern for sensitive receptors is carbon monoxide (CO). Motor vehicle emissions are the dominant source of CO in Sacramento County (SMAQMD, 1994). For purposes of environmental analysis, sensitive receptor locations generally include parks, sidewalks, transit stops, hospitals, rest homes, schools, playgrounds and residences. Commercial buildings are generally not considered sensitive receptors.

Carbon monoxide concentrations are considered significant if they exceed the 1-hour state ambient air quality standard of 20.0 parts per million (ppm) or the 8-hour state ambient standard of 9.0 ppm (state ambient air quality standards are more stringent than their federal counterparts).

### **Answers to Checklist Questions**

#### **Questions A and B**

Operational Impacts: In order to assess whether mobile source emissions for ozone precursor pollutants (NO<sub>x</sub> and ROG), PM<sub>10</sub> and CO are likely to exceed the standards of significance due to operation of the project once completed, an initial project screening was performed using Table 4.2 in the SMAQMD Guide to Air Quality Assessment (July 2004). This table provides project sizes for land use types which, based on default assumptions for modeling inputs using the URBEMIS2002 model, are likely to result in mobile source emissions exceeding the SMAQMD thresholds of significance for these pollutants. For projects approaching or exceeding the thresholds indicated in the table, a more detailed analysis is required. Those projects which do not approach or exceed the threshold levels in the table can be conservatively assumed not to be associated with significant emissions of NO<sub>x</sub>, ROG, PM<sub>10</sub> and CO.

Projects categorized as "Warehouse" land use development types are considered potentially significant at the NO<sub>x</sub> Screening Level for operational impacts at 2,100,000 square feet or higher. Projects categorized as "Industrial Park" land use development types are considered potentially significant at the NO<sub>x</sub> Screening Level for operational impacts at 1,215,000 square feet or higher. The size of the proposed project is 79,919 square feet, which is well below the Table 4.2 criteria for warehouse and industrial park. Therefore, no potentially significant operational impacts are expected to air quality due to mobile source emissions for these criteria pollutants.



Project-Related Construction Impacts: The project was also screened for potential impacts to air quality due to construction of the proposed project, also using Table 4.2 in the SMAQMD *Guide to Air Quality Assessment* (July 2004) as described above. For projects categorized as "Warehouse" land use development types, 57,000 square feet or larger are considered potentially significant at the NO<sub>x</sub> Screening Level for construction impacts. For projects categorized as "Industrial Park" land use development types, 56,000 square feet or larger are considered potentially significant at the NO<sub>x</sub> Screening Level for construction impacts. The size of the proposed project is 79,919 square feet, which is above the Table 4.2 criteria for warehouse and industrial park. As a result, the *URBEMIS 2002 for Windows 7.4.2* model was used to calculate estimated emissions for the proposed project.

Based on the estimated emissions from the URBEMIS model, the proposed project is not likely to exceed the short-term emissions threshold of 85 lbs/day for NO<sub>x</sub>. Estimated NO<sub>x</sub> emissions using the URBEMIS 2002 model were calculated to be as high as approximately 57± lbs/day, which is below the 85 lbs/day threshold. As a result, the proposed project is anticipated to have a less-than-significant impact on air quality.

Additionally, construction would be required to comply with SMAQMD's Rule 403 on Fugitive Dust and Rule 435 on using compliant asphalt paving materials. Compliance with these rules will further ensure impacts from construction activities will remain less-than-significant.

### **Question C and D**

The project would not result in the alteration of air movement, moisture, temperature, or in any change in climate, either locally or regionally. Objectionable odors emanating from the proposed project are not anticipated. Development of the site and eventual operation will be required to comply with all applicable codes regarding the management of waste products. Therefore, impacts associated with alterations of air movements, moisture, temperature, or change in climate; and objectionable odors are anticipated to be less-than-significant.

### **Mitigation Measures**

No mitigation is required.

### **Findings**

This project would result in a less-than-significant impact to air quality with the incorporation of the above mitigation measures.

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| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>6. TRANSPORTATION/CIRCULATION</b><br><i>Would the proposal result in:</i>   |                                |   |                              |
| A) Increased vehicle trips or traffic congestion?  |                                |   | ✓                            |
| B) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? |                                |   | ✓                            |
| C) Inadequate emergency access or access to nearby uses?   |                                |   | ✓                            |
| D) Insufficient parking capacity on-site or off-site?  |                                |   | ✓                            |
| E) Hazards or barriers for pedestrians or bicyclists?  |                                |   | ✓                            |
| F) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)?                          |                                |   | ✓                            |
| G) Rail, waterborne or air traffic impacts?  |                                |   | ✓                            |

**Environmental Setting**

*Roads.* The major street nearest the project site is Mack Road. Mack Road is a four-lane, east-west arterial (SGPU DEIR, Y-40). Existing Level of Service (LOS) for this segment of Mack Road is listed as E (SGPU DEIR, Y-43). Other streets within the project area consist of Center Parkway which is a secondary, two to four-lane, north-south roadway (SGPU DEIR, Y-41).

*Public Transportation.* Regional Transit is the major public transportation service provider within Sacramento County providing 20.6 miles of light rail service and fixed-route bus service on 77 routes covering a 418 square-mile area, 7 days a week, 365 days a year. Light rail service and many of the bus routes are currently oriented to the downtown area. Currently bus routes 54 and 56 travel along Mack Road adjacent to the project site. The Meadowview Light Rail Station is also approximately 2 miles to the west of the proposed project site.

*Bikeways.* There are existing on-street bike lanes on Mack Road and Center Parkway.

*Parking.* Currently, no parking is available at the project site as it is vacant. Parking is not allowed on Mack Road.

### **Standards of Significance**

**Roadways:** An impact is considered significant for roadways when:

- The project causes the facility to degrade from LOS C or better to LOS D or worse
- For facilities operating at LOS D, E or F without the project, an impact is considered significant if the project increases the v/c ratio by 0.02 or more

**Intersections:** A significant traffic impact occurs under the following conditions:

- The addition of project-generated traffic causes the level of service of the intersection to change from LOS A, B, or C to LOS D, E or F
- The addition of project-generated traffic increases the average stopped delay by five seconds or more at an intersection already operating worse than LOS C

**Bicycle Facilities:** A significant Bikeway impact would occur if:

- The project hindered or eliminated an existing designated bikeway, or if the project interfered with implementation of a proposed bikeway
- The project is to result in unsafe conditions for bicyclists, including unsafe bicycle/pedestrian or bicycle/motor vehicle conflicts

**Pedestrian Facilities:** A significant pedestrian circulation impact would occur if:

- The project would result in unsafe conditions for pedestrians, including unsafe increase in pedestrian/bicycle or pedestrian/motor vehicle conflicts.

**Transit Facilities:** A significant impact to the transit system would occur if the project-generated ridership, when added to existing or future ridership, exceeds available or planned system capacity. Capacity is defined as the total number of passengers the system of busses and light rail vehicles can carry during the peak hour of operation.

**Parking:** A significant impact to parking would occur if the anticipated parking demand of the proposed project exceeds the available or planned parking supply for typical day conditions. However, the impact would not be significant if the project is consistent with the parking requirements stipulated in the City Code.

## **Answers to Checklist Questions**

### **Question A**

The proposed project would generate 13 vehicle trips in the a.m. peak hour and 22 vehicle trips in the p.m. peak hour from the development of the 79,919 square feet mini storage facility. The proposed land uses are consistent with the existing land use designations. The project is expected to result in a less intense use than what was previously approved for the site, which included multi-family residential. In addition, the traffic impacts associated with the land use were analyzed in the SGPU EIR. Trips generated from development of the mini storage are not anticipated to create a significant impact on the existing roadway system based on capacity and demand. Therefore, the proposed project is anticipated to have less-than-significant impacts from increased trips or traffic congestion.

### **Questions B & E**

Public improvements required for the proposed project are or will be designed to appropriate, applicable standards. Therefore, creation of hazards is not expected and no mitigation is required.

The proposed project may increase potential bicycle/pedestrian or bicycle/motor vehicle conflicts. However, the frontage improvements along the project site will include sidewalks to appropriate standards to the satisfaction of the Development Services Department, Development Engineering and Finance Division. Potential impacts arising from bicycle/pedestrian or bicycle/motor vehicle conflicts are therefore considered less-than-significant and no mitigation is required.

### **Question C**

Existing road infrastructure provides adequate emergency access to the proposed project site. The project site shall be designed to appropriate standards, to the satisfaction of the City of Sacramento's the Development Services Department, Development Engineering and Finance Division and Fire Department. Potential emergency access impacts are considered to be less-than-significant and do not require mitigation.

### **Question D**

Inadequate parking is not anticipated to result from the proposed project as the site will be required to comply with the City Zoning Code parking requirements. Additionally, there is space for grading equipment and employees to park on site as grading occurs for subdividing the site and constructing the street. A less-than-significant parking impact is anticipated.

**Question F**

No policies concerning alternative forms of transportation would be impacted as the site is proposed for development of a mini storage facility consistent with the designated land use. There is an existing bus stop adjacent to the project on Mack Road. Additionally, the proposed project is not anticipated to impact the existing bike lanes on Mack Road. Therefore, a less-than-significant impact is anticipated.

**Question G**

The project would not result in waterborne or air traffic impacts because the project improvements would be contained within the project site and would be at ground-level. There are no railroad tracks within the project site, so impacts to rail would also be less-than-significant.

**Mitigation Measures**

No mitigation is required.

**Findings**

The project would not result in significant impacts to transportation or circulation.

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| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <b>7. <u>BIOLOGICAL RESOURCES</u></b>   |                                |   |                              |
| <i>Would the proposal result in impacts to:</i>   |                                |   |                              |
| A) Endangered, threatened or rare species or their habitats (including, but not limited to plants, fish, insects, animals and birds)? |                                |   | ✓                            |
| B) Locally designated species (e.g., heritage or City street trees)?  |                                |   | ✓                            |
| C) Wetland habitat (e.g., marsh, riparian and vernal pool)?   |                                |   | ✓                            |

**Environmental Setting**

The proposed project site is located on 7.52± vacant acres that are partially disturbed with segments of asphalt driveways located throughout the site. The site is surrounded by multi-family residential to the east and south, single-family to the north, and commercial to the west. The City of Sacramento General Plan Update identified 10 habitat types in the City of Sacramento. Of the ten habitat types identified in the General Plan, six represent intact natural communities; the remaining four are a result of human habitation. The project site can be characterized as “old field habitat”, a habitat that occurs as a result of human habitation (SGPU DEIR, U-12). Old field habitat develops in “abandoned” fields previously used for dry land or irrigated farming, or which are heavily disturbed by other uses. After cessation of farming or disturbance, these sites slowly return to an annual grassland habitat. Loss of this habitat in the SGPU area is considered less-than-significant (SGPU DEIR, U-28).

The site is vacant and is partially disturbed with existing asphalt driveways located throughout the site. A site visit was conducted on September 15, 2004, to search for any evidence of special status species or other biological resources. No evidence of wetlands or special status species was observed at the project site. The site is surrounded on four sides by development. Elder Creek is also located to the north of the site. Elder Creek is channelized in this area and is within an existing 70± feet wide designated area. With the boundary of this area, the projects northern boundary is approximately 30+ feet away from the channel.

**Special Status Species**

Special Status species are those plants and animals that are legally protected under both the California and Federal Endangered Species Acts (FESA), The Federal Migratory Bird Treaty Act, or the Fish and Game Code of California. In addition, special status species include those

species that are not currently protected by statute but which are considered rare or endangered under CEQA. Special status species are considered to be those species identified by the scientific community to be sufficiently rare to qualify for such listing.

#### **Wetlands/Waters of the United States**

The U.S. Army Corps of Engineers (Corps) and the United States Environmental Protection Agency (EPA) regulate the discharge of dredge and fill material into "waters of the United States" under Section 404 of the Clean Water Act. Wetlands are defined for regulatory purposes as "those areas that are inundated or saturated by surface or groundwater at a frequency and duration sufficient to support, and that under normal circumstances do support, a prevalence of vegetation typically adapted for life in saturated soil conditions." Wetlands generally include swamps, marshes, bogs, vernal pools, and similar areas (33 CFR 328.3, 40 CFR 230.3).

#### **Standards of Significance**

For purposes of this environmental document, an impact would be significant if any of the following conditions or potential thereof, would result with implementation of the proposed project:

- Creation of a potential health hazard, or use, production or disposal of materials that would pose a hazard to plant or animal populations in the area affected;
- Substantial degradation of the quality of the environment, reduction of the habitat, reduction of population below self-sustaining levels of threatened or endangered species of plant or animal;
- Affect other species of special concern to agencies or natural resource organizations (such as regulatory waters and wetlands); or
- Violate the Heritage Tree Ordinance (City Code 12:64.040).

For the purposes of this report, "special-status" has been defined to include those species, which are:

- Listed as endangered or threatened under the federal Endangered Species act (or formally proposed for, or candidates for, listing);
- Listed as endangered or threatened under the California Endangered Species Act (or proposed for listing);
- Designated as endangered or rare, pursuant to California Fish and Game Code (Section 1901);
- Designated as fully protected, pursuant to California Fish and Game Code (Section 3511, 4700, or 5050);

- Designated as species of concern by U.S. Fish and Wildlife Service (USFWS), or as species of special concern to California Department of Fish and Game (CDFG);
- Plants or animals that meet the definitions of rare or endangered under the California Environmental Quality Act (CEQA);

### **Answers to Checklist Questions**

#### **Question A - C**

The proposed project site consists of approximately 7.52± acres of vacant, partially disturbed land dominated by annual grassland species. The plant species include non-native grassland species. Of the ten habitat types identified in the General Plan, six represent intact natural communities; the remaining four are a result of human habitation. The project site can be characterized as "old field habitat," a habitat that occurs as a result of human habitation (SGPU EIR, U-12). Old field habitat develops in "abandoned" fields previously used for dry land or irrigated farming, or which are heavily disturbed by other uses. After cessation of farming or disturbance, these sites slowly return to an annual grassland habitat. There was no evidence of wetland resources at the site. Loss of this old field habitat in the SGPU area is considered less-than-significant. During the site visit in September of 2004 no wildlife was observed at the site. There are trees located on the project site. Based upon the existing site conditions, the lack of habitat, and the absence of evidence of special status species occurring on site, impacts to special status species are anticipated to be less-than-significant.

#### **Mitigation Measures**

No mitigation is required

#### **Findings**

With the incorporation of the mitigation measures listed above, the proposed project would not result in significant impacts to biological resources.



MACK ROAD MINI STORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>8. ENERGY</b>   |                                |   |                              |
| <i>Would the proposal result in impacts to:</i>  |                                |   |                              |
| A) Power or natural gas?   |                                |   | ✓                            |
| B) Use non-renewable resources in a wasteful and inefficient manner?   |                                |   | ✓                            |
| C) Substantial increase in demand of existing sources of energy or require the development of new sources of energy? |                                |   | ✓                            |

**Environmental Setting**

Pacific Gas and Electric (PG&E) is the natural gas utility for the City of Sacramento. Not all areas are currently provided with gas service. PG&E gas transmission pipelines are concentrated north of the City of Sacramento. Distribution pipelines are located throughout the City, usually underground along City and County public utility easements (PUEs).

The Sacramento Municipal Utility District (SMUD) supplies electricity to the City of Sacramento. SMUD operates a variety of hydroelectric, photovoltaic, geothermal and co-generation powerplants. SMUD also purchases power from PG&E and the Western Area Power Administration. Major electrical transmission lines are located in the northeastern portion of the City of Sacramento.

**Standards of Significance**

*Gas Service.* A significant environmental impact would result if a project would require PG&E to secure a new gas source beyond their current supplies.

*Electrical Services.* A significant environmental impact would occur if a project resulted in the need for a new electrical source (e.g., hydroelectric and geothermal plants).

**Answers to Checklist Questions**

**Questions A - C**

Development of the site would require the use of energy when completed and also during

construction. However, this energy use would not require the development of new sources of energy nor would it result in substantial increases in demand for energy. The proposed project is consistent with the uses that have been anticipated at this location by the Sacramento General Plan Update and the associated EIR. Therefore a less-than-significant impact is expected.

**Mitigation Measures**

No mitigation measures are required.

**Findings**

The project would not result in impacts to energy resources.

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>9. HAZARDS</b>  |                                |   |                              |
| <i>Would the proposal involve:</i>   |                                |   |                              |
| A) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)? |                                |   | ✓                            |
| B) Possible interference with an emergency evacuation plan?  |                                |   | ✓                            |
| C) The creation of any health hazard or potential health hazard?   |                                |   | ✓                            |
| D) Exposure of people to existing sources of potential health hazards?   |                                |   | ✓                            |
| E) Increased fire hazard in areas with flammable brush, grass, or trees?   |                                |   | ✓                            |

**Environmental Setting**

The project site is vacant with existing asphalt driveway segments and weedy plant species. There was some scattered trash and other debris observed throughout the site.

### **Standards of Significance**

For the purposes of this document, an impact is considered significant if the proposed project would:

- expose people (e.g., residents, pedestrians, construction workers) to existing contaminated soil during construction activities;
- expose people (e.g., residents, pedestrians, construction workers) to asbestos-containing materials; or
- expose people (e.g., residents, pedestrians, construction workers) to existing contaminated groundwater during de-watering activities; or
- expose people (e.g., residents, pedestrians, construction workers) to increase fire hazards.

### **Answers to Checklist Questions**

#### **Questions A, C & D**

The project site does not appear to contain any hazardous materials. Grading and/or construction activities could reveal unseen hazards that were remnants of previous uses. In addition, grading activities would involve the use of hazardous substances, such as fuels and oils. The proposed project, however, is required to comply with all federal, state, and local laws concerning hazardous substances. These laws include requirements on what to do if suspected contaminants are discovered during construction and how to handle use of hazardous materials. Because construction activities are required to comply with all applicable health and safety laws, hazards impacts during construction are anticipated to be less-than-significant.

#### **Questions B & E**

Development of the site would reduce the potential for flammable brush as the mini storage facility would eliminate the existing weedy vegetation would be removed. The proposed project is required to meet the Uniform Fire Code standards. Therefore, impacts to fire hazards are considered to be less-than-significant.

#### **Mitigation Measures**

No mitigation is required.

#### **Findings**

The proposed project would result in less-than-significant impacts regarding hazards.

MACK ROAD MINI STRORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>10. NOISE</b>   |                                |   |                              |
| <i>Would the proposal result in:</i>                                     |                                |   |                              |
| A) Increases in existing noise levels?<br>Short-term<br>Long Term        |                                |   | ✓<br>✓                       |
| B) Exposure of people to severe noise levels?<br>Short-term<br>Long Term |                                |   | ✓<br>✓                       |

**Environmental Setting**

The SGPU DEIR indicated that the three major noise sources in the City of Sacramento are surface traffic, aircraft, and the railroad (AA-1). The major noise source in the project area is traffic noise from Mack Road. According to the SGPU DEIR, noise from Mack Road is estimated to be approximately 70 dB Ldn at 75 feet from the centerline (AA-25). Center Parkway is estimated to be 61-62 dB Ldn at 75 feet (SGPU DEIR, AA-25)

**Standards of Significance**

Thresholds of significance are those established by the Title 24 standards and by the City's General Plan Noise Element and the City Noise Ordinance. Noise and vibration impacts resulting from the implementation of the proposed project would be considered significant if they cause any of the following results:

- Exterior noise levels at the proposed project, which are above the upper value of the normally acceptable category for various land uses (SGPU DEIR AA-27) caused by noise level increases due to the project. The maximum normally acceptable exterior community noise exposure for residential backyards it is 60 dB and for residential interior it is 45 dB;
- Residential interior noise levels of 45 L<sub>dn</sub> or greater caused by noise level increases due to the project;
- Construction noise levels not in compliance with the City of Sacramento Noise Ordinance;

MACK ROAD MINI STORAGE (P04-146)  
INITIAL STUDY/MITIGATED NEGATIVE DECLARATION

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- Occupied existing and project residential and commercial areas are exposed to vibration peak particle velocities greater than 0.5 inches per second due to project construction;
- Project residential and commercial areas are exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations; and
- Historic buildings and archaeological sites are exposed to vibration peak particle velocities greater than 0.25 inches per second due to project construction, highway traffic, and rail operations.

Construction-generated sound is exempt from limits if construction activities take place between the hours of 7:00 a.m. and 6:00 p.m. Monday-Saturday and between 9:00 a.m. and 6:00 p.m. on Sundays as specified in Section 8.68.080 of the City of Sacramento Noise Ordinance.

### **Answers to Checklist Questions**

#### **Questions A and B**

The major noise source in the project area is traffic noise from Mack Road. The proposed project may temporarily increase noise in the area due to grading and construction activities. However, the noise would be temporary. In addition, the City of Sacramento Noise Ordinance exempts construction-related noise if the construction takes place between the hours of 7:00 a.m. and 6:00 p.m., on Monday through Saturday, and between 9:00 a.m. and 6:00 p.m. on Sunday. Short-term noise impacts would be less-than-significant with adherence to the Noise Ordinance.

Noise levels from Mack Road are listed at 70 dB Ldn at 75 feet from the centerline (SGPU DEIR, AA-25). The proposed mini storage facility is not anticipated to be impacted from noise generated from Mack Road as the normally acceptable exterior noise levels for this type of use are at 70 dB Ldn. Additionally, the project will be constructing a concrete masonry wall ranging in height from 7 to 12 feet high surrounding the facility. There is one on-site residential unit located within the proposed mini storage facility. The residence is located approximately 250 feet north of Mack Road and will be located within the masonry wall. With the development of the wall, exterior noise levels could be reduced to a less-than-significant level.

#### **Mitigation Measures**

No mitigation is required.

#### **Findings**

With the development of the masonry wall, the proposed project would result in less-than-significant noise impacts.

MACK ROAD MINI STORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>11. PUBLIC SERVICES</b>   |                                |   |                              |
| <i>Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:</i> |                                |   |                              |
| A) Fire protection?  |                                |   | ✓                            |
| B) Police protection?  |                                |   | ✓                            |
| C) Schools?  |                                |   | ✓                            |
| D) Maintenance of public facilities, including roads?  |                                |   | ✓                            |
| E) Other governmental services?  |                                |   | ✓                            |

**Environmental Setting**

The City of Sacramento's approach to locating fire stations is on the basis of a maximum of two mile run to any point in the station's service area (South Sacramento Community Plan, 79). The closest station to the site is Fire Station 7, located approximately ¾ miles south of the project site at 6500 Wyndham Drive. Additionally, Station 57 is located approximately 1½ miles to the north of the site on East Parkway and Franklin Boulevard.

The area is served by the Sacramento City Police Department. There are several existing patrol districts in the area (SSCP, 79). With adjacent areas, outside of the City limits, served by the Sacramento County Sheriffs Department.

Presently, there are two school districts that serve the South Sacramento area. These two school districts include Elk Grove Unified and Sacramento City Unified.

Other public services in the area include library services. The Sacramento Public Library, which serves the area, is a Joint Powers Authority (JPA) and is comprised of both the County of Sacramento and the City of Sacramento.

**Standards of Significance**

For the purposes of this report, an impact would be considered significant if the project resulted in the need for new or altered services related to fire protection, police protection, school facilities, roadway maintenance, or other governmental services.

**Answers to Checklist Questions**

**Questions A - E**

Although government services would be needed to serve the project site, this would not result in the need for an alteration to existing services nor would it result in the need to construct any new facilities to provide the additional services. The proposed project is consistent with the planned uses anticipated by the SGPU and the South Sacramento Community Plan. Therefore, a less-than-significant impact on public services is anticipated.

**Mitigation Measures**

No mitigation is required.

**Findings**

The proposed project would result in less-than-significant impacts to public services.

MACK ROAD MINI STORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>12. UTILITIES</b><br><i>Would the proposal result in the need for new systems or supplies, or substantial alterations to the following utilities:</i> |                                |   |                              |
| A) Communication systems?  |                                |   | ✓                            |
| B) Local or regional water supplies?   |                                |   | ✓                            |
| C) Local or regional water treatment or distribution facilities?   |                                |   | ✓                            |
| D) Sewer or septic tanks?  |                                |   | ✓                            |
| E) Storm water drainage?   |                                |   | ✓                            |
| F) Solid waste disposal?   |                                |   | ✓                            |

**Environmental Setting**

*Telephone.* SBC provides telephone service to the project site and throughout the surrounding area. Telephone service to the project area is provided primarily with aboveground transmission lines.

*Water.* The City has the rights to enough water to supply growth within the City limits until buildout and beyond (South Sacramento Community Plan, 67). City water is provided to areas as they develop (SSCP, 68). After the water distribution facilities within a subdivision have been installed, the City takes over operation and maintenance of the system (SSCP, 68). There is a public system that runs throughout the site that was designed to serve a previous use.

*Stormwater and Sewage Drainage.* The site is located in drainage basin 67. Storm Drainage service is provided by a piped drainage system in Mack Road. There is an existing drainage system throughout the site that will be required to be abandoned. Sanitary sewer service is available to all of South Sacramento. The Sacramento Regional County Sanitation District (SRCSD) provides sewage treatment for the cities of Folsom and Sacramento and County Sanitation District (CSD)-1, which serve the unincorporated urban portions of the County and portions of Sacramento. There is also a public system existing on-site that shall be abandoned.

The SRCSD is responsible for the operation of all regional interceptors and wastewater treatment plants, while local collection districts operate the systems that transport less than 10



million gallons of waste flow daily.

*Solid Waste.* The project is required to meet the City's Recycling and Solid Waste Disposal Regulations (Chapter 17.72 of the Zoning Ordinance). The purpose of the ordinance is to regulate the location, size, and design of features of recycling and trash enclosures in order to provide adequate, convenient space for the collection, storage, and loading of recyclable and solid waste material for existing and new development; increase recycling of used materials; and reduce litter.

### **Standards of Significance**

For purposes of this environmental document, an impact is considered significant if the proposed project would:

- Result in a detriment to microwave, radar, or radio transmissions;
- Create an increase in water demand of more than 10 million gallons per day;
- Substantially degrade water quality;
- Generate more than 500 tons of solid waste per year; or
- Generate storm water that would exceed the capacity of the storm water system.

### **Answers to Checklist Questions**

#### **Question A**

The South Sacramento Community Plan and General Plan Update designate this site as Community/Neighborhood Commercial and Offices. The proposed project is consistent with the intended development for the site. No impact to communications systems is expected.

#### **Questions B, C, and E**

The proposed project will be required to abandon the existing water and drainage facilities on site that were constructed for a previous use. They will then be required to design new private on-site water and drainage facilities and connect to the City's water distribution and drainage systems. All connections to the City's utility system will be done to the satisfaction of the City's Department of Utilities. The City's Department of Utilities has reviewed the proposed project, which is consistent with the South Sacramento Community Plan and SGPU, to ensure that adequate water is available. A less-than-significant impact is expected on the City's water supply, water distribution infrastructure, and drainage facilities.

#### **Question D**

The proposed project site will be required to abandoned the existing sewer system on site from the previous use and will also be required to install the proper sized sewer line to provide connection to the existing public sewer system. All public sewers shall be coordinated with and approved by CSD-1. Because the existing utilities systems are adequate to serve the proposed project and will not require any significant alterations or the construction of new systems, this impact would be less-than-significant.

**Question F**

The proposed project is not anticipated to create any significant amounts of solid waste as the proposed project is to develop a mini storage facility on 7.52± acres. All development of the site would be required to comply with the City's Zoning Ordinance (Title 17.72 of the City Code). Therefore, the proposed project is anticipated to result in less-than-significant impacts from solid waste generation.

**Mitigation Measures**

No mitigation is required.

**Findings**

The proposed project would result in less-than-significant impacts to utility systems.

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>13. <u>AESTHETICS, LIGHT AND GLARE</u></b>      |                                |   |                              |
| <i>Would the proposal:</i>                         |                                |   |                              |
| A) Affect a scenic vista or adopted view corridor? |                                |   | ✓                            |
| B) Have a demonstrable negative aesthetic effect?  |                                |   | ✓                            |
| C) Create light or glare?                          |                                |   | ✓                            |
| D) Create shadows on adjacent property?            |                                |   | ✓                            |

**Environmental Setting**

The project site is not in an adopted view corridor or a scenic vista. The area surrounding the project site is presently comprised of residential (north, east, and south of the site) and a commercial use to the west.

**Standards of Significance**

Visual impacts would include obstruction of a significant view or viewshed or the introduction of a façade which lacks visual interest and compatibility which would be visible from a public gathering or viewing area.

*Shadows.* New shadows from developments are generally considered to be significant if they would shade a recognized public gathering place (e.g., park) or place residences/child care centers in complete shade.

*Glare.* Glare is considered to be significant if it would be cast in such a way as to cause public hazard or annoyance for a sustained period of time.

*Light.* Light is considered significant if it would be cast onto oncoming traffic or residential uses.

### **Answers to Checklist Questions**

#### **Questions A and B**

The proposed project is not within an identified scenic corridor or viewshed so impacts to an identified scenic corridor or viewshed would be less-than-significant. The proposed project would be developing a masonry wall surrounding the site to provide security and also shield the storage facility from the surrounding uses. There will also be a landscaped setback area between the masonry wall and Mack Road and the existing private drive off of Mack Road that is used to access the neighboring multi-family residential development. This landscape area will provide some screening of the masonry wall surrounding the facility. Therefore, the proposed project is anticipated to have a less-than-significant impact on aesthetics.

#### **Questions C and D**

Lighting of the proposed project would be installed to meet City standards and would not be directed toward existing residences or oncoming traffic. The proposed project would not leave any adjacent property in complete shadow nor would it contribute to shadows on any recognized public gathering places. Therefore, shadows, light, and glare impacts are anticipated to be less-than-significant.

#### **Mitigation Measures**

No mitigation is required.

#### **Findings**

The project is expected to have a less-than-significant impact to aesthetics, light, or glare.

MACK ROAD MINI STORAGE (P04-146)  
INITIAL STUDY/MITIGATED NEGATIVE DECLARATION

| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <b>14. CULTURAL RESOURCES</b>   |                                |   |                              |
| <i>Would the proposal:</i>  |                                | ✓   |                              |
| A) Disturb paleontological resources?   |                                | ✓   |                              |
| B) Disturb archaeological resources?  |                                | ✓   |                              |
| C) Affect historical resources?   |                                | ✓   |                              |
| D) Have the potential to cause a physical change, which would affect unique ethnic cultural values? |                                |   | ✓                            |
| E) Restrict existing religious or sacred uses within the potential impact area?                     |                                |   | ✓                            |

**Environmental Setting**

The proposed project is not in a Primary Impact Area as defined by the Sacramento General Plan Update Environmental Impact Report (SGPU DEIR, V-5). The SGPU defines a Primary Impact Area as an area that is most sensitive to urban development due to the potential presence of cultural resources.

There are no existing structures on the site only some asphalt driveway segments. The site is vacant and undeveloped.

**Standards of Significance**

Cultural resource impacts may be considered significant if the proposed project would result in one or more of the following:

1. Cause a substantial change in the significance of a historical or archaeological resource as defined in CEQA Guidelines Section 15064.5 or
2. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature.

## **Answers to Checklist Questions**

### **Questions A - D**

The project site does not contain any known cultural or historical resources, nor were any evident during a site visit in September of 2004. However, during construction, previously unidentified cultural or historical resources may be unearthed. The following mitigation measures shall be implemented to ensure a less-than-significant impact:

### **Mitigation Measures**

- CR-1. If subsurface archaeological or historical remains are discovered during construction, work in the area shall stop immediately and a qualified archaeologist and a representative of the Native American Heritage Commission shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less-than-significant level before construction continues.
- CR-2. If human burials are encountered, all work in the area shall stop immediately and the Sacramento County Coroner's office shall be notified immediately. If the remains are determined to be Native American in origin, both the Native American Heritage Commission and any identified descendants must be notified and recommendations for treatment solicited (CEQA Section 15064.5); Health and Safety Code Section 7050.5; Public Resources Code Section 5097.94 and 5097.98.

### **Question E**

There are no existing religious or sacred uses on the project site. Therefore, it is not anticipated that religious or sacred uses will be impacted by the proposed project.

### **Findings**

The project is expected to have less-than-significant impacts on cultural resources with the incorporation of the above mitigation measures.

MACK ROAD MINI STRORAGE (P04-146)  
**INITIAL STUDY/MITIGATED NEGATIVE DECLARATION**

| Issues:   | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|---|--------------------------------|---|------------------------------|
| <b>15. RECREATION</b>   |                                |   |                              |
| <i>Would the proposal:</i>  |                                |   |                              |
| A) Increase the demand for neighborhood or regional parks or other recreational facilities? |                                |   | ✓                            |
| B) Affect existing recreational opportunities?  |                                |   | ✓                            |

**Environmental Setting**

There are no existing recreational amenities within the project site, as the site is vacant private property. Surrounding uses consist of residential development to the north, east, and south. To the north and south are residentially designated uses, which have recently been approved for development. The site across the Deer Creek Drive to the east is vacant land.

**Standards of Significance**

Recreation impacts would be considered significant if the project created a new demand for additional recreational facilities or affected existing recreational opportunities.

**Answers to Checklist Questions**

**Questions A and B**

The proposed project would not affect existing recreational opportunities because there are no existing recreational amenities within the project site. The proposed project is consistent with the South Sacramento Community Plan and the Sacramento General Plan Update land use designations. Additionally, the proposed use of the site for mini storage uses would not create the need for additional park or recreational facilities. Therefore, recreational impacts are anticipated to be less-than-significant.

**Mitigation Measures**

No mitigation is required.

**Findings**

The proposed project would result in less-than-significant impacts to recreational resources.

**MANDATORY FINDINGS OF SIGNIFICANCE**

| Issues:  | Potentially Significant Impact | Potentially Significant Impact Unless Mitigated | Less-than-significant Impact |
|--|--------------------------------|---|------------------------------|
| <b>16. MANDATORY FINDINGS OF SIGNIFICANCE</b>  |                                |   |                              |
| A. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? |                                |   | ✓                            |
| B. Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals?  |                                |   | ✓                            |
| C. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)   |                                |   | ✓                            |
| D. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? Disturb paleontological resources?   |                                |   | ✓                            |

### **Mandatory Findings of Significance Discussion**

- A. As discussed in the previous sections, the project will not degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, or threaten to eliminate a plant or animal community because the project site does not have suitable habitat for special-status species. There are no historically significant buildings or items on the site. Mitigation measures have been included in the case that previously unidentified cultural or historical resources are uncovered during construction.
- B. As discussed in the preceding section, the project does not have the potential to achieve short-term, to the disadvantage of long-term environmental goals.
- C. When impacts are considered along with, or in combination with other impacts, the project-related impacts are less-than-significant. The proposed project will not add substantially to any cumulative effects. Project related impacts would be mitigated to a less-than-significant level; therefore cumulative effects are not considered a significant impact.
- D. The project does not have environmental effects that could cause substantial adverse effects on human beings, either directly or indirectly. The site is not known to contain any hazards. However, construction activities could reveal previously unknown hazards. The proposed project is required to comply with all applicable laws concerning hazardous materials. There are no known paleontological resources on the site. Mitigation measures concerning how to handle paleontological resources were included in the case previously unidentified resources are uncovered during construction activities.



**SECTION IV. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED**

The environmental factors checked below potentially would be affected by this project.

- |   |   |
|---|---|
| <input type="checkbox"/> Land Use and Planning        | <input type="checkbox"/> Hazards                            |
| <input type="checkbox"/> Population and Housing       | <input type="checkbox"/> Noise                              |
| <input type="checkbox"/> Geological Problems          | <input type="checkbox"/> Public Services                    |
| <input type="checkbox"/> Water                        | <input type="checkbox"/> Utilities and Service Systems      |
| <input type="checkbox"/> Air Quality                  | <input type="checkbox"/> Aesthetics, Light & Glare          |
| <input type="checkbox"/> Transportation/Circulation   | <input checked="" type="checkbox"/> Cultural Resources      |
| <input type="checkbox"/> Biological Resources         | <input type="checkbox"/> Recreation                         |
| <input type="checkbox"/> Energy and Mineral Resources | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> None Identified              |   |

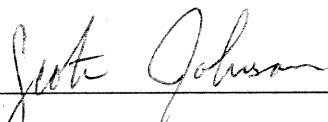
**SECTION V. DETERMINATION**

On the basis of the initial evaluation:

I find that the Proposed Project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

- X I find that although the Proposed Project could have a significant effect on the environment, there will not be a significant effect in this case because the project-specific mitigation measures described in Section III have been added to the project. A NEGATIVE DECLARATION will be prepared.

I find that the Proposed Project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

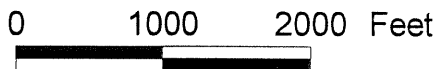
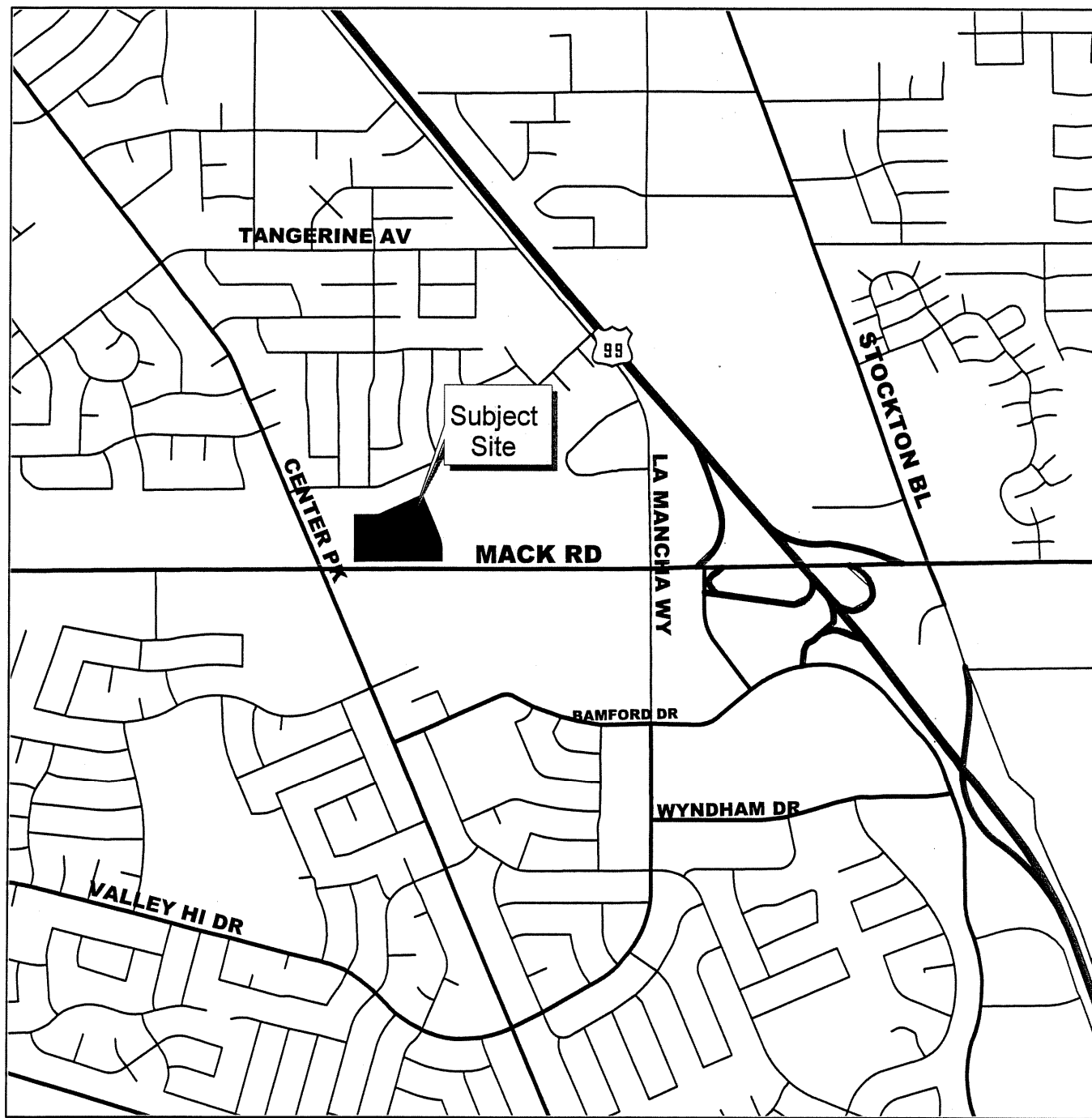
  
\_\_\_\_\_  
Signature

12-14-04  
\_\_\_\_\_  
Date

Scott Johnson  
\_\_\_\_\_  
Printed Name

# **ATTACHMENT A**

## ***Vicinity Map/Location Map***



A logo featuring a stylized map of a region with a circular seal in the center. The seal contains a building and some text. To the right of the logo, the text reads 'Planning &amp; Building Department' and 'Geographic Information Systems'.

August 17, 2004

# Vicinity Map P04-146





Planning & Building  
Department

Geographic  
Information  
Systems

August 17, 2004

# Location Map

## P04-146



# **ATTACHMENT B**

## *Project Plan*

**DEVELOPER**  
 JIM KORRENTAS  
 1500 W. 10TH ST.  
 SACRAMENTO, CA 95811  
 TELEPHONE: 916-441-4274

**ARCHITECT**  
 VALLI ARCHITECTURAL GROUP  
 810 COLLEMAN BLVD.  
 SACRAMENTO, CA 95811  
 TELEPHONE: 916-441-1771

**SITE AREA**  
 54,771 SQ. FT.  
 1.25 AC.

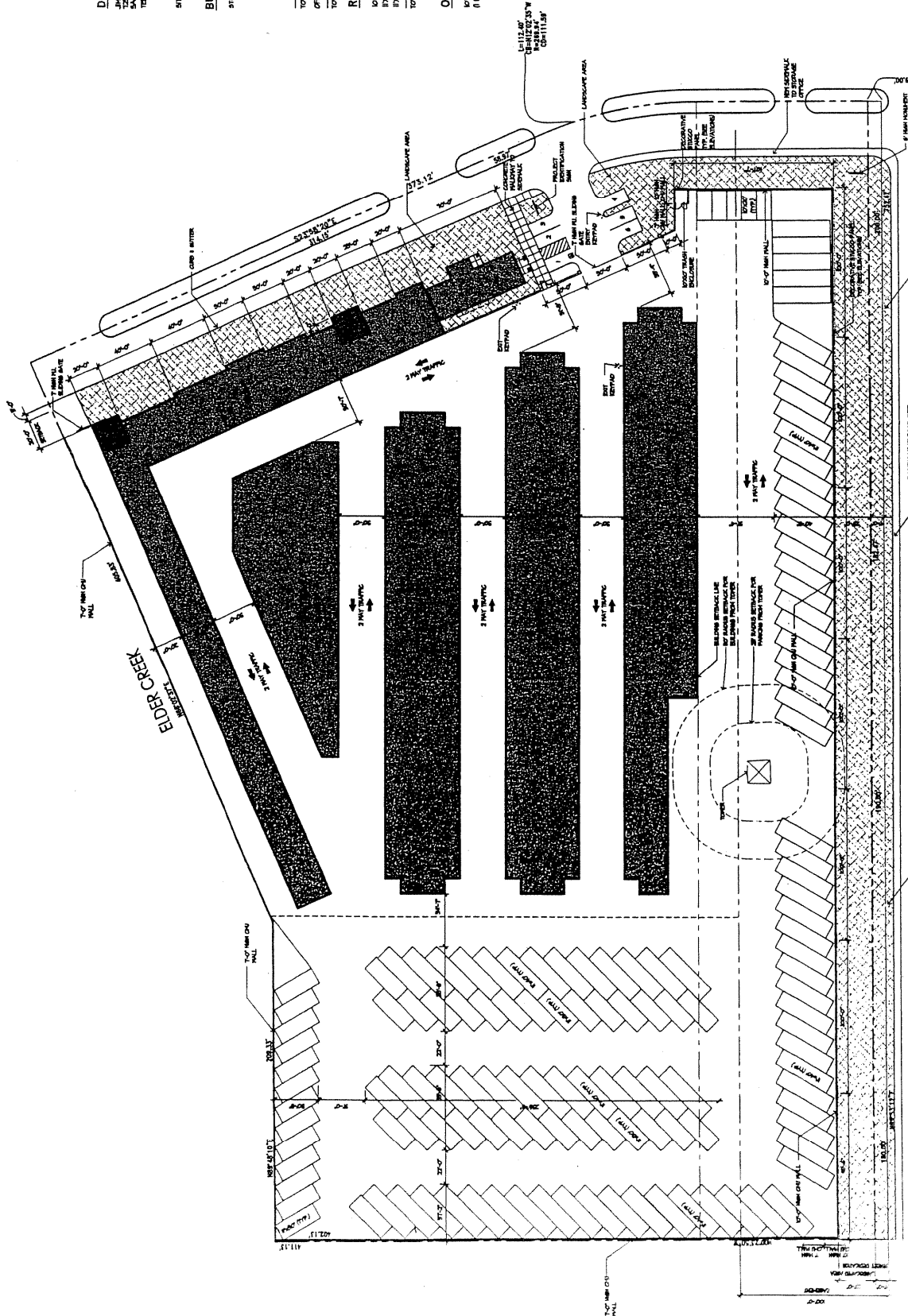
**BUILDING AREA**

**STORAGE**  
 BUILDING A (0 STORY) 16,225 SQ. FT.  
 BUILDING B (0 STORY) 16,600 SQ. FT.  
 BUILDING C (0 STORY) 16,600 SQ. FT.  
 BUILDING D (0 STORY) 16,600 SQ. FT.  
 BUILDING E (0 STORY) 16,600 SQ. FT.  
 TOTAL STORAGE GRASS 71,225 SQ. FT.

**OFFICE/PRESENCE (0 STORY)** 2,790 SQ. FT.  
**TOTAL BUILDING AREA** 74,015 SQ. FT.

**RV PARKING**  
 10'x20' 3 PLS.  
 10'x30' 44 PLS.  
 10'x40' 50 PLS.  
**TOTAL RV UNITS** 97 PLS.

**OFFICE PARKING**  
 10'x20' 2 PLS.  
 10'x30' 1 PLS. (VAN ACCESSIBLE SPACE)



**SITE PLAN**  
 SCALE: 1" = 30'-0"  
 JOB NO. 03-030  
 04/25/04

**MACK ROAD SELF STORAGE**  
 SACRAMENTO, CALIFORNIA

