

DIRECTOR(S) Bruce Monighan, Design Director, Sean de Courcy, Preservation Director Kevin Colin, Zoning Administrator

<u>CITY STAFF</u> Jordyn Tanaka, Administrative Technician



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Thursday May 09, 2024 1:00 p.m.

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1. 1300 C Street Apartment Project (DR23-088) (Noticed 4/26/2024)

Location: 1300 and 1310 C St, APN: 002-0084-001-0000 and 002-0084-001-0000; (District 4, Represented by Councilmember Valenzuela)

Entitlements: Item A. Environmental Exemption (Per Public Resources Code – PRC 21155.4); **Item B**. Site Plan and Design Review to construct a three story residential apartment building with 127 dwelling units on a 0.59-acre parcel in the Light Industrial (M-1-SPD) Zone and Central City special Planning District. The existing commercial structure will be demolished; and **Item C**. Tree Permit for the removal of three protected trees.

Contact: Sierra Peterson, Associate planner, 916-808-7181, <u>SPeterson@cityofsacramento.org</u>

2. New Attached Carport (DR24-006) (Noticed 4/26/2024)

Location: 1036 46th Street, APN: 008-0152-004-0000; (District 4, Represented by Mayor Steinberg)

Entitlements: Item A. Environmental Exemption (Per CEQA 15311-Accesory Structures); **Item B**. Site Plan and Design Review to construct a new attached carport to the existing single-unit dwelling with a deviation to side yard setback standards on a 0.21-acre parcel in the Single-Unit Dwelling (R-1) Zone within the Citywide Design Review Area.

Contact: Rod Lawlor, Assigned planner, 916-808-7829, RLawlor@cityofsacramento.org

3. Taylor Street Tentative Subdivision Map (Z21-108) (Noticed 4/26/2024)

Location: 4101 Taylor St, APN(s): 237-0660-003-0000; (District 2, Represented by Councilmember Thao)

Entitlements: Item A. Addendum to Mitigated Negative Declaration; **Item B**. Tentative Subdivision Map to subdivide one vacant ±6.5-acre parcel into 78 lots within the Single-Unit or Duplex Dwelling (R-1A) Zone; and **Item C**. Site Plan and Design Review of the Tentative Subdivision Map with deviations to lot size and lot depth. There is no proposed site development (construction) with this application. **Contact:** Danny Abbes, Associate planner, 916-808-5873, DAbbes@cityofsacramento.org

Item withdrawn to be renoticed.



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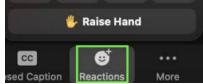
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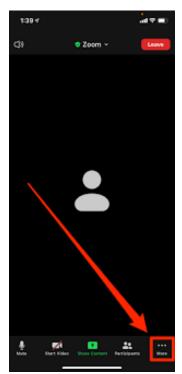


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DIRECTOR REPORT

STAFF RECOMMENDATION

Staff recommends the Design Director approve, with conditions, Site Plan and Design Review and Tree Permit for the project known as 1300 C Apartment Project (DR23-088). Draft Conditions of Approval and Findings of Fact for the project are included below.

REQUESTED ENTITLEMENTS

- Site Plan and Design Review Site Plan and Design Review to construct a threestory residential apartment building with 127 dwelling units on a 0.59-acre parcel in the Light Industrial (M-1-SPD) zone and Central City Special Planning District. The existing commercial structure will be demolished.
- 2. **Tree Permit** for the removal of three private protected trees.

PROJECT INFORMATION

Location:	1300 and 1310 C Street
Parcel Number:	002-0084-001-0000 and 002-0084-001-0000
Council District:	4 (Councilmember Katie Valenzuela)
Applicant:	Alberto Esquivel SKK Developments 2409 L St. Suite 200, Sacramento, CA 95816
Property Owner:	Alberto Esquivel Comani/Phoenix Fund II, LLC 2409 L St. Suite 200, Sacramento, CA 95816
Project Planner:	Sierra Peterson, Associate Planner
Hearing Date:	May 9, 2024

Land Use Information General Plan: Community Plan Area: Specific Plan: Zoning: Special Planning District: Planned Unit Development: Design Review Area: Parking District: Open Space District: Historic Landmark: Historic District:	Employment Center Low Ris Central City Downtown Light Industrial (M-1-SPD) Central City N/A Central City Traditional Traditional No N/A	e (ECLR)
<u>Surrounding Land Use and 2</u> North: South: West: East:	<u>Zoning</u> M-1-SPD R-3A-SPD M-1-SPD R-3A-SPD	Automotive Use Residential Commercial/Warehouse Residential
<u>Site Characteristics</u> Existing Property Area: Topography: Street Improvements: Utilities: Existing Land Use:	25,870 square feet / 0.59-ac Flat Existing Existing Vacant commercial building v	
Other Information Concurrent Files: Previous Files:	N/A IR23-252 - SB330 for subjec	t application

ATTACHMENTS

Attachment 1: Project Plans Attachment 2: Arborist Report Attachment 3: Comment Letters

PROPOSED PROJECT AND ANALYSIS

Background

The subject parcels are on the southeast corner of 13th Street and C Street. 1300 C Street is a vacant lot and 1310 C Street has a vacant commercial building that is proposed to be

demolished. The site is surrounded by light industrial and commercial uses to the north and west, and residential uses to the south and east. The site has been vacant for several years and trees have grown on the vacant site and there are also street trees in the north and west sidewalk planter strips. According to the Arborist report (attachment 2), there are 19 trees proposed for removal, three of which are protected trees and require a Tree Permit for removal.

Project Analysis

The applicant is proposing to construct a three-story, multi-unit dwelling building with 127 dwelling units and amenity space in the interior courtyard between the buildings with additional open space to the east of the buildings.

This entitlement request includes a Site Plan and Design Review and Tree Permit which requires a Director Level public hearing to approve. This project would typically be reviewed at staff level if it did not require a tree removal permit, as there are no SPDR deviations.

General Plan Consistency

The project site is subject to the General Plan 2035 because the applicant submitted an SB330 application (Record IR23-252) that vested on September 19, 2023. The applicant has 180 days to submit a complete planning entitlement application that includes all information necessary for the City to review the housing development project under all regulations in effect on the vesting date. The vesting of the application occurred after a complete application was submitted; therefore, the application is subject to regulations in effect on the vesting date of the SB330 application and not the regulations in effect at time of approval.

The subject parcels have a land use designation of Employment Center Low Rise (ECLR). This designation plays an important role in the city by supporting businesses and providing employment. The combination of high-density buildings and low site coverage in existing employment centers provides the opportunity for new infill development in these areas with complementary uses that transform the existing single use areas into more self-sufficient mixed-use areas with reduced dependence on automobile transportation. Building heights generally range from one to three stories and 60 percent lot coverage. The ECLR does not have minimum or maximum density limits and the floor area is not applicable to residential only developments. The project is consistent with the ECLR designation in that it provides a multi-unit dwelling residential use on an underutilized property with a vacant commercial building. Residential uses in the ECLR are expected in areas of transition to urban development as a supportive mixed use and adjacent to a large employment center. The property is on the border of a residential area to the south and commercial area to the north and is an appropriate transition between the two uses.

Site Plan and Design Review

Site Plan and Design Review is required for the project to ensure compliance with applicable development standards and design guidelines. Site Plan and Design Review is necessary to review the physical characteristics of development, such as the design features and site improvements and not take into consideration the use for which the property will be used (SCC §17.808.110(B)). The project is in the Light Industrial Zone (M-1-SPD), Central City Special Planning District, and Central City Design Review Area. The proposed project complies with all applicable development standards of the M-1-SPD zone, as shown in Table 1 below.

Table 1: Applicable Development Standards M-1-SPD Zone				
Standard	Code Section	Required	Provided	Deviation?
Density	General Plan 2035	N/A (ECLR)	N/A	Ν
Floor Area Ratio	General Plan 2035	N/A (ECLR)	N/A	Ν
Height	§17.220.130(A)	70' – 0"	38' – 0"	Ν
Front Setback		0' – 0"	5' – 3" (west)	Ν
Interior Setback		5' – 0"	9' – 9" (east)	Ν
Street-Side Setback	§17.220.140	0' – 0"	5' – 9" (south) 2' – 0" (north)	Ν
Open Space	§17.444.050 (B.3)	6,350 SQ FT	6,663 SQ FT	Ν

Architectural Analysis

The project is subject to the guidelines listed within the Central City Design Guidelines. These principles are intended to direct future growth in a manner that builds upon the existing context including the neighborhood's market strengths, cultural and social amenities, and plan direction, while also acknowledging the potential for infill and modest growth. The design of the project is consistent with the polices and goals of the Central City Design Guidelines in that the proposed building design is in scale with the existing neighborhood, provides a clear entrance to the project and also provides direct access to the street from individual units, appropriate separation from the street with landscaping, activation of the alley, and a fenced open spaces that is visible to the public right-of-way.

The proposed project completes the half block of C Street between 13th and 14th street with an existing three-story residential development to the east of the subject site. The project is in scale with the one-story commercial buildings to the north and west, and two-to three-story residential buildings to the south and east. There is a main entrance adjacent 13th Street with direct access to the main lobby and apartment office which have floor to ceiling storefront systems. The entrance is highlighted with a large awning

covering the entrance to the lobby and providing covered access to the fenced open space courtyard between the two buildings. The fenced courtyard and pedestrian bridges above the main entrance are visible to the pedestrians walking by the site. The internal circulation of the courtyard and open space area have access to open stairwells and pedestrian bridges over the open space which provides activation of the whole courtyard. Pedestrian circulation is also provided through the security gates at the east side of the site through the north and south fences. The courtyard provides landscaping, open seating, dog washing station, and direct access to the fitness room adjacent to the lobby.

The design guidelines highlight the importance of all publicly visible building elevations that should be designed consistent with the design concept with a complementary level of detail and material quality. The proposed building is highly visible from the public rightof-way on three sides of the building on the north, south, and west elevations. The east elevation is adjacent to the existing residential buildings; however, the building is setback approximately 10-feet and could be visible from C Street or the alley. The building employs rhythmic fenestration pattern that is interrupted by rectangular white and orange cubed massing at the second and third story on the west elevation, and the first and second stories on the north elevation. The alley elevation contrasts the predominately gray stucco elevations with white stucco and gray stairwell element. The north, south and west elevations include ground floor residential units with direct access to the street and provide landscaping adjacent to the base of the building. The floor plans of all three stories are similar and do not provide any private balconies on the upper stories. The primary architectural finish for the building will consist of cement plaster in contrasting colors of grey, white and charcoal, corrugated metal panel in the color terra cotta, aluminum storefront systems, vinyl windows, louvered grills painted to match adjacent finishes, metal railings, and metal fencing.

Access, Circulation, and Bicycle Parking

The subject site has public right-of-way on the north, south and west sides, C Street, Chinatown Alley, and 13th Street respectively. The site is located within the Traditional parking district and is located within 0.25 miles of a light rail station and is subject to AB 2097. As such, no vehicle parking is required.

This project meets bicycle parking requirements for multi-unit dwelling developments in the Traditional Parking District. The parking ratio for multi-unit residential uses without private garage space or dedicated storage space for each unit is one space for every two dwelling units for long-term bicycle parking, and one space for every 10 dwelling units. A bicycle storage room will be provided and is located near the main gate entrance and lobby on the first floor. Short-term bicycle parking space will be located adjacent to 13th Street near the lobby.

Table 2: Urban Parking Requirements - Bicycle					
	Requ	uired	Prov	rided	
Use	Long- Term	Short- Term	Long- Term	Short- Term	Deviation?
Multi-Unit Dwelling – 127 Units	64	13	76	14	Ν

Open Space

This project is required to comply with Section 17.444.050 of the City Code which requires a combination of private and common open spaces at a ratio of 50 square feet of open space per dwelling unit for multi-unit dwellings. The project will provide 6,663 square feet of common open space, where 6,650 is required.

PUBLIC / NEIGHBORHOOD OUTREACH AND COMMENTS

The project was routed to Preservation Sacramento, Civic Thread, Sacramento Area Bicycle Advocates, Region Builders, The River District PBID, and Alkali and Mansion Flats Historic Neighborhood Association. Staff has received ten comment letters from Civic Thread, Midtown Association, Sacramento Area Bicycle Advocates, and seven public comments. Letters from Civic Thread, Midtown Association, Sacramento Area Bicycle Advocates were in support of the project; however, did comment that additional bicycle parking should be provided since no vehicular parking is provided, pedestrian lighting for safety should be provided, wayfinding signage, and a mural should be provided for visual character. Staff also received seven public comments from neighboring residents and/or property owners. The comments discussed the lack of parking and need for more bicycle parking, affordability of the project, noise and light pollution, and safety of surrounding streets.

All property owners, residents, and neighborhood associations within 500 feet of the subject site were mailed a public hearing notice on April 26, 2024, and a notice was posted at the project site.

ENVIRONMENTAL CONSIDERATIONS

The proposed 1300 C Apartments Studios Project (DR23-088) located at 1300 and 1310 C Street is a "project" as defined in the California Environmental Quality Act (CEQA). The City, as lead agency, is required to proceed in accordance with CEQA requirements prior to considering any approval.

Staff has determined that the 13C Studios Project (DR23-088) qualifies for a statutory exemption pursuant to Public Resources Code (PRC) section 21155.4, which was added to the PRC by SB 743. The legislature included the following that applies to section 21155.4:

With the adoption of...the Sustainable Communities and Climate Protection Act of 2008, the Legislature signaled its commitment to encouraging land use and transportation planning decisions and investments that reduce vehicle miles traveled and contribute to the reductions in greenhouse gas emissions required in the California Global Warming Solutions Act of 2006...Similarly, the California Complete Streets Act of 2008...requires local governments to plan for a balanced, multimodal transportation network that meets the needs of all users of streets, roads and highways for safe and convenient travel.

Section 21155.4 is thus part of a comprehensive legislative approach to reducing VMT and supporting the state's efforts to reduce greenhouse gas emissions. Section 21155.4 provides as follows:

- (a) Except as provided in subdivision (b), a residential, employment center, as defined in paragraph (1) of subdivision (a) of Section 21099, or mixed-use development project, including any subdivision, or any zoning, change that meets all of the following criteria is exempt from the requirements of this division:
 - (1) The project is proposed within a transit priority area, as defined in subdivision (a) of Section 21099.
 - (2) The project is undertaken to implement and is consistent with a specific plan for which an environmental impact report has been certified.
 - (3) The project is consistent with the general use designation, density, building intensity, and applicable policies specified for the project area in either a sustainable communities strategy or an alternative planning strategy for which the State Air Resources Board, pursuant to subparagraph (H) of paragraph (2) of subdivision (b) of Section 65080 of the Government Code, has accepted a metropolitan planning organization's determination that the sustainable communities strategy or the alternative planning strategy would, if implemented, achieve the greenhouse gas emissions reduction targets.
 - (4) Further environmental review shall be conducted only if any of the events specified in Section 21166 have occurred.

The requirements of the section are set forth below, followed by text that identifies the manner in which the proposed project complies in italics.

A. The project must be a residential, employment center or mixed-use development project.

The 13C Studios Project proposes a residential development consisting of 126 studio apartment units on a 0.59-acre parcel site located at 1300 and 1310 C Street in the M-1-SPD zone of the City of Sacramento within the Central City Special Planning District. "Mixed-use development combines two or more types of land use into a building or set of buildings that are physically and functionally integrated and mutually supporting. This can be some combination of residential, commercial, industrial, office, institutional, or other land uses." (San Joaquin Valley Blueprint Planners Toolkit, online access 9/24/2018)

The proposed project qualifies as a mixed-use development project.

 B. The project must be located in a transit priority area, as defined in subdivision (a) of Section 21099.

Section 20199 defines "transit priority area" as including an area within ½ mile of an existing major transit stop. Pursuant to PRC section 21064.3, a major transit stop is "a site containing an existing rail transit station or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods." Pursuant to PRC section 21155(b), a high-quality transit corridor is defined as a corridor with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours.

The City prepared an EIR for the Central City Specific Plan (CCSP), which was certified on April 19, 2018 (Resolution No. 2018-0129). Figure 4.12-13 in the CCSP EIR showed the portion of the CCSP area that would meet the criteria for proximity to transit in the legislation. The map identified areas one-half mile from RT's existing light rail stations and one-half mile from high quality transit corridors with service intervals of 15 minutes or less (CCSP EIR Figure 4.12-13). The project site is located in the area identified as a transit priority area. More specifically, the 1300 C Apartments project site is approximately ¼ mile to Sacramento Regional Transit's (Sac RT) Blue Line (light rail transit or LRT). Additionally, Sac RT bus routes 33, 129, and 134, provide bus stops on E and F Street. There are additional bus routes with stops located within a few blocks of the project site.

The proposed project is located in a transit priority area.

C. The project is undertaken to implement and is consistent with a specific plan for which an environmental impact report has been certified.

The proposed project's land use is consistent with the CCSP. The City prepared and certified an EIR for the CCSP.

D. The project is consistent with the general use designation, density, building intensity, and applicable policies specified for the project area in a sustainable communities strategy for which the State Air Resources Board (ARB) has accepted a metropolitan planning organization's determination that the sustainable communities CEQA review strategy or the alternative planning strategy would, if implemented, achieve the greenhouse gas emissions reduction targets.

The proposed project is consistent with the Metropolitan Transportation Plan/Sustainable Communities Strategy (MTP/SCS) adopted by the Sacramento Area Council of Governments (SACOG). The ARB has accepted SACOG's determination that the plan would achieve GHG reduction targets. The written concurrence from SACOG regarding the City's determination of consistency with the MTP/SCS is included as Attachment 4.

The proposed project is consistent with the MTP/SCS.

E. Further environmental review shall be conducted only if any of the events specified in Section 21166 have occurred.

This requirement confirms that the requirement of consistency with a specific plan for which an EIR was prepared would be sufficient unless substantial changes have been proposed in the specific plan that would require major changes in the EIR, or changes have occurred in the circumstances under which the EIR was prepared or new information becomes available. The CCSP EIR was certified, and the CCSP adopted, on April 19, 2018. There have been no substantial changes in the CCSP or in the circumstances in the specific plan area that would affect the EIR analysis and conclusions.

None of the events identified in PRC 21166 have occurred.

PRC section 21155.4 provides that a project that meets the section requirements is exempt from CEQA review, unless one or more of the events identified in subsection (b) have occurred. As noted, none of those circumstances have occurred. The proposed project is, therefore, exempt from CEQA review.

FLOOD HAZARD ZONE

State Law (SB 5) and Planning and Development Code chapter 17.810 require that the City must make specific findings prior to approving certain entitlements for projects within a flood hazard zone. The purpose is to ensure that new development will have protection from a 200-year flood event or will achieve that protection by 2025. The project site is within a flood hazard zone and is an area covered by SAFCA's Improvements to the State Plan of Flood Control System, and specific findings related to the level of protection have been incorporated as part of this project. Even though the project site is within a flood hazard zone, the local flood management agency, SAFCA, has made adequate progress on the construction of a flood protection system that will ensure protection from a 200-

year flood event or will achieve that protection by 2025. This is based on the SAFCA Urban level of flood protection plan, adequate progress baseline report, and adequate progress toward an urban level of flood protection engineer's report that were accepted by City Council Resolution No. 2016-0226 on June 21, 2016 and the SAFCA 2023 Adequate Progress Annual Report accepted by City Council Resolution No. 2023-0337 on October 24, 2023.

DRAFT CONDITIONS OF APPROVAL

Design Review / Planning

- 1. The proposed construction of the multi-unit dwelling building is approved per attached plans and conditions of approval.
- 2. Provide the following building materials on the multi-unit dwelling building as indicated per approved plans:
 - a. Single-Ply Membrane Roofing
 - b. Cement plaster with reveals in contrasting colors of grey, white and charcoal
 - c. Terra cotta colored corrugated metal panel
 - d. Prefinished dark grey aluminum storefront systems and ground level doors
 - e. Dark grey or black vinyl windows
 - f. Punched louvered grills painted to match adjacent finishes
 - g. Prefinished black Ornamental metal mesh railings
 - h. Prefinished black ornamental metal fencing a maximum of eight feet in height.
- 3. All mechanical equipment proposed shall be screened to fit in with the design of the project per approved plans, and not exceed parapet height or be visible from any street or pedestrian views.
- 4. Lighting:
 - a. The type and location of the outdoor and exterior lighting (buildings, parking areas, etc.) shall be approved by Planning and Design Review staff during building plan check. Lighting shall be provided at the ground-level on all sides of the building and at each building entry.
 - b. Per Section 17.612.030(B), exterior lighting shall reflect away from public streets. Fixtures shall be unobtrusive and complementary to the architectural design of the building. Lighting shall be designed so as not to

produce hazardous and annoying glare to motorists and building occupants, adjacent residents, or the general public.

- c. The design of the light fixtures within the surface parking area shall be reviewed and approved by Planning and Design Staff. A lighting and photometric plan shall be provided for review and approval as part of Building Permit documents.
- 5. Provide the required 13 short-term bicycle spaces and 64 long-term bicycle storage spaces as shown on the approved plans in accordance with Municipal Code section 17.608.040N.1 and 17.608.040N.2. Bicycle parking shall comply with the City's Bicycle Rack Design and Placement Design Standards.
- 6. Construct landscape improvements including all required irrigation, ground coverings, shrubs, and trees as shown on the final approved landscape plan exhibit.
- 7. Any work involving a city tree, private protected tree, or within the park strip requires a tree permit. If any work involving a city tree, private protected tree, or within the park strip a tree permit shall be obtained from Urban Forestry.
- 8. No signage has been reviewed or approved through the Site Plan and Design Review process. All future signage shall conform to the sign code standards found in SCC section 15.148.160 and shall be reviewed under the separate sign permitting process.
- 9. A photometric plan shall be provided for all outdoor lighting in and around the site at the time of plan check. Any new building mounted lighting shall be subject to final review and approval by Planning staff.
- 10. Planning In-progress inspections (93) shall be called for prior to the following Building Inspections: 10 Bldg-Foundation Forms, 12 Bldg-Concrete Slab Forms, and 19 Bldg-Frame. An 89 Planning Final shall be called for prior to 29 Building Final.
- 11. Any modifications to the approved project shall be subject to the review and approval of Planning staff (and may require additional entitlements).
- 12. The applicant shall obtain all necessary building and encroachment permits prior to commencing construction. No permits shall be issued within the 10-day appeal period.
- 13. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any work that differs from the final set of plans approved by the Planning staff shall be subject to review and approval prior to issuance of a building permit.
- 14. This approval shall expire in three years from the approval date.

Public Works

- 15. Construct standard public improvements as noted in these conditions pursuant to Title 17 of the City Code. Improvements shall be designed to City Standards and assured as set forth in Section 17.502.130 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Public Works. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk adjacent to the subject property along C Street and 13th Street per City standards and to the satisfaction of the Department of Public Works.
- 16. The applicant shall remove all existing driveways along C Street and 13th Street and reconstruct the frontage improvements matching existing to the satisfaction of the Department of Public Works.
- 17. The applicant shall repair or replace/reconstruct (in concrete) any deteriorated portion of Chinatown alley adjacent to the subject property (from the subject property's eastern property line to 13th Street) per City standards to the satisfaction of the Department of Public Works.
- 18. With the proposed development along the alley, the applicant shall install alley name signs at both alley entrances when required by either the Building, Police and/or Fire Departments. In the event the alley name signs are required, the applicant shall coordinate with the Department of Public Works on the location of the alley name signs at alley entrances. The installation of the alley name signs shall be to the satisfaction of the Department of Public Works.
- 19. The applicant shall merge the existing parcels that cover the project site which creates the parcel boundary shown on the proposed site plan prior to obtaining any Building Permits.
- 20. Bulb outs/curb extensions are required where there is on-street parking in the central City area or as directed by the Department of Public Works. Locations of bulb outs must be reviewed and approved by the City Traffic Engineer. The applicant shall construct bulb-outs/ curb extension at the following locations to the satisfaction of Public Works:
 - a. South-east corner of C Street and 13th Street intersection
- 21. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A. standards at the south-east corner of C Street and 13th Street intersection to the satisfaction of the Department of Public Works.

- 22. This project will require the installation of streetlights on all public streets fronting this property to the satisfaction of Public Works. The number and locations of these lights will be determined when development plans are submitted for review.
- 23. The applicant shall provide a signing and striping improvement plan if new signage or striping is proposed; or if existing signing and/or striping is removed or relocated. The plans shall be to the satisfaction of the Department of Public Works.
- 24. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Public Works.

Department of Utilities

- 25. Applicant shall participate in the Central City Impact Fee Finance Plan and pay all required fees.
- 26. Per City Code Section, 13.04.070, multiple water service to a single lot or parcel may be allowed if approved by DOU Development Review and Operations and Maintenance staff. Any new water services (other than fire) shall be metered. Excess services shall be abandoned to the satisfaction of the DOU. Note: There is an existing 1-inch domestic service for 1310 C Street in the Alley.
- 27. Prior to or concurrent with the submittal of improvement plans, prepare a project specific water study for review and approval by the DOU. The water distribution system shall be designed to satisfy the more critical of the two following conditions: (1) at maximum day peak hour demand, the operating or "residual" pressure at all water service connections shall be at least 30 pounds per square inch, (2) at average maximum day demand plus fire flow, the operating or "residual" pressure in the area of the fire shall not be less than 20 pounds per square inch. The water study shall determine if the existing and proposed water distribution system is adequate to supply fire flow demands for the project. A water supply test may be required for this project. The applicant is advised to contact the City of Sacramento Utilities Department Development Review Section (916-808-7890) at the early planning stages to address any water related requirements. Failure to submit the water study may delay review and approval.
- 28. Common area landscaping shall have a separate street or alley tap for a metered irrigation service.

- 29. Water services shall conform to the current Onsite Design Manual and/or Design and Procedures. Private water services running parallel within 13th Street right of way are not allowed.
- 30. This project is served by the Combined Sewer System (CSS). Therefore, the developer/property owner will be required to pay the Combined Sewer System Development Fee prior to the issuance of building permit. The fee will be used for improvements to the CSS. The applicant is recommended to contact the Department of Utilities Development Services at 916-808-7890 for a CSS fee estimate.
- 31. Any sewer connections to the existing 12-inch CSS main in the Alley shall be mitigated. The proposed project is contributing increased sewer flows to the CSS and shall evaluate the available capacity of existing CSS mains from the project's point of service to the nearest 18-inch main. If any portion of the City mains to the nearest 18-inch main is determined to have insufficient capacity to accommodate the increased sewer flow, the development shall be required to improve the undersized mains to the nearest 18-inch main. The applicant is advised to contact the City of Sacramento Utilities Department Sewer Planning Section (916-808-7890) at the early planning stages to address any sewer related requirements.
- 32. The development of this site must comply with the current drainage design standards. To meet this requirement 7,600 cubic feet of detention must be provided per each additional acre of impervious area. The maximum discharge rate must be limited to 0.18 cfs/acre. The required detention volume can be partially or fully mitigated by implementing Low Impact Development (LID) features such as Stormwater planters and porous pavement, provided these features conform to the DOU's Hydromodification Management Plan (HMP) requirements. Designer shall utilize the latest edition of the Sacramento Area Hydrology Model (SAHM) when using LID features.
- 33. Onsite sewer and drainage facilities shall be separated systems.
- 34. All on-site drainage systems shall be designed to the standards specified in the DOU onsite design manual.
- 35. Onsite domestic water and sanitary sewer services, surface and subsurface drainage shall not cross property lines unless owner/applicant either; (1) merges 1300 C St and 1310 C St lots; or (2) enters into a Conveyance of Easement Agreement with the City to the satisfaction of the DOU and City Attorney.
- 36. Finished floor elevations shall be a minimum of 6-inches above the 100-year HGL or 1-foot above the overland flow release elevation, whichever is higher or as approved by the DOU.

- 37. No more than 6,000 square feet is allowed to sheet drain over a public sidewalk. If the area is larger than 6,000 square feet, then an on-site surface drainage system is required and shall be connected to the street drainage system by means of a storm drain service tap. All on-site systems shall be designed to the standard specified in the DOU onsite manual.
- 38. A grading plan showing existing and proposed elevations is required. Adjacent off-site topography shall also be shown to the extent necessary to determine impacts to existing surface drainage paths. No grading shall occur until the grading plan has been reviewed and approved by the DOU.
- 39. The applicant must comply with the City of Sacramento's Grading, Erosion and Sediment Control Ordinance. This ordinance requires the applicant to show erosion and sediment control methods on the construction drawings. These plans shall also show the methods to control urban runoff pollution from the project site during construction.

Urban Forestry

- 40. The Tree Permit for the removal of two (2) City trees and one (1) private protected tree, City tree #30, an 18-inch DSH Northern California black walnut and City tree #32, a 42-inch DSH Northern California black walnut, and private protected tree, tree #12, a 26-inch DSH Ailanthus tree, as identified, according to the Arborist report, by CalTLC, dated March 6, 2024, and related exhibits is approved.
- 41. The applicant shall retain all trees permitted for removal until after all fees associated with the application for a grading permit have been paid.
- 42. The following Tree Preservation Measures shall appear on all future plans in the General Notes, Grading Plans, Utility Plans, Demolition Plan, Landscape Plan and the offsite plans if the trees will be impacted by work proposed on each sheet. This does not replace any request for a Project Arborist's tree protection plan.

Required Tree Preservation Measures for Private Protected Trees

- a. This project shall contract with a Project Arborist experienced with tree protection and construction that is required to:
 - 1. Attend the preconstruction meetings to approve of and inform contractors of all tree protection measures.
 - 2. Visit the site before and after demolition, grading and landscaping as well as at least twice each month during construction to ensure that tree protection measures are implemented and maintained.
 - 3. Be responsible for correcting any site conditions that may negatively impact the trees and revisit the site to ensure corrective action was

properly implemented.

- 4. The Project Arborist shall report in writing to Urban Forestry all violations and tree protection failures along with corrective action taken and expected outcomes.
- b. All concrete sidewalks and driveways shall be retained throughout construction to protect the roots and soil from the impacts of construction activities.
 - Existing driveways shall be used as the sole access to the site. Where
 there are no existing driveways, access shall be limited to a one or two
 locations outside the dripline of protected trees that have protection
 from soil compaction with the use of one or more of the following: A 6inch layer of hardwood chips covered by ³/₄-inch plywood or trench
 plates, geotextile fabric covered by a 6-inch layer of hardwood chips or
 an alternative that is approved by the City Arborist.
- c. Right-of-way planters and City trees shall be separated from the construction site with a six-foot high chain link fence that shall remain throughout the duration of the project to protect trees and to prevent construction traffic from compacting the soil in the planters.
- d. Construction trailers and port-a-potties shall be placed on existing hardscape or bridged over the tree protection zone or planter so as not to compact soil.
- e. Any Regulated Work within the dripline or Tree Protection Zone of a regulated tree shall be separately permitted prior to the start of construction and supervised by a Certified Arborist. Submit a tree permit application and a tree protection plan created by a Certified Arborist to UrbanForestry@cityofsacramento.org and refer to the planning project number or off-site project number.
- f. All excavation, grading, or trenching within the dripline of a protected tree for the purpose of utility installation, constructing foundations, footings, sidewalks, curbs, gutters, or any other reason shall employ one of the following methods: Hydro-excavation, pneumatic excavation, or hand digging and shall be directly supervised by a Certified Arborist.
- g. There shall be no excavation deeper than the existing excavation for sidewalks within the dripline of protected trees.
- h. There shall be no grade changes within the dripline of protected trees. All grade changes shall be accommodated onsite.
- i. There shall be no soil compaction within the dripline of protected trees.
- j. There shall be no non-native soil, non-organic matter or structural soil added to the right-of-way planter.
- k. The following is a list of activities that require a tree permit if they are to occur or be used within the right-of-way planter and/or within the tree protection zone of protected trees: any regulated work as defined in SCC 12.56, excavation, grade changes, trenches, root or canopy pruning, or boring.
- I. The following is a list of activities that are prohibited within the right-of-way

planter and/or tree protection zone of protected trees: pedestrian and equipment traffic that could compact the soil or physically damage roots, parking vehicles, equipment and/or port-a-potties, storing of soil, construction materials, petroleum products, water or building refuse, disposing of wash water, paint, cement, fuel or other potentially damaging liquids, and any other activities that may have negative impacts on the trees and soil.

- m. All trees shall be watered regularly according to the recommendation of the Project Arborist.
- n. The applicant shall be financially responsible for any damage to City trees associated with the project. Accidental or negligent actions that damage City trees may result in a penalty. The monetary value of any such damages will be appraised by the City Urban Forester or his authorized representative and shall be expressed as the monetary equivalent of all labor and materials required to bring the tree in question to a state of comparable utility with regards to its condition and function prior to the beginning of the project.
- 43. The applicant shall provide for the planting and irrigation of new City street trees according to the project final approved landscape plan and to the satisfaction of the City Urban Forester.

Environmental Planning Services

- 44. In the event that archaeological resources or human remains are encountered during construction, work within 100 feet of the discovery shall cease until a notice to proceed is issued by the City. The applicant shall notify the City of Sacramento Manager of Environmental Planning Services (phone 311 or (916) 799-1531; email tbuford@cityofsacramento.org) and shall comply with City direction, and federal and State regulations and guidelines regarding the treatment of cultural resources and human remains. The Coroner shall be notified in the event human remains are discovered; the applicant shall be responsible for the employment of a qualified archaeologist to advise regarding treatment of any artifacts.
- 45. The project applicant shall conduct any tree removal activities required for project construction outside of the migratory bird and raptor breeding season (February 1 through August 31) where feasible. All trees slated for removal during the nesting season shall be surveyed by a qualified biologist no more than 48-hours before removal to ensure that no nesting birds are occupying the tree. (See federal Migratory Bird Treaty Act, California Fish and Game Code (Sections 3503, 3503.5, 3511, and 3800)
- 46. For any construction activities that will occur between February 1 and August 31, the applicant shall employ a qualified biologist to conduct preconstruction surveys in suitable nesting habitat on or near the construction area for nesting raptors and migratory birds. If the biologist determines that construction may

occur without impacting the breeding effort, the nest(s) shall be monitored by the biologist during construction. If the biologist concludes that the project would impact the nest, construction activities will cease until the nest is no longer active. Completion of the nesting cycle shall be determined by the biologist.

47. Sacramento Metropolitan Air Quality Management District Basic Construction Emission Control Practices apply and compliance is required. See www.airquality.org. Other air district rules may apply and the air district should be consulted.

Fire Department

- 48. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction. California Fire Code Section 501.4.
- 49. Provide a water flow test. (Make arrangements with the Department of Utilities at 916-808-7890 or by email at DOUdevelopmentreview@cityofsacramento.org, California Fire Code Section 507.4.
- 50. Provide the required fire hydrants in accordance with California Fire Code Section 507 and Appendix C, Section C102.1 as amended the Sacramento City Code Section 507.5.1.
- 51. Provide the required fire hydrants in accordance with California Fire Code Section 507 and Appendix C, Section C105.1. Hydrant spacing shall be decreased where T courts are used. Hydrants shall be provided halfway between each T court, on one side of the street, and to the satisfaction of the Fire Department.
- 52. Provide appropriate Knox access for site. California Fire Code Section 506.
- 53. Locate and identify Fire Department Connections (FDCs) on address side of building no farther than 50 feet and no closer than 15 feet from a fire hydrant and not more than 30 feet from a paved roadway.
- 54. An approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. The room shall contain all system control valves, fire alarm control panels and other fire equipment required by the Fire Code Official. Fire Control rooms shall be located within the building at a location approved by the Fire Code Official, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. Fire Control rooms shall not be less than 50 square feet for rooms without a fire pump and 200 square feet when a fire pump is present. California Fire Code as Amended by the Sacramento City Code Section 901.4.7 if buildings are separated a

dedicated fire sprinkler system and a separate fire control room may be required per building.

- 55. Provide standpipe hose valves at the intermediate landing levels of stairways as required by the Sacramento Fire Official.
- 56. Emergency Responder Radio Coverage may be required. Testing shall be conducted by an authorized technician to verify compliance with section 510, California Fire Code. This test shall verify that the building will support the Sacramento City Fire Department Radio Communication System. This test shall be performed in accordance with California Fire Code section 510.4.1.

Park Planning and Development Services

57. <u>Maintenance District:</u> The applicant shall initiate and complete the formation of a parks maintenance district (assessment or Mello-Roos special tax district) or annex the project into an existing parks maintenance district. The applicant shall pay all city fees for formation of or annexation to a parks maintenance district. (Contact Infrastructure Finance, Brent Mueller, (916) 808-5715).

Sacramento Regional County Sanitation District

54. The subject property is outside the boundaries of SacSewer but within the Urban Service Boundary and Sacramento Regional County Sanitation District (Regional San) boundaries. Regional San will provide ultimate conveyance and treatment of the sewer generated from this site, but the City of Sacramento Utilities Department's approval will be required for local sewage service.

Sacramento Area Sewer District

55. Before the ISSUANCE OF A BUILDING PERMIT: The owner must contact the Regional San Permit Services Unit at PermitServices@sacsewer.com or by phone at (916) 876-6100 to determine if sewer impact fees are due. Fees are to be paid before the issuance of building permits.

Sacramento Municipal Utility District (SMUD)

56. SMUD has existing overhead 21kV and low voltage/secondary facilities along the south side of the property (south side of alley) that will need to remain. The Applicant shall be responsible for maintaining all CalOSHA and State of California Public Utilities Commission General Order No. 95 safety clearances during construction and upon building completion. If the required clearances cannot be maintained, the Applicant shall be responsible for the cost of relocation.

- 57. Structural setbacks less than 14-feet shall require the Applicant to conduct a preengineering meeting with all utilities to ensure property clearances are maintained.
- 58. Any necessary future SMUD facilities located on the Applicant's property shall require a dedicated SMUD easement. This will be determined prior to SMUD performing work on the Applicant's property.
- 59. In the event the Applicant requires the relocation or removal of existing SMUD facilities on or adjacent to the subject property, the Applicant shall coordinate with SMUD. The Applicant shall be responsible for the cost of relocation or removal.
- 60. SMUD reserves the right to use any portion of its easements on or adjacent to the subject property that it reasonably needs and shall not be responsible for any damages to the developed property within said easement that unreasonably interferes with those needs.
- 61. The Applicant shall not place any building foundations within 5-feet of any SMUD trench to maintain adequate trench integrity. The Applicant shall verify specific clearance requirements for other utilities (e.g., Gas, Telephone, etc.).
- 62. In the event the City requires an Irrevocable Offer of Dedication (IOD) for future roadway improvements, the Applicant shall dedicate a 12.5-foot public utility easement (PUE) for overhead and/or underground facilities and appurtenances adjacent to the City's IOD.
- 63. The Applicant shall comply with SMUD siting requirements (e.g., panel size/location, clearances from SMUD equipment, transformer location, service conductors). Information regarding SMUD siting requirements can be found at: <u>https://www.smud.org/en/Business-Solutions-and-Rebates/Design-and-Construction-Services</u>.
- 64. The shown transformer location and space on the provided plans will have to be validated by SMUD to ensure it meets all location, spacing and design criteria per the requested service size. Additional space for a switch may be necessary pending the requested service size. Please contact SMUD Line Design for additional information.

Sacramento Police Department

Lighting:

65. Exterior lighting shall be white light using LED lamps with full cutoff fixtures to limit glare and light trespass. Color temperature shall be between 2700K and 4100K with a color rendering index of 80 or higher and a light loss factor of .95

or better. When choosing lamps, the applicant shall look for efficiency of 110 lumens per watt or better. All existing exterior fixtures shall be replaced with fixtures that meet this requirement.

- 66. Light poles, if applicable, shall be no higher than 16'.
- 67. Broken or damaged exterior lighting shall be repaired or replaced within 48 hours of being noted.
- 68. Entry drives, drive aisles, parking and bicycle parking shall be illuminated to a maintained minimum of 1.5 foot candles per square foot of parking area at a 6:1 average to minimum ratio.
- 69. Exterior walkways, alcoves and passageways shall be illuminated to a maintained minimum of 1/3 foot candles per square foot of surface area at a 6:1 average to minimum ratio.
- 70. Exterior lighting distribution and fixtures shall be approved by the Sacramento Police Department CPTED Sergeant (or designee) prior to issuance of a building permit.
- 71. Exterior lighting shall be designed in coordination with the landscaping plan to minimize interference between the light standards and required illumination and the landscape trees and required shading.
- 72. Exterior lighting shall be shielded or otherwise designed to avoid spill-over illumination to adjacent streets and properties.

Landscaping:

- 65. All mature landscaping shall follow the two-foot, six-foot rule. All landscaping shall be ground cover, two feet or less and lower tree canopies of mature trees shall be above six feet. This increases natural surveillance, eliminates hiding areas within the landscape, and provides for tenants and users a safer environment.
- 66. Tree canopies shall not interfere with or block lighting. This creates shadows and areas of concealment. The landscaping plan shall allow for proper illumination and visibility regarding lighting and surveillance cameras through the maturity of trees and shrubs.

Security:

67. Fencing, if applicable, shall be of decorative tubular steel, no climb type and a minimum of 6' in height.

- 68. A Video Assessment and Surveillance System (VASS) shall be installed at the site and maintained by a property management company, security company, or designee.
- 69. Manager with access to VASS storage shall be able to respond to any activation within two hours.
- 70. Cameras shall be day/night capable with a resolution of no less than two (2) megapixels and a minimum frame rate of 15 frames per second.
- 71. Each driveway entrance and each building entrance shall be covered by a camera set at 100 pixels per foot or higher.
- 72. VASS shall be capable of exporting footage to common media in a standard viewing format and shall not require proprietary software for third party viewing.
- 73. VASS shall be capable of storing no less than 30 days' worth of activity.
- 74. VASS shall provide comprehensive coverage of:
 - areas of ingress and egress
 - parking lot
 - coverage of all four (4) exterior sides of the property
 - adjacent public rights of way
 - main entrance of building
 - hallways
 - elevators
- 75. All dumpsters shall be kept locked or in locked enclosures. Gating for dumpster enclosures should be slatted to allow visual surveillance of the interior.
- 76. Exterior trash receptacles shall be of a design to prevent unauthorized removal of articles from the trash bin.
- 77. Any graffiti painted or marked upon the premises or on any adjacent area under the control of the applicant shall be removed or painted over with matching paint within 72 hours of being applied.
- 78. Exterior benches shall be constructed so as to deter skateboarding (e.g., center armrest partitions).
- 79. Property management shall be responsible for the daily removal of all litter from the site.
- 80. Applicant shall install a <u>law enforcement</u> "Knox Box" for police access to common areas on the premises, including, but not limited to the main entrance for EACH building, main entrance gate, etc.

81. Applicant shall employ uniformed security to respond to disturbances 24/7. Security shall conduct site inspections no less than three (3) times daily. The contracted security company shall be registered and in good standing with the Bureau of Security and Investigative Services (BSIS). Applicant may request a modification of this condition at any time. Any request for modification shall be in writing and submitted to the Sergeant of the Sacramento Police Department's CPTED unit, or designee, and specify the desired modification(s). The Sacramento Police Department will evaluate the modification request and will respond within 30 days of receipt of the request.

During Construction:

- 82. The applicant shall enclose the entire perimeter of the project with a chain link fence with necessary construction gates to be locked after normal construction hours.
- 83. The location shall be monitored by security after normal construction hours during all phases of construction. This can be done via remote camera monitoring.
- 84. Adequate security lighting shall be provided to illuminate vulnerable equipment and materials. Lighting shall be white light with full cut off fixtures.

Recycling and Solid Waste Division

- 85. Project must meet the requirements outlined in City Code Chapter 13.10, 13.24 and 17.616.
- 86. The trash room must have sufficient space to accommodate bins for trash, recycling, and organics. Containers may be used with an increase in collection frequency as an alternative to bins.
- 87. The proposed trash room design does not offer an internal access point and requires residents and maintenance staff to access the trash room via Chinatown Alley to dispose of waste and recycling. It is strongly recommended that an internal entrance be incorporated into the trash room design to provide residents and staff a convenient and efficient entrance and to provide increased security while disposing of materials.
- 88. Applicant must provide a statement of how trash, recycling and organics will be organized and collected after project is complete, per City Code Chapter 17.616.020. This statement must explain how the property manager will collect from the trash, recycling and organics receptacles located around the site and place each material type in the appropriate bin in the trash enclosure.
- 89. Solid waste trucks must be able to safely move about the project, with minimum backing, and able to empty the bins and cans safely.

90. This project will be required to submit a Construction and Demolition (C&D) Debris plan, as outlined on the City's website at http://www.cityofsacramento.org/public-works/RSW/Collection-Services/Recycling/Construction-and-Demolition. Please contact the Solid Waste C&D team if you have any questions: Phone: (916) 808-0965, Email: <u>C&D@cityofsacramento.org</u>

Wilton Rancheria- Cultural Preservation Department

- 91. If potential tribal cultural resources (TCRs), archaeological artifacts, other cultural resources, articulated, or disarticulated human remains are discovered during construction activities, all work will cease within 100 feet of the find (based on the apparent distribution of the resources. Examples of potential cultural materials include but are not limited to midden soils, artifacts, chipped or worked stone, baked clay, shell, or bone.)
- 92. A Native American Representative from the federally recognized, Wilton Rancheria will assess the significance of the find and make recommendations for further evaluation and treatment if necessary. Culturally appropriate treatment that preserves or restores the cultural qualities and integrity of a Tribal Cultural Resource may be, but is not limited to, processing materials for reburial, minimizing handling of cultural objects, leaving objects in place within the landscape, construction monitoring of any further activities by a tribal representative, and or returning the objects to a location within the project area where they will not be subject to future impacts.
- 93. Wilton Rancheria does not consider curation of TCRs to be appropriate or respectful and requests that materials not be permanently curated, unless specifically requested by the Tribe.
- 94. If any human remains are discovered during construction activities, the County Coroner and the Native American Heritage Commission shall be contacted immediately. Upon determination by the County Coroner that the remains are Native American in origin, the Native American Heritage Commission will assign the Most Likely Descendant(s) (MLD) who will work the project proponents to define proper treatment and disposition.
- 95. After review of the find and consultation with the MLD, the authority to proceed may be accompanied by the addition of development requirements which provide for protection and preservation of the site and/or additional measures necessary to address the sensitive and unique nature of the site. All treatment recommendations made by the tribe and other cultural resources specialists will be documented in the confidential portion of the project record. Work in the area(s) of the cultural find may only proceed after authorization from the lead agency in coordination with the Tribe.

Advisory Notes

The following advisory notes are informational in nature:

Planning

ADV1. Alternate Water Systems Requirement. Pursuant to Chapter 15.24.030, 15.24.040 and 15.24.050 of Sacramento City Code, beginning on July 1, 2023, new buildings that are 10,000 square feet or greater must include a gray water system to provide subsurface irrigation and buildings that are 50,000 square feet or greater must include installation of a separate, additional piping system for an on-site treated non-potable gray water system for water closets and urinals. Limited exceptions apply. Please see City webpage for more details.

Complete building permit applications (including payment of all required fees) filed with and accepted by the City's Building Division prior to July 1, 2023, will not be subject to Alternate Water Systems requirements. Applicants are advised to plan for alternate water systems beginning with initial early design. For more information, please visit the City's website: https://www.cityofsacramento.org/Community-Development/Planning/Major-Projects/General-Plan/About-The-Project/Climate Change/Onsite-Water-Reuse-Study.

Department of Utilities

- ADV2. Many projects within the City of Sacramento require on-site booster pumps for fire suppression and domestic water systems. Prior to design of the subject project, the DOU suggests that the applicant request a water supply test to determine what pressure and flows the surrounding public water distribution system can provide to the site. This information can then be used to assist the engineers in the design of the on-site fire suppression system.
- ADV3. Water meters shall be located at the point of service, which is back of curb for separated sidewalks, back of walk for connected sidewalks or at the inside edge of the right-of-way for public alleys.
- ADV4. As of January 1, 2018, all new multi-family residential construction will require sub-metering of each residential unit pursuant to all requirements found in California Senate Bill 7 (SB7). These requirements at a minimum include installing sub meters, billing for water based on the sub meters, and long-term calibration and maintenance of the sub meter. The SB7 requirements are the responsibility of the property owner.

ADV5. The proposed project is located in a Zone X on the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Maps (FIRMs). Accordingly, the project site lies in an area with no requirements to elevate or flood proof.

Park Planning and Development Services

- ADV6. As per City Code, the applicant will be responsible to meet his/her obligations regarding:
 - a. Title 18, 18.56 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee due for this project is estimated at \$203,073. The Park Development Impact Fee due for this project is based on the Central City Zone rate of \$2.15 per square foot for residential projects, with a minimum rate of \$1,599 for units under 750 square feet and a maximum of \$4,267 for units over 2,000 square feet. The applicant would likely receive credit for the demolition of any existing structures. The demolition credit, based on the warehouse rate of \$0.20 per square foot, is \$820. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is <u>submitted</u> for building permit.
 - b. Community Facilities District 2002-02, Neighborhood Park Maintenance CFD Annexation.

Police

ADV7. City of Sacramento permits must be obtained for private patrol, alarms, and camera systems.

Solid Waste

ADV8. Standard Truck Dimensions

				Inside Turning	
	Height			Circle	Pickup
Туре	Clearance	Length	Width	Diameter	Clearance
Side Loader	13 ft.	32 ft.	10 ft.	62 ft.	17 ft.
Rear Loader	13 ft.	35 ft.	10 ft.	47 ft.	13 ft.
Front Loader	14 ft.	33 ft.	10 ft.	49 ft.	20 ft.

ADV9. Standard Bin and Can Dimensions

Size	Height	Depth	Width
32 gal. can	39 in.	25 in.	20 in.
64 gal. can	41 in.	30 in.	28 in.
96 gal. can	47 in.	35 in.	29 in.
1 yd. bin	4 ft.	2 ft., 9 in.	6 ft., 10 in.
2 yd. bin	4 ft., 5 in.	4 ft.	6 ft., 10 in.
3 yd. bin	5 ft., 1 in.	3 ft., 7in.	6 ft., 10 in.
4 yd. bin	5 ft., 9 in.	4 ft., 8 in.	6 ft., 10 in.
5 yd. bin	5 ft., 3 in.	5 ft., 9 in.	6 ft., 10 in.
6 yd. bin	6 ft.	5 ft., 10 in.	6 ft., 10 in.

DRAFT FINDINGS OF FACT

Environmental

Environmental Determination

Section 1. Staff finds as follows:

- a. On April On April 19, 2018, pursuant to the California Environmental Quality Act (Public Resources Code §21000 *et seq*. ("CEQA"), the CEQA Guidelines (14 California Code of Regulations §15000 *et seq*.), and the City of Sacramento environmental guidelines, the City Council approved an Environmental Impact Report (EIR) and adopted Findings of Fact and Statement of Overriding Considerations and approved the Central City Specific Plan.
- b. The 1300 C Apartments (project) is a residential development project within the meaning of PRC section 21155.4(a).
- c. The project is located in a transit priority area within the meaning of PRC section 21155.4(a)(1).
- d. The project is consistent with the Central City Specific Plan as required by PRC section 21155.4(a)(2).
- e. The project is consistent with the general use designation, density, building intensity, and applicable policies specified for the project area as set forth in the Sacramento Area Organization of Governments (SACOG) Metropolitan Transit Plan/Sustainable Communities Strategy, which has been accepted by the California Air Resources Board as applicable achieving greenhouse gas emission reduction targets.

f. There have been no substantial changes proposed in the specific plan that would require major changes in the CCSP EIR, or changes in the circumstances under which the EIR was prepared or new information that has become available.

Section 2. Upon approval of the 1300 C Apartments Project (DR23-088), the Planning Director shall file or cause to be filed a Notice of Exemption with the Sacramento County Clerk and, if the project requires a discretionary approval from any state agency, with the State Office of Planning and Research, pursuant to section 21152(a) of the Public Resources Code and the State EIR Guidelines adopted pursuant thereto.

Section 3. Pursuant to Guidelines section 15091(e), the documents and other materials that constitute the record of proceedings upon which the Design Director based its decision, including the previously-certified EIR, are located in and may be obtained from, the Community Development Department at 300 Richards Boulevard, Third Floor, Sacramento, California 95811. The Planning Director is the custodian of records for all matters before the Planning and Design Commission.

Site Plan and Design Review

- 1. The design, layout, and physical characteristics of the proposed project are consistent with the general plan and any applicable specific plan or transit village plan in that the proposal adheres to the goals and policies of the general plan land use designation of Employment Center Low Rise (ECLR).
- 2. The design, layout, and physical characteristics of the proposed project are consistent with all applicable design guidelines and with all applicable development standards in that the proposal meets all applicable development standards for the M-1-SPD zone and the architectural design standards for multi-unit dwelling design guidelines within the Central City Design Review Area. Furthermore, the project substantially conforms with the Central City design principles in relation to massing and scale for commercial structures due to the reasoning discussed within the Site Plan and Design Review analysis portion of the report.
- 3. All streets and other public access ways and facilities, parking facilities, and utility infrastructure are adequate to serve the subject site and comply with all applicable design guidelines and development standards.
- 4. The design, layout, and physical characteristics of the proposed project are visually and functionally compatible with the surrounding neighborhood in that the exterior design and massing of the project are compatible with the design of the adjacent commercial structures.

- 5. The design, layout, and physical characteristics of the proposed project ensure energy consumption is minimized and use of renewable energy sources is encouraged.
- 6. The design, layout, and physical characteristics of the proposed project are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance in that the proposal meets height and setback requirements and will not be a detriment to the neighborhood.

Tree Permit

The Tree Permit for the removal of two (2) City trees and one (1) private protected tree is approved based on the following Findings of Fact:

- 7. The trees are proposed for removal because they conflict with the project footprint, associated grading and construction, and required infrastructure improvements.
- 8. A tree replacement waiver for the equivalent of 86 inches DSH (60 City, 26 private) due to poor tree conditions and uncorrectable structural defects, documented according to the Arborist report and related exhibits, is consistent with the replacement waiver requirements set forth in section 12.56.060 of the Tree Planting, Maintenance and Conservation Ordinance.

200-Year Flood Protection

7. The project site is within an area for which the local flood-management agency has made adequate progress (as defined in California Government Code section 65007) on the construction of a flood-protection system that, for the area intended to be protected by the system, will result in flood protection equal to or greater than the urban level of flood protection in urban areas for property located within a flood-hazard zone, as demonstrated by the SAFCA Urban Level of Flood Protection Plan and Adequate Progress Baseline Report and the SAFCA Adequate Progress Toward an Urban Level of Flood Protection Engineer's Report, each accepted by the City Council on June 21, 2016 (Resolution No. 2016-0226), and the SAFCA 2023 Adequate Progress Annual Report accepted by the City Council on October 24, 2023 (Resolution No. 2023-0337).

Respectfully Submitted:

Sierra Peterson Associate Planner

Recommendation Approved: Matthew Sites (May 3, 2024 10:17 PDT)

Matthew Sites Senior Architect, AIA

The decision of the Design Director may be appealed to the Planning and Design Commission. An appeal must be filed within 10 days of the Design Director's hearing. If an appeal is not filed, the action of the Design Director is final.





RENDERING - OVERALL BUILDING - NORTHWEST 12" = 1'-0"



SHEET LIST

G-000	COVER SHEET
G-001	NIGHT RENDE
G-002	MAIN ENTRY R
G-003	CODE ANALYS
G-004	SITE DEVELOF
G-005	STREETSCAPE
02 CIVIL	
C 1.0	EXISTING CON
C 2.0	DEMOLITION F
C 3.0	SURFACE IMPI
C 3.1	SURFACE IMPI
C 4.0	GRADING PLA
C 4.1	GRADING PLA
C 5.0	UTILITY PLAN
03 LANDS	CAPE
L-1	PRELIMINARY
	ARCHITECTUR
SD-102	DEMOLITION F

01 GENERAL

SD-101	ARCHITECTUR
SD-102	DEMOLITION F
SD-103	FLOOR PLANS
SD-104	OPEN SPACE I
SD-201	BUILDING ELE
SD-202	BUILDING ELE
SD-203	3D PERSPECT
SD-205	TRASH ROOM

ERING RENDERING 'SIS OPMENT PHOTOS PE ELEVATIONS

NDITIONS PLAN PROVEMENTS PROVEMENTS

Y LANDSCAPE PLAN

JRAL SITE PLAN PLAN IS & ROOF PLAN EEXHIBIT EVATIONS EVATIONS TIVES

M & BIKE ROOM EXHIBIT

PROJECT TEAM

OWNER SKK DEVELOPMENTS 2409 L Street, Suite 200 Sacramento, CA 95816 CONTACT: Alberto Esquivel PH. 916-449-8989 E. alberto@skkdevelopments.com

ARCHITECT 19SIX ARCHITECTS 1715 R Street, Suite 200 Sacramento, CA 95811 CONTACT: Terry Green PH. 916-786-8178 tgreen@19six.com

<u>CIVIL</u> BKF ENGINEERS 980 9th Street, Suite 2300 Sacramento, CA 95814 CONTACT: James McCurdy PH. 916-556-5800 E. jmccurdy@bkf.com

<u>LANDSCAPE</u> YAMASAKI LANDSCAPE ARCHITECTURE 1223 High Street Auburn, CA 95603 CONTACT: Theresa Zaro PH. 530-885-0040 E. theresa@yamasaki-la.com



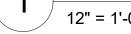








1 NIGHT RENDERING - NORTHWEST CORNER





13 | C STUDIOS SKK DEVELOPMENTS SACRAMENTO, CA











1 RENDERING - MAIN BUILDING ENTRY - 13TH STREET













BUILDING CODE SUP	MMARY						
Project name Export date	13C 02/22/2023						
APPLICABLE CODES California Building Code 2022							
BUILDING DATA							
<u>Story</u>	Space Name	Occupancy Group	Function of Space	Designed Area (gross sq. ft.)	Area Per Story		
1	Residential North Mech/Elect/Bike	R-2	Residential Residential	7,900	17,200		
	Residential South	R-2 R-2	Residential Residential	700 7,900			
2 to 3	Mech/Elect/Bike Residential Northo	R-2 R-2	Residential Residential	700 8,600	17,200		
	Residential South	R-2	Residential	8,600			
				Total Gross Area:	51,600		
<u>Sprinkler type</u> NFPA 13	<u>Type of Construction</u> VA	Fire Separation	<u>High-Rise</u> No				
HEIGHTS AND AREAS							
Building Area Single Occupancy		503					
Occupancy Group R-2	<u>Designed Area (sq. ft.)</u> 51,600	<u>Allowed Area (Aa) (sq. ft.)</u> 72,000					
Factor Increase was determine The calculation incorporates t		prinklors					
Per Section 508.3.1, use area	requirements for most restrictiv	ve occupancy group per story. O		rictive area: R-2 = 72,000 sq. ft.			
a. The area of any accessory o	ccupancy has been added to th	e designed area of the main occ	upancy per Section 508.2.				
Building Height in Stories		504.4					
Single Occupancy							
Occupancy Group	Highest Story Above Grade Plane Occupancy Appears On	Highest Allowable Story Above Grade Plane	2				
R-2	3	4					
Accessory occupancies are no	t included in these calculations	per Section 508.2					
The calculation incorporates t							
Building Height in Feet Single Occupancy		504.3					
	Highest Elevation Above	Highest Allowable Elevation					
Occupancy Group R-2	<u>Grade Plane Occupancy</u> <u>Appears On</u> 36.0 ft.	Above Grade Plane 60 ft.					
Average Roof Surface Height	Highest Allowable Elevation						
Above Grade Plane 36.0 ft.	<u>Above Grade Plane</u> 60 ft.						
Accessory occupancies are no		nor Section EOS 2					
The calculation incorporates t							
EGRESS INFORMATION							
Occupant Loads							
Story	Space Name	Occupancy Group	Function of Space	Designed Area (gross sq. ft.)	Designed Area (net sq. ft.)	OLF	Occupant Load
1	Residential North	R-2	Residential	7,900	-	200	40
	Mech/Elect/Bike Residential South	R-2 R-2	Residential Residential	700 7,900	-	200 200	4 40
	Mech/Elect/Bike	R-2	Residential	700	-	200	4
2 to 3	Residential Northo Residential South	R-2 R-2	Residential Residential	8,600 8,600	-	200 200	43 43
Min. Exits per Space More than one exit must be p	rovided if the space occupant lo	1006.2.1 bad or maximum common path o	1006.3.3 of egress travel distance are exc	ceeded. More exits may be requ	ired under high occupant load	or other special conditions (see	e notes below)
Story	<u>Space Name</u>	Occupancy Group	Function of Space	Cum. Occupant Load of Space	Max Single Exit Cumulative	Min. Number of Exits per	Max. Common Path of Egress
	Residential Northo	R-2			Load of Space	<u>Space</u>	Allowed for Single Exit
2 to 3	Residential South	R-2 R-2	Residential Residential	43 43	20 20	2 2	125 125
1	Residential North Mech/Elect/Bike	R-2 R-2	Residential Residential	40 4	20 20	2	125 125
	Residential South	R-2	Residential	40	20	2	125
	Mech/Elect/Bike	R-2	Residential	4	20	1	125
Min. Exits per Story		1006.3.3					
Story	Total Occupancy Load by Stor	Y <u>Minimum Number of Exits or</u> <u>Exit Accesses</u>	<u>Max Exit Access Travel</u> Distance for 1 Exit				
3	86	2	n/a				
2 1	86 88	2 2	n/a n/a				
Max. Exit Access Travel Distan		1017.2 Id be at a maximum the values b	alow for each accurancy group				
Occupancy Group	Max Exit Access Travel	id be at a maximum the values t	below for each occupancy group	D.			
R-2	<u>Distance</u> None ft.						
Max. Dead-End Corridor Leng		1020.5					
	h occupancy group should be a <u>Max Dead-end Corridor</u>	t a maximum the length given b	elow.				
Occupancy Group R-2	Distance 50 ft.						
		one exit or exit access doorway i ne dead-end corridor is less thar					



Story	Corridor	Occupancies Served	Cum. Occupant Load	Min. Corridor Width					1
1	North Corridor	R-2	8	36" (See note a)					
1	South Corridor	R-2	8	36" (See note a)					
2 to 3	North Corridor	R-2	8	36" (See note a)					
2 to 3	South Corridor	R-2	8	36" (See note a)					
Use 24" for access to and	utilization of mechanical plum	bing or electrical systems or equ	uipment						
				all not reduce the results in the	humora than 7 in the	no position may reduce the	ad width human that	alf	
		xcept where allowed by Section	נטטב.7. Fully opened doors sha	an not reduce the required width	by more than 7 inches, and ir	n no position may reduce the requir	eu width by more than one-f	idii.	
a. Use 36" for corridors wi	tnın a dwelling unit								
Min. Stairway Width		1005.3.1	1011.2						ZONING ANALYSIS - 13C
Story	<u>Stairway</u>	Min. Calculated Value	Min. Value per Sec. 1011	L.2 Final Min. Stairway Width					ITEM SUBJECT
1	Central Stair	5.4"	36"	36"					
	North Stair	2.7"	36"	36"					1 APN : 002-0084-001 and Ø 02
2 to 3	South Stair Central Stair	2.7" 4.5"	36" 36"	36" 36"					
2 10 5	North Stair	4.5 2.4"	36"	36"					Existing Addresses: 1300 and
	South Stair	2.4"	36"	36"					1300 C Street: 12,935 SF / 0.
									424 12th Street: 12,935 SF /
									Total Lot Size: 25,870 / .594 a 2 Zoning Info
FIRE-RESISTANCE RATING	5								M-1-SPD Industrial / SPD (Ce
Interior Building Element	Fire-Resistance Rating	704.1							Low Rise
Story	Interior Building Element	Type Min. Fire Resistance Ratin	ngs						Density (Land Use Diagram):
<u>_</u>									FAR: Residential-Only not lim
	Primary structural frame Interior Bearing Walls	1 1							Max Bldg Lot Coverage: No F
	Interior Bearing Walls	s and							
	partitions	0							
	Floor construction and								3 Maximum Allowed Height
	associated secondary	1							
	structural members								Zoning Code max height: 70'
	Roof construction and associated secondary	1							
	structural members	1							
									Stories: 1-3
									A Control City Desiry Desiry
Min. Fire Wall Ratings	t most the minimum fire and it	706.4 tance ratings below by occupant	ov group						4 Central City Design Review I Guidelines Apply
	Min. Fire Wall Resistance		cy Broup						6 Parking Requirements
Occupancy Group	Rating (hrs.)	-							Traditional Parking District –
R-2	2								
Fire walls for occurs		R-21 R-22 and Huithin Tur-	Il or V construction are servit	ted to have a reduced fire resist-	nce rating These values are	eflected above			Short-Term Bicycle Parking: 1
Fire wans for occupancy g	оирз А, Β, Ε, Η-4, Ι, L, К-1, К-2,	n-2.1, n-2.2, and U within Type	in or v construction are permit	ted to have a reduced fire-resista	nce rading. These values are r	enected above.			Long-Term Bicycle Parking: 1
1									II I
									**Transit Oriented Develop
Min. Fire Area Separation	Ratings	707.3.10							**Transit Oriented Develop modification
	Ratings Min. Fire Area Separation								modification
Min. Fire Area Separation									
	Min. Fire Area Separation								Reduced parking requiremen station off-street vehicle par 8 Setbacks
Occupancy Group R-2	Min. Fire Area Separation Rating (hrs.) 2	<u>1</u>	itomatic fire sprinkler system t	the fire walls fire barriers berize	atal assemblies, or the sombi	nation thoroof must be constructed	in accordance with Table 70	7.2.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mini-
<u>Occupancy Group</u> R-2 Where a building is divide	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as	<u>1</u>	utomatic fire sprinkler system, t	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	Reduced parking requiremen station off-street vehicle par 8 Setbacks
Occupancy Group R-2	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as	<u>1</u>	utomatic fire sprinkler system, t	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mint • Street Side-Yard Setback: No
<u>Occupancy Group</u> R-2 Where a building is divide	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as	<u>1</u>	utomatic fire sprinkler system, t	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mint • Street Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint 9 Open Space for Multi-Unit D
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as 1. nce Ratings	n s to eliminate the need for an au 1020.2	utomatic fire sprinkler system, t	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mint • Street Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint 9 Open Space for Multi-Unit D Sections 17.600.135 A and 1
<u>Occupancy Group</u> R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t	<u>Min. Fire Area Separation</u> <u>Rating (hrs.)</u> 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat	n s to eliminate the need for an au <u>1020.2</u> tings below		the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mint • Street Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint 9 Open Space for Multi-Unit D
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u>	n s to eliminate the need for an au <u>1020.2</u> tings below <u>Occupancies Served</u>	Corridor Rating (hrs.)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requiremer station off-street vehicle par 8 Setbacks • Front-Yard Setback: No Min • Street Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min 9 Open Space for Multi-Unit D 9 Sections 17.600.135 A and 1 dwellings DO NOT apply with apply
<u>Occupancy Group</u> R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t	<u>Min. Fire Area Separation</u> <u>Rating (hrs.)</u> 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor	n s to eliminate the need for an au <u>1020.2</u> tings below <u>Occupancies Served</u> R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part station off-station off-stationo
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1	<u>Min. Fire Area Separation</u> <u>Rating (hrs.)</u> 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor	n s to eliminate the need for an au <u>1020.2</u> tings below <u>Occupancies Served</u> R-2 R-2 R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requiremer station off-street vehicle par 8 Setbacks • Front-Yard Setback: No Min • Street Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min 9 Open Space for Multi-Unit D 9 Sections 17.600.135 A and 1 dwellings DO NOT apply with apply
<u>Occupancy Group</u> R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t	<u>Min. Fire Area Separation</u> <u>Rating (hrs.)</u> 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor	n s to eliminate the need for an au <u>1020.2</u> tings below <u>Occupancies Served</u> R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part station off-station off-stationo
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1 2 to 3	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor North Corridor South Corridor	1020.2 tings below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2 R-2 R-2 R-2	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part station off-station off-stat
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1 2 to 3	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor North Corridor South Corridor	1020.2 tings below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requirement station off-street vehicle part 8 Setbacks • Front-Yard Setback: No Mint • Street Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint • Interior Side-Yard Setback: No Mint 9 Open Space for Multi-Unit D Sections 17.600.135 A and 1' dwellings DO NOT apply with apply Traditional zone for Open Space 50 square feet of open space front-yard, side-yard, and reminimum depth of 3 feet. Co depth of 20 feet.
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1 2 to 3	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor North Corridor South Corridor	1020.2 tings below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2 R-2 R-2 R-2	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requiremer station off-street vehicle par 8 Setbacks • Front-Yard Setback: No Min • Street Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min 9 Open Space for Multi-Unit D Sections 17.600.135 A and 1 dwellings DO NOT apply with apply Traditional zone for Open Space front-yard, side-yard, and reminimum depth of 3 feet. Co depth of 20 feet. 10 M-1 Light Industrial Zone in A. Allowed uses. Except
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1 2 to 3 a. This rating is not require	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor South Corridor South Corridor South Corridor	1020.2 tings below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2 R-2 R-2	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requiremer station off-street vehicle par 8 Setbacks • Front-Yard Setback: No Min • Street Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min 9 Open Space for Multi-Unit D Sections 17.600.135 A and 1' dwellings DO NOT apply with apply Traditional zone for Open Space 50 square feet of open space front-yard, side-yard, and reminimum depth of 3 feet. Co depth of 20 feet. 10 M-1 Light Industrial Zone in A. Allowed uses. Except zone outside the Central
Occupancy Group R-2 Where a building is divide higher rating must be used Min. Corridor Fire-Resista Each corridor must meet t Story 1 2 to 3	Min. Fire Area Separation Rating (hrs.) 2 d into separate fire areas, so as d. nce Ratings he minimum fire resistance rat <u>Corridor</u> North Corridor South Corridor South Corridor South Corridor South Corridor	1020.2 tings below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2 R-2 R-2 R-2	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	the fire walls, fire barriers, horizo	ntal assemblies, or the combi	nation thereof must be constructed	in accordance with Table 70	7.3.10. In a mixed occupancy the	modification Reduced parking requiremer station off-street vehicle par 8 Setbacks • Front-Yard Setback: No Min • Street Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min • Interior Side-Yard Setback: No Min 9 Open Space for Multi-Unit D Sections 17.600.135 A and 1' dwellings DO NOT apply with apply Traditional zone for Open Space front-yard, side-yard, and reminimum depth of 3 feet. Co depth of 20 feet. 10 M-1 Light Industrial Zone in A. Allowed uses. Except zone outside the Central Central City SPD.
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Min. Stairway Width		1005.3.1	1011.2							NG ANALYSIS - 13C
tory	<u>Stairway</u>	Min. Calculated Value	Min. Value per Sec. 1011.2	Final Min. Stairway Width	<u>1</u>					SUBJECT
	Central Stair	5.4"	36"	36"						
	North Stair	2.7"	36"	36"					1	APN: 002-0084-001 and 202-0084-002
to 3	South Stair Central Stair	2.7" 4.5"	36" 36"	36" 36"						Existing Addresses: 1300 and 1310 C St
0.0	North Stair	2.4"	36"	36"						Existing Addresses. 1500 and 1510 C 5
	South Stair	2.4"	36"	36"						1300 C Street: 12,935 SF / 0.297 acres
										424 12th Street: 12,935 SF / .297 acres Total Lot Size: 25,870 / .594 acres
RE-RESISTANCE RAT									2	Zoning Info
RE-RESISTANCE RATI	INGS									M-1-SPD Industrial / SPD (Central City F
terior Building Elem	ent Fire-Resistance Rating	704.1								Low Rise
	Interior Building Element Ty	Min. Fire Resistance Ratin	gs							Density (Land Use Diagram): N/A (2035
ory		(hrs.)								
	Primary structural frame Interior Bearing Walls	1								FAR: Residential-Only not limited by FA Max Bldg Lot Coverage: No Restrictions
	Interior Nonbearing walls a	nd								
	partitions	0								
L	Floor construction and								3	Maximum Allowed Height
	associated secondary	1								
	structural members Roof construction and									Zoning Code max height: 70'
	associated secondary	1								
	structural members									
										Stories: 1-3
	-	706 4							4	Central City Design Review District and
1in. Fire Wall Ratings ny fire walls present i	s must meet the minimum fire resistar	706.4 nce ratings below by occupanc	v group							Guidelines Apply
	Min. Fire Wall Resistance		1 0, 0 % P						6	Parking Requirements
ccupancy Group	Rating (hrs.)									 Traditional Parking District – Multi-Unit
-2	2									
iro walls for occupant	cy groups A, B, E, H-4, I, L, R-1, R-2, R-	21 P 22 and U within Type	Il or V construction are normitted	d to have a reduced fire resists	ance rating Those values are r	offected above				Short-Term Bicycle Parking: 1 space per Long-Term Bicycle Parking: 1 space per
	cy groups A, B, E, 11-4, I, L, N-1, N-2, N-	-2.1, R-2.2, and 0 within Type			ance fatting. These values are f	enected above.				Long-Term bicycle Farking. I space per
										**Transit Oriented Development Ordir
/lin. Fire Area Separat	tion Ratings	707.3.10								modification
	Min. Fire Area Separation									Reduced parking requirements near tra
Occupancy Group										incouced parking requirements near tha
	Rating (hrs.)									station off-street vehicle parking not re
<u>Occupancy Group</u> R-2									8	station off-street vehicle parking not re Setbacks
R-2	<u>Rating (hrs.)</u> 2	o eliminate the need for an au	tomatic fire sprinkler system, the	e fire walls, fire barriers, horizo	ontal assemblies, or the combi	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	8	station off-street vehicle parking not re
R-2 Where a building is div	Rating (hrs.) 2 vided into separate fire areas, so as to	o eliminate the need for an au	tomatic fire sprinkler system, the	e fire walls, fire barriers, horizo	ontal assemblies, or the combi	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	8	station off-street vehicle parking not re Setbacks • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or N • Interior Side-Yard Setback: 5'
2-2 Vhere a building is div	Rating (hrs.) 2 vided into separate fire areas, so as to	o eliminate the need for an au	tomatic fire sprinkler system, the	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	8	station off-street vehicle parking not re Setbacks • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or N • Interior Side-Yard Setback: 5' • Rear-Yard Setback: No Min or Max
2-2 Vhere a building is div igher rating must be u	Rating (hrs.) 2 vided into separate fire areas, so as to used.		tomatic fire sprinkler system, the	e fire walls, fire barriers, horizc	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	8	station off-street vehicle parking not re Setbacks • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or N • Interior Side-Yard Setback: 5'
-2 Vhere a building is div igher rating must be u Ain. Corridor Fire-Res	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings	1020.2	tomatic fire sprinkler system, the	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident for the setbacks Front-Yard Setback: No Min or Max Street Side-Yard Setback: No Min or Max Interior Side-Yard Setback: 5' Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All
-2 Vhere a building is div igher rating must be u Ain. Corridor Fire-Res ach corridor must me	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin	<u>1020.2</u> Igs below		e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident sets and the set of the set o
-2 Vhere a building is div igher rating must be u Ain. Corridor Fire-Res ach corridor must me	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings	1020.2	tomatic fire sprinkler system, the <u>Corridor Rating (hrs.)</u> 1 (See note a)	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident for the setbacks Front-Yard Setback: No Min or Max Street Side-Yard Setback: No Min or Max Interior Side-Yard Setback: 5' Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All
-2 Vhere a building is div igher rating must be u <u>Ain. Corridor Fire-Res</u> ach corridor must me <u>tory</u>	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin, <u>Corridor</u> North Corridor South Corridor	1020.2 Igs below Occupancies Served R-2 R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a) 1 (See note a)	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident in the street side set back: No Min or Max • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or Max • Interior Side-Yard Setback: S' • Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All apply Traditional zone for Open Space District common open space shall be provided
R-2 Where a building is div higher rating must be u Min. Corridor Fire-Res Each corridor must me <u>story</u>	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin <u>Corridor</u> North Corridor South Corridor North Corridor	1020.2 Igs below Occupancies Served R-2 R-2 R-2 R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a) 1 (See note a) 1 (See note a)	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident in the street side set back: No Min or Max • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or Max • Interior Side-Yard Setback: S' • Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All apply Traditional zone for Open Space District common open space shall be provided to so square feet of open space per dwelling
R-2 Where a building is div higher rating must be u Min. Corridor Fire-Res Each corridor must me <u>story</u>	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin, <u>Corridor</u> North Corridor South Corridor	1020.2 Igs below Occupancies Served R-2 R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a) 1 (See note a)	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not resident in the street side set back: No Min or Max • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or Max • Interior Side-Yard Setback: S' • Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All apply Traditional zone for Open Space District common open space shall be provided
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-2 Vhere a building is div igher rating must be u <u>Ain. Corridor Fire-Res</u> ach corridor must me tory to 3	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin <u>Corridor</u> North Corridor South Corridor North Corridor South Corridor	1020.2 ngs below <u>Occupancies Served</u> R-2 R-2 R-2 R-2 R-2	<u>Corridor Rating (hrs.)</u> 1 (See note a) 1 (See note a) 1 (See note a) 1 (See note a)	e fire walls, fire barriers, horizo	ontal assemblies, or the combin	nation thereof must be constructed in	accordance with Table 70	7.3.10. In a mixed occupancy the	9	station off-street vehicle parking not reside. Setbacks • Front-Yard Setback: No Min or Max • Street Side-Yard Setback: No Min or Max • Interior Side-Yard Setback: S' • Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135 C dwellings DO NOT apply within SPD. All apply Traditional zone for Open Space District common open space shall be provided 50 square feet of open space per dwell front-yard, side-yard, and rear-yard set minimum depth of 3 feet. Common open depth of 20 feet. M-1 Light Industrial Zone in Central Cite
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R-2 Where a building is divisingher rating must be of Min. Corridor Fire-Res fach corridor must me itory to 3 This rating is not req Required Occupancy S fee Section 420 for de PLUMBING Required Plumbing Fixed tory tory tory tory ther kitchen sink per apar kitchen sink per apar kitchen sink per apar	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin Corridor North Corridor South Corridor Separations etailed requirements for sleeping and xtures Occupancy Group R-2 R-2 R-2 rtment. 1 laundry tray or 1 automatic Water Closets	1020.2 Igs below Occupancies Served R-2 R-2 R-2 R-2 d within a dwelling unit or sleet 508.4 I dwelling units 1210 Space Description Apartment house Apartment house C clothes washer connection per	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) ping unit in Group R. OL 88 86 er unit or 1 laundry tray or 1 auto er unit or 1 laundry tray or 1 auto Lavatories	Water Closets 1 per apartment 1 per apartment 2 pomatic clothes washer connect 2 pomatic clothes washer connect 3 pathtubs and Showers	Urinals - - - tion for each 12 unit: tion for each 12 unit:	Lavatories 1 per apartment 1 per apartment 1 per apartment 1 per apartment 1 per apartment 1 per apartment. 1 kitchen sink per apartment. 1 laundry tray or 1 automatic	Bathtubs/Showers 1 per apartment 1 per apartment 1 per apartment		REQU 1. Thi requir	station off-street vehicle parking not responsible for corrective action taken station off-street vehicle parking not responsible for correcting a street Side-Yard Setback: No Min or Max Street Side-Yard Setback: 5' Rear-Yard Setback: No Min or Max Open Space for Multi-Unit Dwellings Sections 17.600.135 A and 17.600.135C dwellings DO NOT apply within SPD. All apply Traditional zone for Open Space District common open space shall be provided to 50 square feet of open space per dwelling front-yard, side-yard, and rear-yard setting minimum depth of 3 feet. Common open depth of 20 feet. M-1 Light Industrial Zone in Central City A. Allowed uses. Except as provided zone outside the Central City SPD ar Central City SPD. B. Conditional uses. Except as provided approval of a conditional use permit other restrictions or requirements of the M-1 zone outside the Central City SPD. C. Prohibited Uses. N/A D. Temporarily Prohibited Uses. N/A E. Parking. Maximum for Industrial and sproject shall contract with a project red to: a. Attend preconstruction meeting b. Visit the site before and after d month during construction to ensing c. Be responsible for correcting a to ensure corrective action taken concrete sidewalks and driveways stall repor
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-2 /here a building is division of the second seco	Rating (hrs.) 2 vided into separate fire areas, so as to used. sistance Ratings eet the minimum fire resistance ratin Corridor North Corridor South Corridor Separations etailed requirements for sleeping and xtures Occupancy Group R-2 R-2 R-2 rtment. 1 laundry tray or 1 automatic Water Closets	1020.2 Igs below Occupancies Served R-2 R-2 R-2 R-2 d within a dwelling unit or sleet 508.4 I dwelling units 1210 Space Description Apartment house Apartment house C clothes washer connection per	Corridor Rating (hrs.) 1 (See note a) 1 (See note a) ping unit in Group R. OL 88 86 er unit or 1 laundry tray or 1 auto er unit or 1 laundry tray or 1 auto Lavatories	Water Closets 1 per apartment 1 per apartment 2 pomatic clothes washer connect 2 pomatic clothes washer connect 3 pathtubs and Showers	Urinals - - - tion for each 12 unit: tion for each 12 unit:	Lavatories 1 per apartment 1 per apartment 1 per apartment 1 per apartment 1 per apartment 1 laundry tray or 1 automatic clothes washer connection pe unit or 1 laundry tray or 1 automatic clothes washer	Bathtubs/Showers 1 per apartment 1 per apartment 1 per apartment		REQU 1. Thi requir	station off-street vehicle parking not respective station off-street vehicle parking not respective action taken of the site before and after d month during construction to ensure corrective action taken of the site before and after d month during construction activities a. Existing driveways shall be used

separated fr fence that shall remain throughout the duration of the project from compacting the soil in the planters. 4. Construction trailers and port-a-potties shall be placed on protection zone or planter so as not to compact soil.

5. Any Regulated Work within the dripline or Tree Protection permitted prior to the start of construction and supervised by and a tree protection plan created by a Certified Arborist to L planning project number or off-site project number.

6. All excavation, grading, or trenching within the dripline of constructing foundations, footings, sidewalks, curbs, gutters following methods: Hydro-excavation, pneumatic excavation

a Certified Arborist. 7. There shall be no excavation deeper than the existing extrees.

8. There shall be no grade changes within the dripline of pro accommodated onsite.

9. There shall be no non-native soil, non-organic matter or s 10. The following is a list of activities that require a tree perm way planter and/or within the tree protection zone of protect

12.56, excavation, grade changes, trenches, root or canopy 11. The following is a list of activities that are prohibited with of protected trees: pedestrian and equipment traffic that cou parking vehicles, equipment and/or port-a-potties, storing of water or building refuse, disposing of wash water, paint, cer any other activities that may have negative impacts on the t

12. All trees shall be watered regularly according to the reco 13. The applicant shall be financially responsible for any da Accidental or negligent actions that damage City trees may

damages will be appraised by the City Urban Forester or his the monetary equivalent of all labor and materials required to utility with regards to its condition and function prior to the be



		CO
	SPECIFIC REQUIREMENTS / ALLOWABLE	CODE SECTION / REFERENCE STANDARD Parcel Map
g District) / Employment Center		Land Use Map
040 GP)	N/A 25,870 SF max	Table LU 1 - General Plan
		17.220 Article 1, 117.444.130 and Land Use Map
		17.444.130
	3 Stories	Table LU 1
n Review		17.600.100.B
ential: 1 spaces per unit**	No Parking Required	Figure 17.608-1
relling units lling units	126/10 = 13 short term 126/2 = 63 long term	17.444.090.F Table
Parking Requirement		17.608.030C 2020 TOD
ations: within 1/4 mile of a light rail		Ordinance
		17.220.140
ted to open space for multi-unit		17.444.050
provisions of section 17.600.135 combination of private and	50 sf x 126 units = 6,300	17.444.050
v multi-unit dwellings at a ratio of t beyond the minimum required Private open space must have a the must have a minimum width or	SF Min	17.444.030
ning District w, the uses allowed in the M-1 ved in the M-1 zone inside the		17.444.130
this section, if this title requires the ner discretionary permit or imposes		
establishment of a particular use in approval of the same the same restrictions or ne M-1 zone inside the Central City		
mmerical, N/A		
ES FOR CITY AND PRIVATE	PROTECTED TREES	
rist experienced with tree prote approve of and inform contractor tion, grading, and landscaping, at tree protection measures are e conditions that may negativel y implemented. riting to Urban Forestry all viola expected outcomes. e retained throughout construct	ors of all tree protection as well as at least twic e implemented and mai ly impact trees and revi- ations and tree protectio	measures. e each ntained. sit the site n failures
the sole access to the site. Wh one or two locations outside th vith the use of one or more of th wood or trench plates, geotexti t is approved by the City Arbor	ne dripline of protected t ne following: A 6-inch la le fabric covered by a 6	rees that yer of
separated from the constructio of the project to protect trees a		
be placed on existing hardscap soil.	C C	
ee Protection Zone of a protect upervised by a Certified Arbori Arborist to UrbanForestry@ci per.	st. Submit a tree permit	application
e dripline of a protected tree fo urbs, gutters, or any other reas ic excavation or hand digging a	on shall employ one of	the
e existing excavation for sidewa	alks within the dripline c	f protected
ripline of protected trees. All gr c matter or structural soil adde	-	nter
e a tree permit if they are to oc ne of protected trees: any regul	cur or be used within th	e right-of-
ot or canopy pruning, or boring ohibited within the right-of-way affic that could compact the soi	planter and/or tree prot	tection zone
es,storing of soil, construction r er, paint, cement, fuel or other acts on the trees and soil.	naterials, petroleum pro	oducts,
g to the recommendation of the		iaat
e for any damage to City trees y trees may result in a penalty. prester or his authorized repres ils required to bring the tree in o prior to the beginning of the pro	The monetary value of sentative and shall be e question to a state of co	any such xpressed as







SITE MAP









E







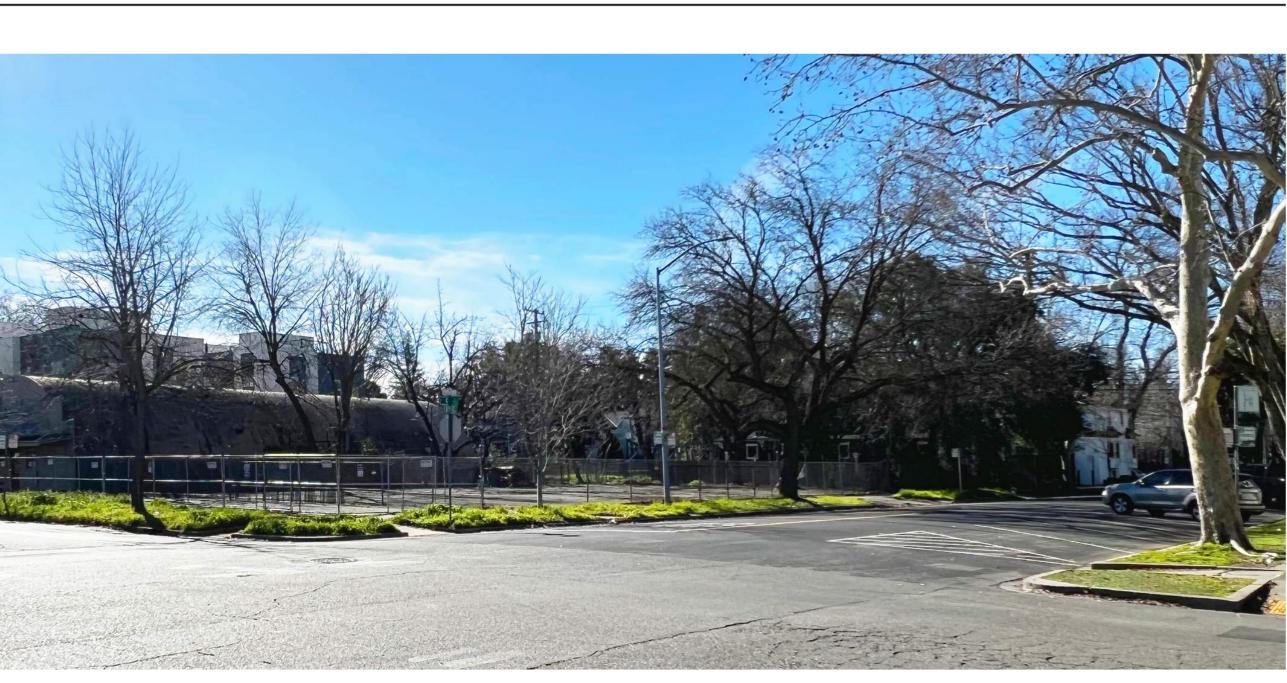
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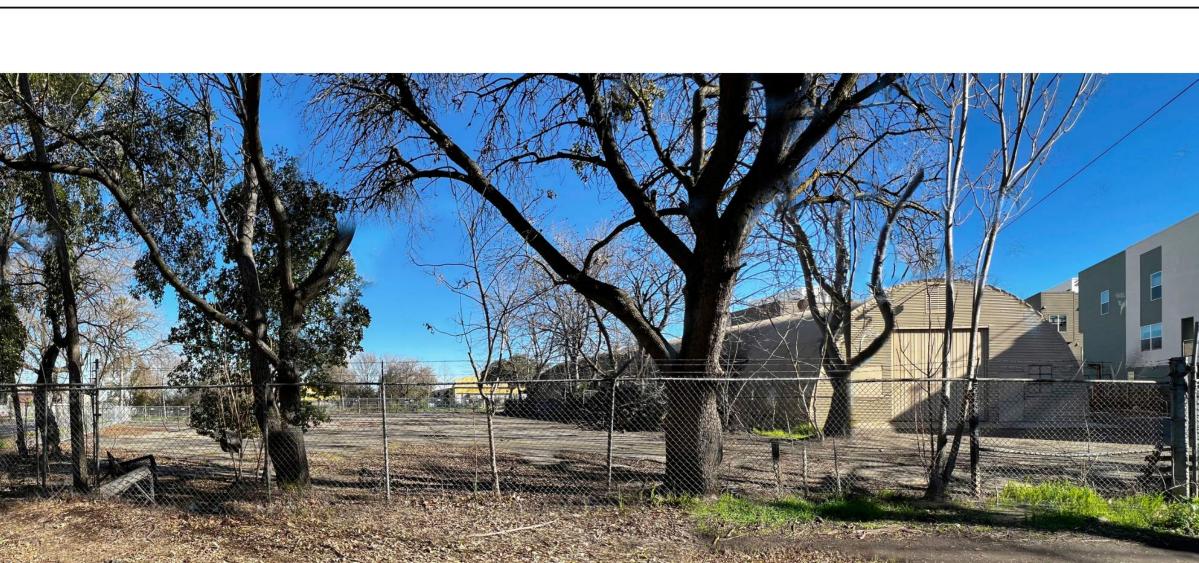


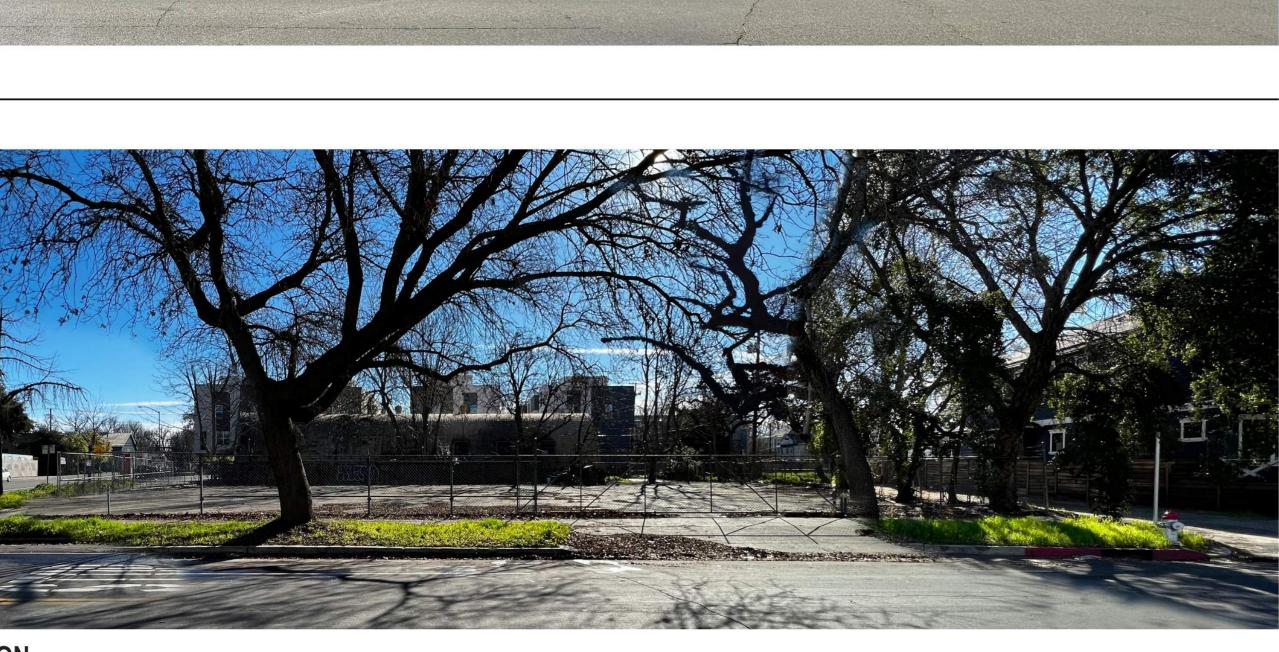




13TH & C STREET - NORTHWEST CORNER













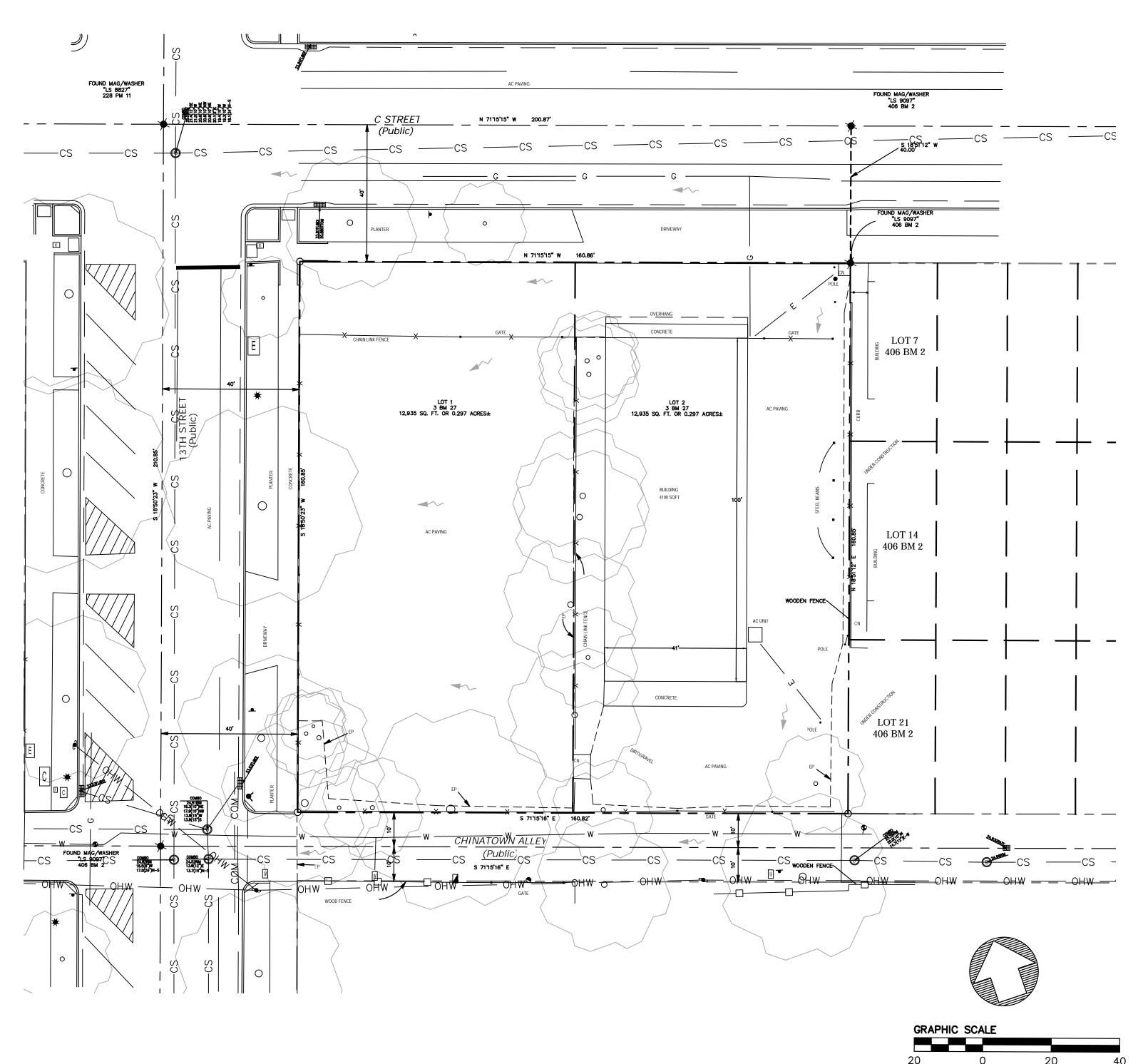












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EXISTING CONDITION NOTES

- 1. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE KNOWN UNDERGROUND UTILITIES. THE TYPES, LOCATIONS, EXTENTS, SIZES AND DEPTHS OF EXISTING UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE OBTAINED FROM VARIOUS SOURCES, INCLUDING TOPOGRAPHIC FIELD SURVEYS, AND AS-BUILT DRAWINGS PROVIDED BY THE PROPERTY OWNER. ABANDONED UNDERGROUND UTILITIES MAY EXIST, THE LOCATION OF WHICH ARE NOT KNOWN. CONTRACTOR IS CAUTIONED THAT THE TYPES, LOCATIONS, EXTENTS, SIZES AND DEPTHS OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS MAY NOT BE THE TYPES, LOCATIONS,
- EXTENTS, SIZES AND DEPTHS ENCOUNTERED IN THE FIELD. 2. ONLY EXCAVATION AND PHYSICAL EXPOSURE WILL REVEAL THE ACTUAL TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR POTHOLING EXISTING UNDERGROUND UTILITIES WITHIN THE AREA OF WORK. CONTRACTOR SHALL RETAIN A PRIVATE UTILITY LOCATOR TO IDENTIFY EXISTING UTILITIES. THE CONTRACTOR SHALL POTHOLE AS NECESSARY AT CONNECTION LOCATIONS AND PROPOSED UTILITY CROSSINGS TO FULLY LOCATE AND IDENTIFY EXISTING UTILITIES. POTHOLING SHALL BE DONE AND THE RESULTS, INCLUDING ANY CONFLICTS IDENTIFIED, REPORTED TO THE ENGINEER PRIOR TO THE START OF WORK.
- 3. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING HIS WORK TO AVOID CONFLICTS BETWEEN SEWER LATERALS, STORM DRAINS, WATER MAINS, AND ALL OTHER UNDERGROUND UTILITIES.
- 4. UTILITY INFORMATION IS OBTAINED BY AS-BUILT INFORMATION AS WELL AS SURVEY OBSERVATION. ALL EXISTING UTILITY INFORMATION IN THIS PLAN SET IS IDENTIFIED PER THE AMERICAN SOCIETY OF CIVIL ENGINEERS (ASCE) STANDARD FOR LEVEL OF QUALITY. BELOW IS A LIST OF THE LEVEL OF QUALITY CATEGORIES WITH THE ASSOCIATED DEFINITION PER ASCE STANDARDS.
- (C) INFORMATION OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION.
- (D) UTILITY LOCATION INFORMATION DERIVED FROM EXISTING RECORDS OR ORAL RECOLLECTIONS.

SURVEY NOTE: BOUNDARY AND TOPOGRAPHIC SURVEY BASED UPON FIELD SURVEY PERFORMED BY:

MORROW SURVEYING INC. IN AUGUST 2021 THIS SURVEY WAS PROVIDED TO BKF AS THE BASIS FOR DESIGN.

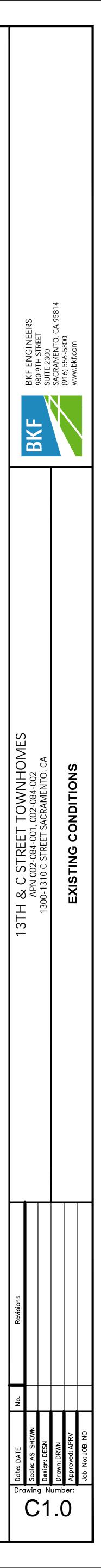
ABBREVIATIONS

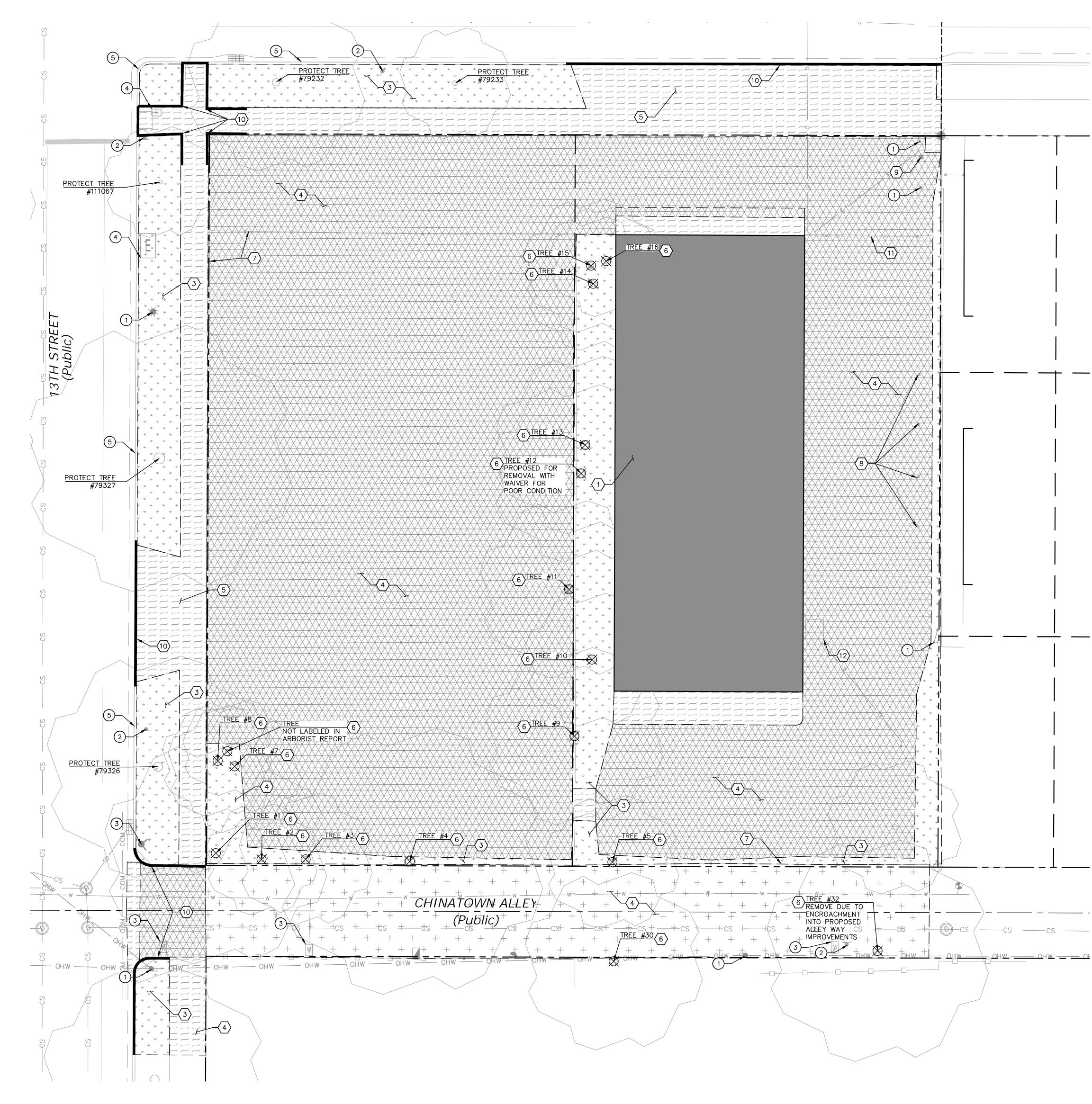
EA EP EG FF FH FL GB GND INV JP LG MH MON MISC OH PVC	ASPHALTIC CONCRETE BACK OF CURB BACKFLOW PREVENTER BUILDING BENCHMARK BOLLARD BACK OF WALK CENTERLINE CONCRETE COMMUNICATION COMBINED SYSTEM DIAMETER DRIVEWAY EDGE OF ASPHALT EDGE OF PAVEMENT ELECTRICAL EXISTING GRADE FINISH FLOOR ELEVATIO FIRE HYDRANT FLOW LINE GAS GRADE BREAK ELECTRICAL GROUND INVERT ELEVATION JOINT POLE LIP OF GUTTER MANHOLE MONUMENT MISCELLANEOUS OVERHEAD POLYVINYL CHLORIDE
INV	INVERT ELEVATION
JP	JOINT POLE
LG	LIP OF GUTTER
MH	MANHOLE
MON	MONUMENT
MISC	MISCELLANEOUS
PVC	POLYVINYL CHLORIDE
PVMT	PAVEMENT
RCP	RIGHT OF WAY REINFORCED CONCRETE REMOVABLE SEE ARCHITECTURAL DR
SD	STORM DRAIN
SDCB	STORM DRAIN CATCH B
SDMH	STORM DRAIN MANHOLE
SLD	SEE LANDSCAPE DRAWIN
STOR	STORAGE
TC	TOP OF CURB
UB	UTILITY BOX
WM	WATER METER
₩V	WATER VALVE

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LEGEND

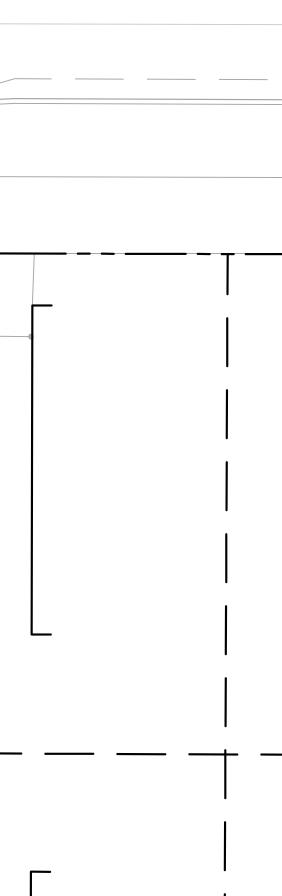
PROPOSED LOT LINE	
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OVERLAND RELEASE	





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DEMOLITION NOTES

- 1. CONTRACTOR TO USE CAUTION WHEN WORKING ADJACENT TO EXISTING BUILDING.
- 2. CONTRACTOR TO CLEAR AND GRUB WITHIN THE LIMITS OF PROPOSED WORK, UNLESS
- OTHERWISE NOTED ON PLANS. . RIM ELEVATIONS FOR EXISTING UTILITY STRUCTURES SHALL BE RAISED TO PROPOSED SURFACE ELEVATIONS.
- 4. THE UTILITY LINES AND STRUCTURES SHOWN ON THESE PLANS ARE DERIVED FROM RECORD DATA AND/OR SURFACE OBSERVATION AND ARE APPROXIMATE ONLY. ACTUAL LOCATION AND SIZE, TOGETHER WITH THE PRESENCE OF ANY ADDITIONAL UTILITY LINES AND STRUCTURES NOT SHOWN ON THIS PLAN, SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR DURING CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
- THE CONTRACTOR IS REQUIRED BY STATE LAW TO ACTIVELY RESEARCH THE WORK AREA PRIOR TO COMMENCEMENT OF CONSTRUCTION, REFERENCE ANY MONUMENTS, AND REPLACE THOSE DAMAGED OR REMOVED DURING CONSTRUCTION.
- 6. THE LIMITS OF DEMOLITION SHOWN ARE APPROXIMATE ONLY. CONTRACTOR IS RESPONSIBLE FOR MATCHING EXISTING SURROUNDINGS, LANDSCAPE AND OTHER IMPROVEMENTS WITH A SMOOTH TRANSITION IN PAVING, CURBS, GUTTERS, AND SIDEWALKS AND AVOIDING ANY ABRUPT OR APPARENT CHANGES IN GRADES OR CROSS SLOPES OR HAZARDOUS CONDITIONS.
- EXISTING CURB AND SIDEWALK AND LANDSCAPE/IRRIGATION WITHIN THE PROJECT LIMITS THAT ARE DAMAGED OR DISPLACED, EVEN THOUGH THEY WERE NOT TO BE REMOVED, SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST
- 8. PROTECT ALL EXISTING UTILITIES AND SITE FEATURES FROM BEING DAMAGED, UNLESS OTHERWISE NOTED. ALL UTILITIES AND IMPROVEMENTS THAT BECOME DAMAGED DURING CONSTRUCTION SHALL BE COMPLETELY RESTORED AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE ENGINEER.
- 9. TREE NUMBERS ARE IN REFERENCE TO ARBORIST REPORT, "PRELIMINARY ARBORIST FINDINGS FOR A CONCEPTUAL DEVELOPMENT PLAN", BY CALIFORNIA TREE AND LANDSCAPING CONSULTING, INC, DATED SEPTEMBER 2,2021.

DEMOLITION ITEMS

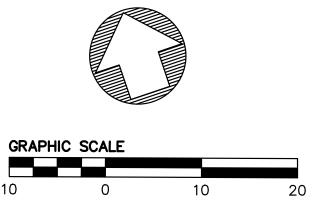
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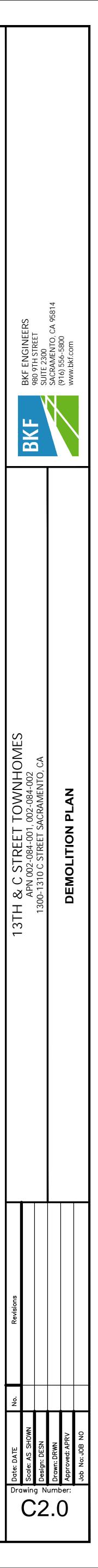
PROTECTION ITEMS

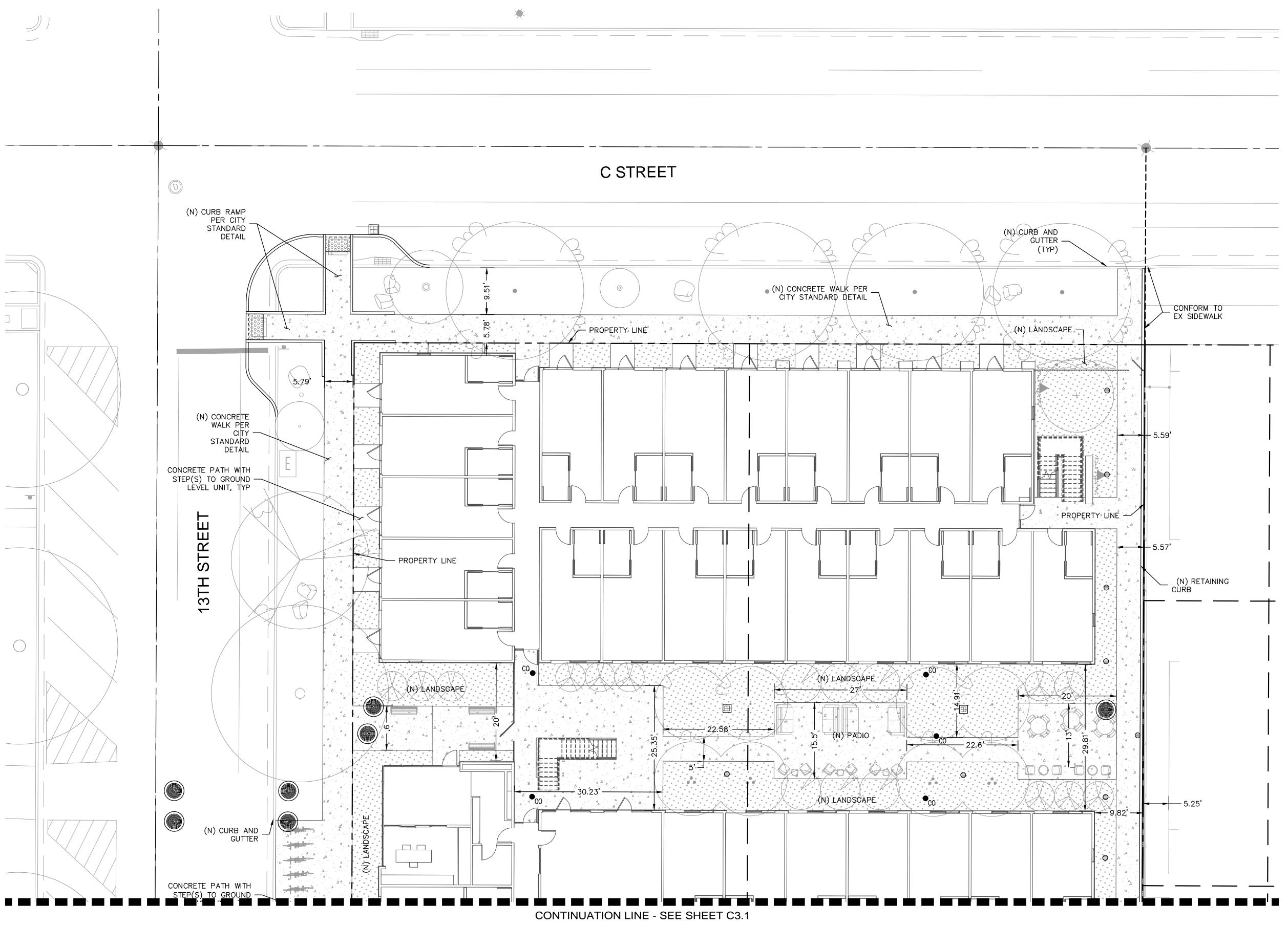
(1)	PROTECT EX	POLE
2	PROTECT EX	SIGN
3	PROTECT EX	WATER STRUCTURE
4	PROTECT EX	ELEC STRUCTURE

5 PROTECT EX CURB AND GUTTER

- TREE PRESERVATION MEASURES FOR CITY AND PRIVATE PROTECTED TREES
- 1. THIS PROJECT SHALL CONTRACT WITH A PROJECT ARBORIST EXPERIENCED WITH TREE PROTECTION AND CONSTRUCTION THAT IS REQUIRED TO:
 - a. ATTEND PRECONSTRUCTION MEETINGS TO APPROVE OF AND INFORM CONTRACTORS OF ALL TREE PROTECTION MEASURES.
 - b. VISIT THE SITE BEFORE AND AFTER DEMOLITION, GRADING, AND LANDSCAPING, AS WELL AS AT LEAST TWICE EACH MONTH DURING CONSTRUCTION TO ENSURE THAT TREE PROTECTION MEASURES ARE IMPLEMENTED AND MAINTAINED.
 - c. BE RESPONSIBLE FOR CORRECTING ANY SITE CONDITIONS THAT MAY NEGATIVELY IMPACT TREES AND REVISIT THE SITE TO ENSURE CORRECTIVE ACTION WAS PROPERLY IMPLEMENTED.
 - d. THE PROJECT ARBORIST SHALL REPORT IN WRITING TO URBAN FORESTRY ALL VIOLATIONS AND TREE PROTECTION FAILURES ALONG WITH CORRECTIVE ACTION TAKEN AND EXPECTED OUTCOMES.
- 2. ALL CONCRETE SIDEWALKS AND DRIVEWAYS SHALL BE RETAINED THROUGHOUT CONSTRUCTION TO PROTECT THE ROOTS AND SOIL FROM THE IMPACTS OF CONSTRUCTION ACTIVITIES.
 - a. EXISTING DRIVEWAYS SHALL BE USED AS THE SOLE ACCESS TO THE SITE. WHERE THERE ARE NO EXISTING DRIVEWAYS, ACCESS SHALL BE LIMITED TO A ONE OR TWO LOCATIONS OUTSIDE THE DRIPLINE OF PROTECTED TREES THAT HAVE PROTECTION FROM SOIL COMPACTION WITH THE USE OF ONE OR MORE OF THE FOLLOWING: A 6-INCH LAYER OF HARDWOOD CHIPS COVERED BY 3/-INCH PLYWOOD OR TRENCH PLATES, GEOTEXTILE FABRIC COVERED BY A 6-INCH LAYER OF HARDWOOD CHIPS OR AN ALTERNATIVE THAT IS APPROVED BY THE CITY ARBORIST.
- 3. RIGHT-OF-WAY PLANTERS AND CITY TREES SHALL BE SEPARATED FROM THE CONSTRUCTION SITE WITH A SIX-FOOT HIGH CHAIN LINK FENCE THAT SHALL REMAIN THROUGHOUT THE DURATION OF THE PROJECT TO PROTECT TREES AND TO PREVENT CONSTRUCTION TRAFFIC FROM COMPACTING THE SOIL IN THE PLANTERS.
- 4. CONSTRUCTION TRAILERS AND PORT-A-POTTIES SHALL BE PLACED ON EXISTING HARDSCAPE OR BRIDGED OVER THE TREE PROTECTION ZONE OR PLANTER SO AS NOT TO COMPACT SOIL. 5. ANY REGULATED WORK WITHIN THE DRIPLINE OR TREE PROTECTION ZONE OF A PROTECTED
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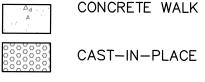


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SURFACE IMPROVEMENTS LEGEND



CAST-IN-PLACE DETECTABLE WARNING SURFACE

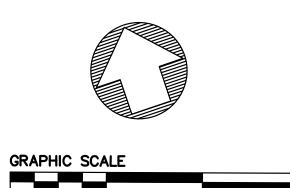
LANDSCAPED AREA

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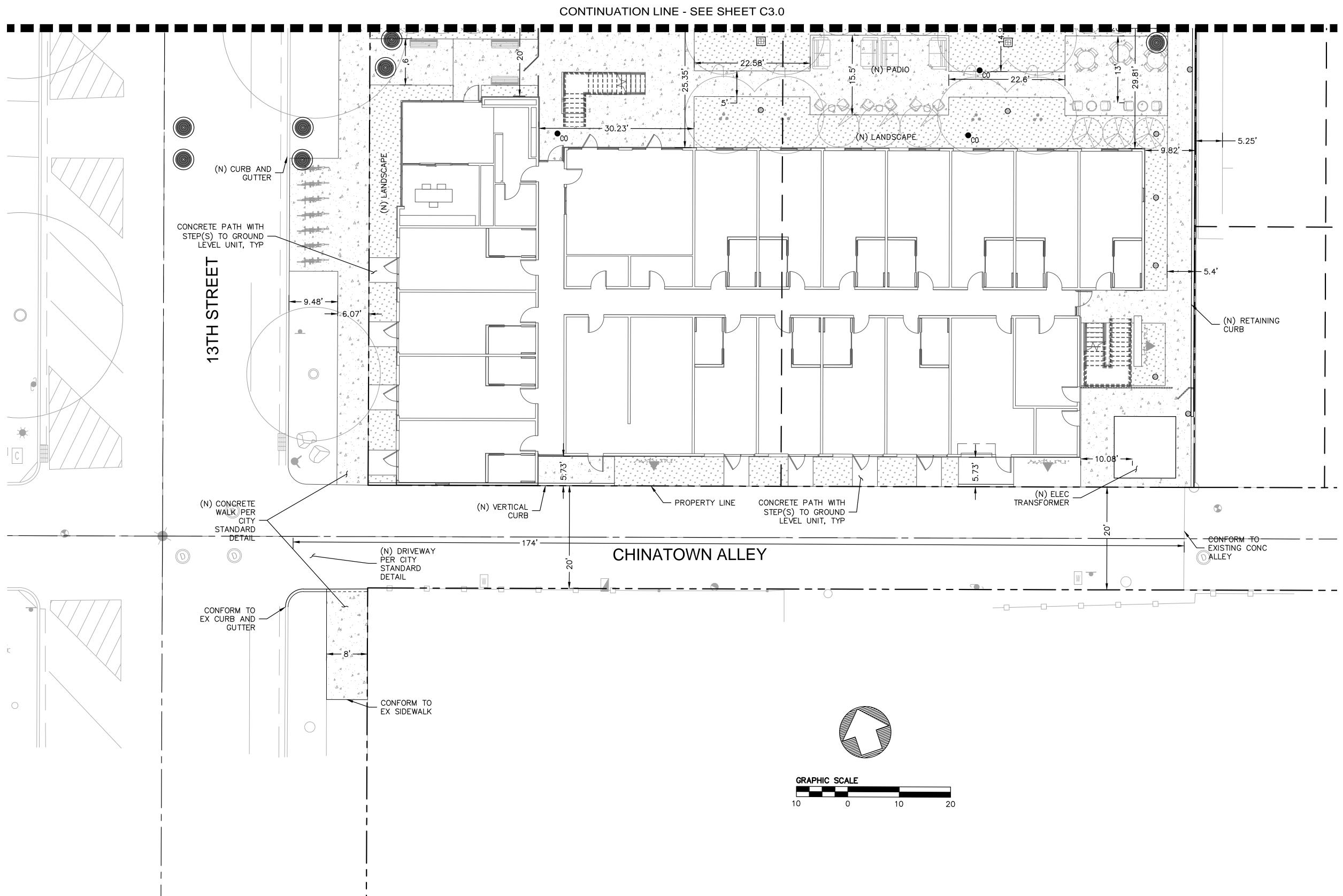
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	BKF ENGINEERS		SACRAMENTO, CA 95814	(9.10) 330-3600 www.bkf.com	
13TH & C STREET TOWNHOMES	APN 002-084-001, 002-084-002	1300-1310 C STREET SACRAMENTO, CA		SURFACE IMPROVEMENTS	
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SURFACE IMPROVEMENTS LEGEND

▲	CONCRETE WALK
	CAST-IN-PLACE DETECTABLE WARNING SURFACE
4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4	LANDSCAPED AREA

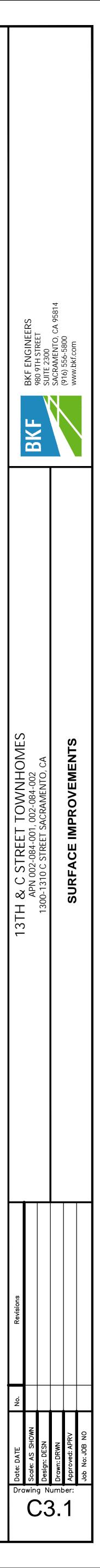
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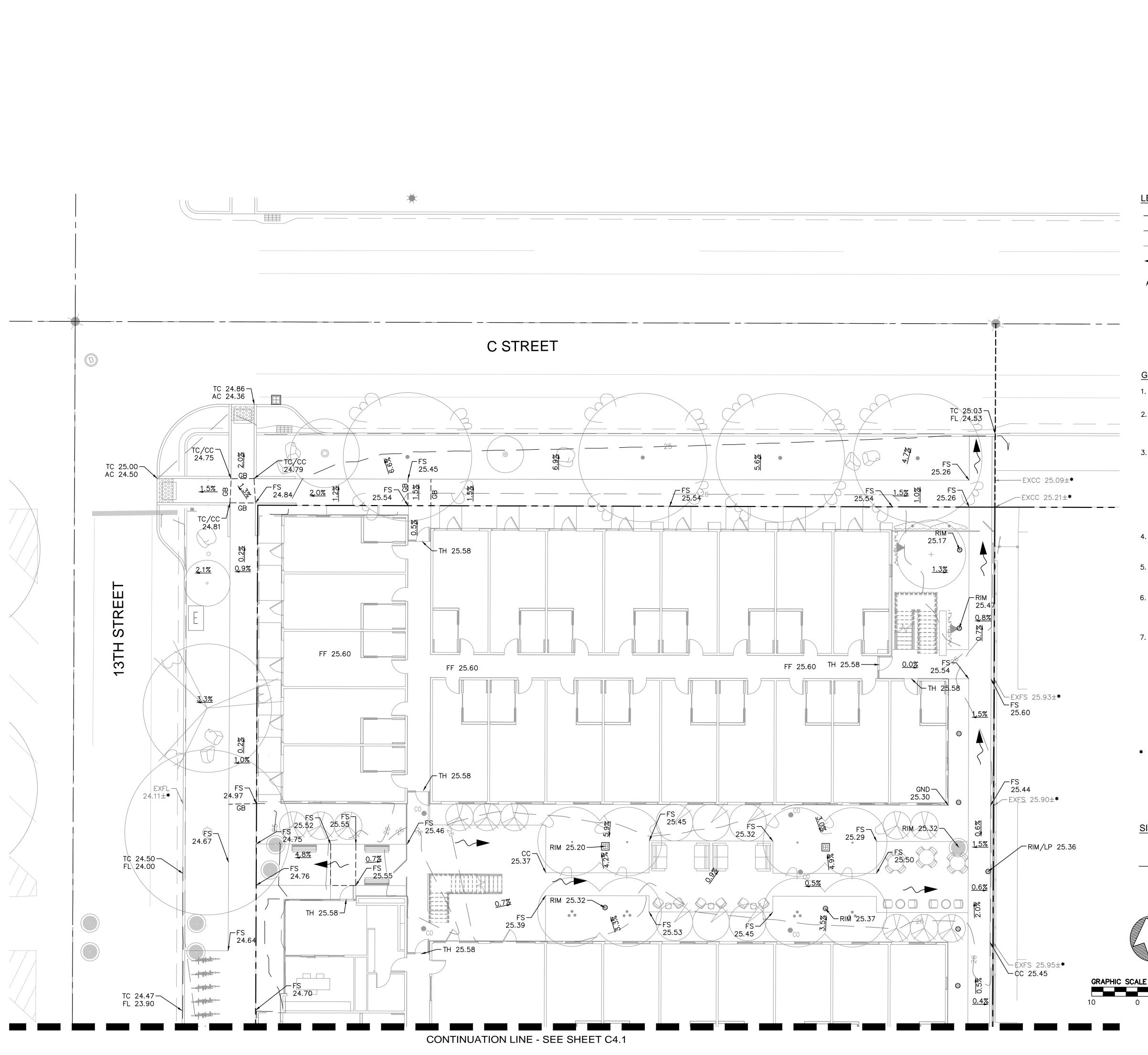
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CITY TREES
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<u>LEGEND</u>

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LOOR URFACE TION TC TOP OF CURB TFC TOP OF FLUSH CURB TH BUILDING THRESHOLD

GRADING NOTES

- 1. PROPOSED SITE PLAN AND UTILITIES HAS BEEN SCREENED FOR CLARITY.
- 2. GRADING WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS AND THE REQUIREMENTS AND RECOMMENDATIONS CONTAINED IN THE PROJECT GEOTECHNICAL REPORT.
- 3. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITHIN TOLERANCE OF ONE-TENTH OF A FOOT IN LANDSCAPED AREAS AND TWO-HUNDREDTHS OF A FOOT IN HARDSCAPED OR PAVED AREAS. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTOR SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE OWNER.
- 4. CONTRACTOR SHALL FIELD VERIFY FINISHED FLOOR ELEVATIONS OF EXISTING BUILDINGS PRIOR TO STARTING CONSTRUCTION.
- 5. CONTRACTOR SHALL TAKE CAUTION WHEN GRADING ADJACENT TO EXISTING BUILDINGS, TO PROTECT EXISTING FOUNDATIONS AND TO NOT TO COVER EXISTING VENTS.
- 6. RIMS OF UTILITY STRUCTURES SHALL BE ADJUSTED TO FINISHED GRADE IN AREAS OF RE-GRADING. IN PAVED AREAS, ELEVATION DIFFERENCE SHALL NOT BE MORE THAN $\frac{1}{4}$ INCH BETWEEN RIMS AND ADJACENT SURFACE.
- 7. WHERE IMPROVEMENTS INVOLVE ADA ACCESSIBILITY, CONTRACTOR'S ATTENTION IS DIRECTED TO THE FOLLOWING PARAMETERS THAT NEED TO BE MET WITH THE FINISHED CONSTRUCTION: a. WALKWAYS SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE
- OF 4.9% AND A MAXIMUM CROSS SLOPE OF 2%. b. LANDINGS SHALL HAVE A MAXIMUM SLOPE OF 2% IN ALL
- DIRECTIONS, INCLUDING DIAGONAL. c. ADA PARKING STALLS AND STRIPED AISLE SHALL HAVE A
- MAXIMUM SLOPE OF 2% IN ALL DIRECTIONS, INCLUDING DIAGONAL. d. TRANSITIONS BETWEEN PROPOSED IMPROVEMENTS AND EXISTING CONDITIONS SHALL BE SMOOTH AND FREE OF
- ABRUPT CHANGES. * CONTRACTOR SHALL FIELD VERIFY EXISTING GRADES AT

CONFORMS PRIOR TO FORMING/AC WORK.

SITE IMPERVIOUS AREAS:

PRE-IMPERVIOUS: 30,886 SF POST IMPERVIOUS: 31,962 SF

TOTAL ADDED: 1,076 SF

10

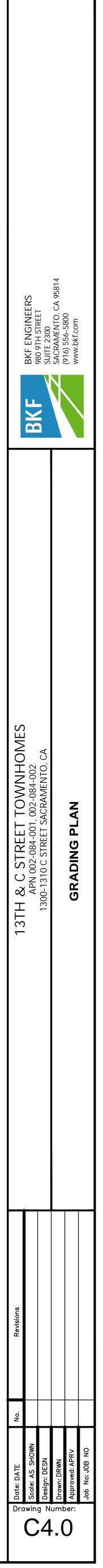
a. ATTEND PRECONSTRUCTION MEETINGS TO APPROVE OF AND INFORM CONTRACTORS OF ALL TREE PROTECTION MEASURES. b. VISIT THE SITE BEFORE AND AFTER DEMOLITION, GRADING, AND LANDSCAPING, AS

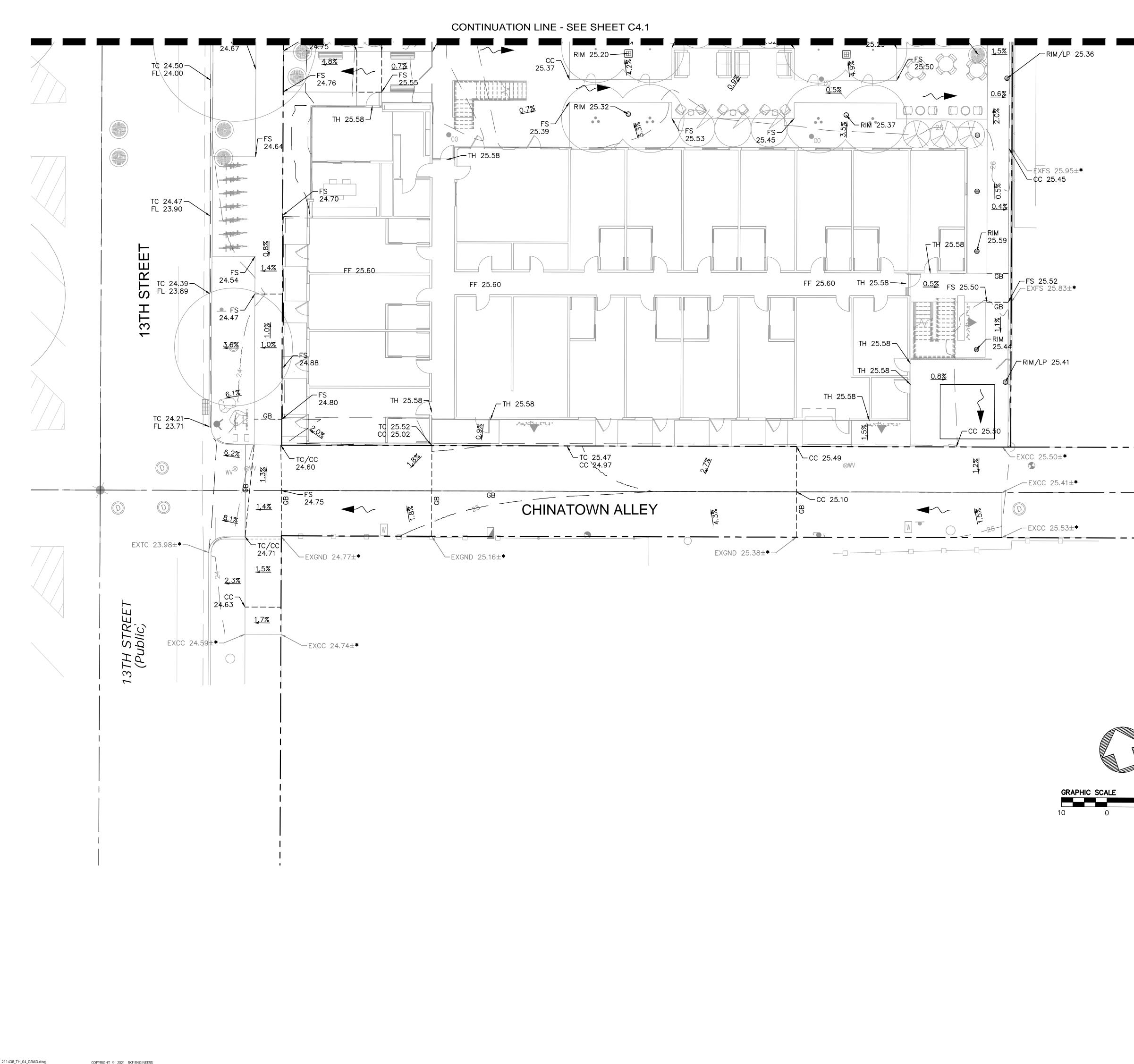
PROTECTION AND CONSTRUCTION THAT IS REQUIRED TO:

- WELL AS AT LEAST TWICE EACH MONTH DURING CONSTRUCTION TO ENSURE THAT TREE PROTECTION MEASURES ARE IMPLEMENTED AND MAINTAINED. c. BE RESPONSIBLE FOR CORRECTING ANY SITE CONDITIONS THAT MAY NEGATIVELY
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- 3. RIGHT-OF-WAY PLANTERS AND CITY TREES SHALL BE SEPARATED FROM THE CONSTRUCTION SITE WITH A SIX-FOOT HIGH CHAIN LINK FENCE THAT SHALL REMAIN THROUGHOUT THE DURATION OF THE PROJECT TO PROTECT TREES AND TO PREVENT CONSTRUCTION TRAFFIC FROM COMPACTING THE SOIL IN THE PLANTERS.
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- 6. ALL EXCAVATION, GRADING, OR TRENCHING WITHIN THE DRIPLINE OF A PROTECTED TREE FOR THE PURPOSE OF UTILITY INSTALLATION, CONSTRUCTING FOUNDATIONS, FOOTINGS, SIDEWALKS, CURBS, GUTTERS, OR ANY OTHER REASON SHALL EMPLOY ONE OF THE FOLLOWING METHODS: HYDRO-EXCAVATION, PNEUMATIC EXCAVATION OR HAND DIGGING AND SHALL BE DIRECTLY SUPERVISED BY A CERTIFIED ARBORIST.
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- 10. THE FOLLOWING IS A LIST OF ACTIVITIES THAT REQUIRE A TREE PERMIT IF THEY ARE TO OCCUR OR BE USED WITHIN THE RIGHT-OF-WAY PLANTER AND/OR WITHIN THE TREE PROTECTION ZONE OF PROTECTED TREES: ANY REGULATED WORK AS DEFINED IN SCC 12.56, EXCAVATION, GRADE CHANGES, TRENCHES, ROOT OR CANOPY PRUNING, OR BORING.
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THIS PROJECT SHALL CONTRACT WITH A PROJECT ARBORIST EXPERIENCED WITH TREE

TREE PRESERVATION MEASURES FOR CITY AND PRIVATE PROTECTED TREES





LEGEND

	GRADE BREAK
	MAJOR CONTOUR
	MINOR CONTOUR
	OVERLAND RELEA
, → BW XX.XX	ELEVATION LABEL CC CONCRETE EX EXISTING

ASE ELEVATION FF FINISHED FLOOR

FL FLOW LINE FS FINISHED SURFACE GND GROUND RIM RIM ELEVATION

TC TOP OF CURB TFC TOP OF FLUSH CURB TH BUILDING THRESHOLD

GRADING NOTES

- 1. PROPOSED SITE PLAN AND UTILITIES HAS BEEN SCREENED FOR CLARITY.
- 2. GRADING WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS AND THE REQUIREMENTS AND RECOMMENDATIONS CONTAINED IN THE PROJECT GEOTECHNICAL REPORT.
- 3. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITHIN TOLERANCE OF ONE-TENTH OF A FOOT IN LANDSCAPED AREAS AND TWO-HUNDREDTHS OF A FOOT IN HARDSCAPED OR PAVED AREAS. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES. THE CONTRACTOR SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE OWNER.
- 4. CONTRACTOR SHALL FIELD VERIFY FINISHED FLOOR ELEVATIONS OF EXISTING BUILDINGS PRIOR TO STARTING CONSTRUCTION.
- 5. CONTRACTOR SHALL TAKE CAUTION WHEN GRADING ADJACENT TO EXISTING BUILDINGS, TO PROTECT EXISTING FOUNDATIONS AND TO NOT TO COVER EXISTING VENTS.
- 6. RIMS OF UTILITY STRUCTURES SHALL BE ADJUSTED TO FINISHED GRADE IN AREAS OF RE-GRADING. IN PAVED AREAS. ELEVATION DIFFERENCE SHALL NOT BE MORE THAN $\frac{1}{4}$ INCH BETWEEN RIMS AND ADJACENT SURFACE.
- 7. WHERE IMPROVEMENTS INVOLVE ADA ACCESSIBILITY, CONTRACTOR'S ATTENTION IS DIRECTED TO THE FOLLOWING PARAMETERS THAT NEED TO BE MET WITH THE FINISHED CONSTRUCTION: a. WALKWAYS SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 4.9% AND A MAXIMUM CROSS SLOPE OF 2%.
- b. LANDINGS SHALL HAVE A MAXIMUM SLOPE OF 2% IN ALL DIRECTIONS, INCLUDING DIAGONAL c. ADA PARKING STALLS AND STRIPED AISLE SHALL HAVE A
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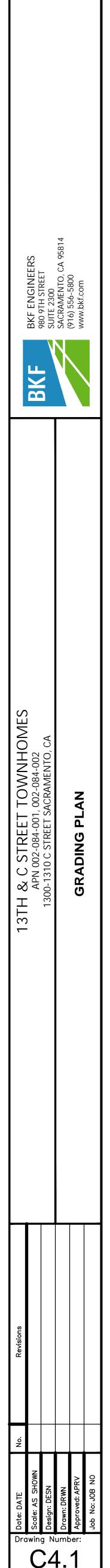
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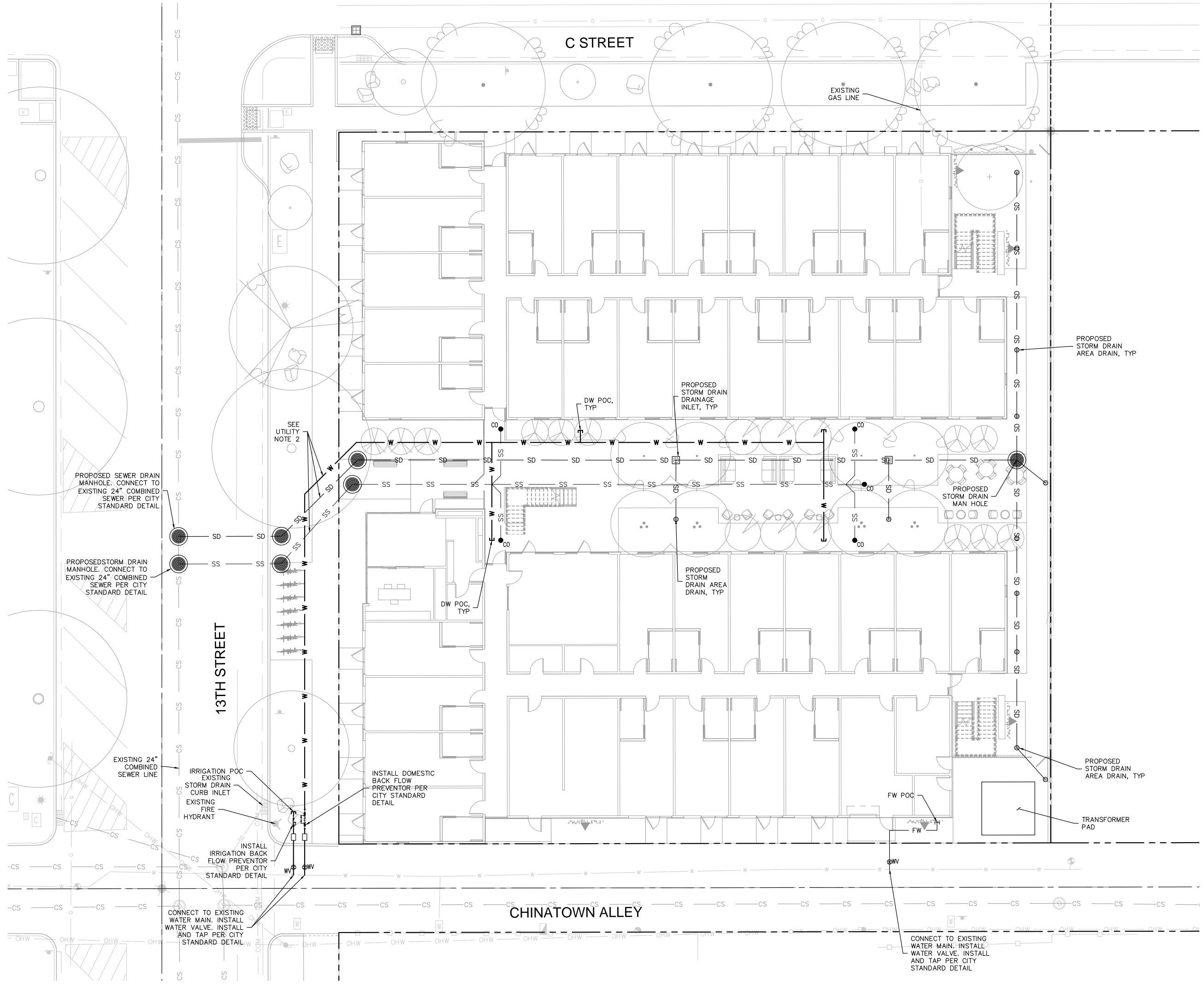
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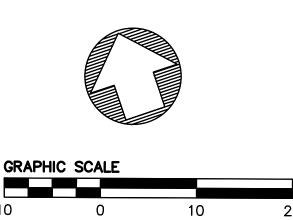
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LEGEND:

SD	PROPOSED STORM DRAIN LINE
	PROPOSED STORM DRAIN MANHOLE
CO ●	PROPOSED STORM DRAIN CLEANOUT
SS	PROPOSED SANITARY SEWER LINE
	PROPOSED SANITARY SEWER MANHOLE
● SSC0	PROPOSED SANITARY SEWER CLEANOUT
— w ——	PROPOSED WATER LINE
WV ⊗	PROPOSED WATER VALVE
	PROPOSED BACK FLOW PREVENTER

UTILITY NOTES

- 1. STORMWATER LID IS NOT REQUIRED. PROJECT QUALIFIES AS MULTI-FAMILY RESIDENTIAL WITH AN IMPERVIOUS AREA LESS THAN 1 ACRE PER TABLE 3-2 IN THE SACRAMENTO REGION STORMWATER QUALITY DESIGN MANUAL. SOURCE CONTROLS WILL BE IMPLEMENTED
- WATER, STORM, AND SEWER LINES TO BE ROUTED AS FAR FROM EXISTING STREET TREE AS POSSIBLE WHILE STILL MAINTAINING ALL MIN. REQUIRED CLEARANCES. TRENCHING TO BE CONDUCTED WITH HYDROVAC WITHIN THE ROOT ZONE TO MINIMIZE DISTURBANCE.

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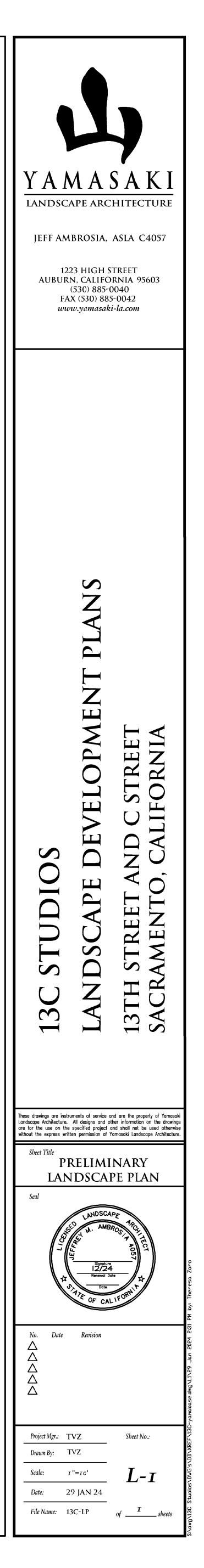
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	The plant palette of shrubs, grasses, groundcover, and limited perennial plantings are all low-water use species, climate-appropriate, long-lived, and requiring no more than monthly maintenance activities. Plants that offer aroma-therapeutic and visually calming benefits will be prioritized. No annual plants or mowed lawn will be used.
2	Plants will be grouped in symmetrical masses of single species to create soft, unfussy, calm planted spaces that offer movement and textural interest. Visual sightlines throughout the courtyard will be maintained by using only plants that are maximum 3' height, and high-canopied trees. Shade trees are deciduous species that together offer summer shade and seasonal interest.
	All planted areas shall be properly amended, compacted, finish graded, and treated with a pre-emergent herbicide prior to planting. 4 inches of 'Walk-On' fir bark mulch shall be applied in all planted areas. Shredded 'Gorilla Hair' mulch shall not be used. All planted areas shall be amended with organic material per horticultural soils report recommendations. A soils test shall be completed by the landscape contractor and shall include soil texture, cat-ion exchange capacity, and soil fertility.
	Shrubs and ground covers will be no less than I-gallon size. Trees will be no less than 24" box size. Plant materials shall be spaced to provide substantial cover, but also to allow adequate room to mature into their natural form and ultimate size without required pruning. POLICE CPTED NOTES:
	FOLICE CFTED NOTES: All mature landscaping outside of the securely fenced courtyard shall follow the two-foot, six-foot rule: All landscaping shall be ground cover, two feet or less and lower tree canopies of mature trees shall be above six feet. This increases natural surveillance, eliminates hiding areas within the landscape, and provides for tenants and users a safer environment.
	Tree canopies shall not interfere with or block lighting. This creates shadows and areas of concealment. The landscaping plan shall allow for proper illumination and visibility regarding lighting and surveillance cameras through the maturity of trees and shrubs.
	IRRIGATION NARRATIVE
	A new single point of connection with dedicated water meter and backflow preventer for the irrigation system shall be tapped into the public water mainline adjacent to the domestic point of connection. All landscaped areas including offsite landscape strips will be serviced with a fully automatic irrigation system to include below-ground PVC mainline and lateral piping, remote control valves and wiring, flow sensor, quick coupling valves, and master shut-off valve.
	Irrigation to all new shrubs, grasses, and groundcover plantings shall be via subsurface in-line drip irrigation. Irrigation to all new trees shall be surface bubblers. Stations / hydrozones shall be delineated based on similar water demands, sun exposure, and microclimates.
	Irrigation shall be controlled via a commercial-quality 'smart' controller (minimum 12 stations) that is capable of multiple programs and linked to a wireless weather sensor installed in an unobstructed location.
	All trenching for irrigation shall be performed using in a manner that does not sever roots of City trees, such as hand-digging or hydro-excavation. Watering to existing City trees shall continue throughout construction.
	"I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED
	THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN"
	REQUIRED TREE PRESERVATION MEASURES FOR CITY TREES
	 I. This project shall contract with a project arborist experienced with tree protection and construction that is required to: a. Attend pre-construction meetings to approve of and inform contractors of all tree protection measures. b. Visit the site before and after demolition, grading, and landscaping, as well as at least twice each month during construction to ensure that tree protection measures are implemented and maintained. c. Be responsible for correcting any site conditions that may negatively impact trees and revisit the site to ensure corrective action was properly implemented. d. The project arborist shall report in writing to Urban Forestry all violations and tree protection failures along with corrective action taken and expected outcomes.
	 All concrete sidewalks and driveways shall be retained throughout construction to protect the roots and soil from the impacts of construction activities. a. Existing driveways shall be used as the sole access to the site. Where there are no existing driveways, access shall be limited to a one or two locations outside the dripline of protected trees that have protection from soil compaction with the use of one or more of the following: A 6-inch layer of hardwood chips covered by ³/₄-inch plywood or trench plates, geotextile fabric covered by a 6-inch layer of hardwood chips or an alternative that is approved by the City Arborist.
	 Right-of-way planters and City trees shall be separated from the construction site with a six-foot high chain link fence that shall remain throughout the duration of the project to protect trees and to prevent construction traffic from compacting the soil in the planters. Construction trailers and port-a-potties shall be placed on existing hardscape or bridged over the tree protection zone
x	or planter so as not to compact soil. 5. Any Regulated Work within the dripline or Tree Protection Zone of a protected tree shall be separately permitted prior to the start of construction and supervised by a Certified Arborist. Submit a tree permit application and a tree
	protection plan created by a Certified Arborist to UrbanForestry@cityofsacramento.org and refer to the planning project number or off-site project number. 6. All excavation, grading, or trenching within the dripline of a protected tree for the purpose of utility installation, constructing foundations, footings, sidewalks, curbs, gutters, or any other reason shall employ one of the following
RMER	methods: Hydro-excavation, pneumatic excavation or hand digging and shall be directly supervised by a Certified Arborist. 7. There shall be no excavation deeper than the existing excavation for sidewalks within the dripline of protected trees.
	 8. There shall be no grade changes within the dripline of protected trees. All grade changes shall be accommodated onsite. 9. There shall be no soil compaction within the dripline of protected trees.
U	10. There shall be no non-native soil, non-organic matter or structural soil added to the right-of-way planter.
	II. The following is a list of activities that require a tree permit if they are to occur or be used within the right-of-way planter and/or within the tree protection zone of protected trees: any regulated work as defined in SCC 12.56, excavation, grade changes, trenches, root or canopy pruning, or boring.
	12. The following is a list of activities that are prohibited within the right-of-way planter and/or tree protection zone of protected trees: pedestrian and equipment traffic that could compact the soil or physically damage roots, parking vehicles, equipment and/or port-a-pottles, storing of soil, construction materials, petroleum products, water or building refuse, disposing of wash water, paint, cement, fuel or other potentially damaging liquids, and any other activities that may have negative impacts on the trees and soil.
NORTH	13. All trees shall be watered regularly according to the recommendation of the project arborist. 14. The applicant shall be financially responsible for any damage to City trees associated with the project. Accidental or negligent actions that damage City trees may result in a penalty. The monetary value of any such damages will be
40'	appraised by the City Urban Forester or his authorized representative and shall be expressed as the monetary equivalent of all labor and materials required to bring the tree in question to a state of comparable utility with regards to its condition and function prior to the beginning of the project.
TO REMAIN	PLANT PALETTE
IN ARE IDENTIFIED E; SEE ALSO ONDITION DETAILS:	<u>SHRUBS AND WOODY PERENNIALS</u> Cistus salviifolius / Sageleaf Rockrose Gardenia × 'Fool Proof' / Gardenia Lavandula angustifolia 'Hidcote' Loropetalum chinense 'Purple Daydream' / Dwarf Loropetalum
j 0	Mahonia 'Soft Caress' / Soft Caress Mahonia Mahonia repens / Creeping Mahonia Nandina 'Flirt' / Dwarf Heavenly Bamboo Salvia nemerosa 'Marcus' / Garden Sage Sarcococca hookeriana humilis / Sweet Box
Maple alley Oak alley Oak	<u>GROUNDCOVERS</u> Nepeta faasenii 'Snowflake' / Catmint Rosmarinus officinalis 'Huntington Carpet' / Carpet Rosemary Stachys byzantina / Lamb's Ear Thymus ×. citriodorus 'Aureus' / Lemon Thyme Trachelospermum asiaticum / Star Jasmine Zauschneria californica / California Fuchsia
6) Ge	<u>GRASSES & GRASS-LIKE PLANTS</u> Calamagrostis acutifolia 'Karl Foerster' / Feather Reed Grass Carex tumulicola / Berkeley Sedge Dianella caerulea 'Clarity Blue' / Blue Flax Lily Hesperaloe parvifolia 'Brake Lights' / Red Yucca Lomandra 'Platinum Beauty' / Mat Rush

PLANTING NARRATIVE





1 SITE PLAN 1" = 10'-0"



C STREET

SHEET NOTES - SD-101

SECURITY GATE NEW 10'-0" SECURITY FENCE, VERTICAL PICKETS (TYP.) GROUND FLOOR UNIT ENTRIES (TYP.) PLANTING AREA, SEE LANDSCAPE DRAWINGS (TYP.) DECORATIVE METAL SCREEN (TYP.) PROPOSED AREA OF PLANTING, SEE LANDSCAPE DRAWINGS (TYP.) NEW CONCRETE PAVEMENT 14 SHORT TERM BIKE PARKING

FAR: 2.0

SCALE: 1" = 10'-0"

13 | C STUDIOS SKK DEVELOPMENTS SACRAMENTO, CA

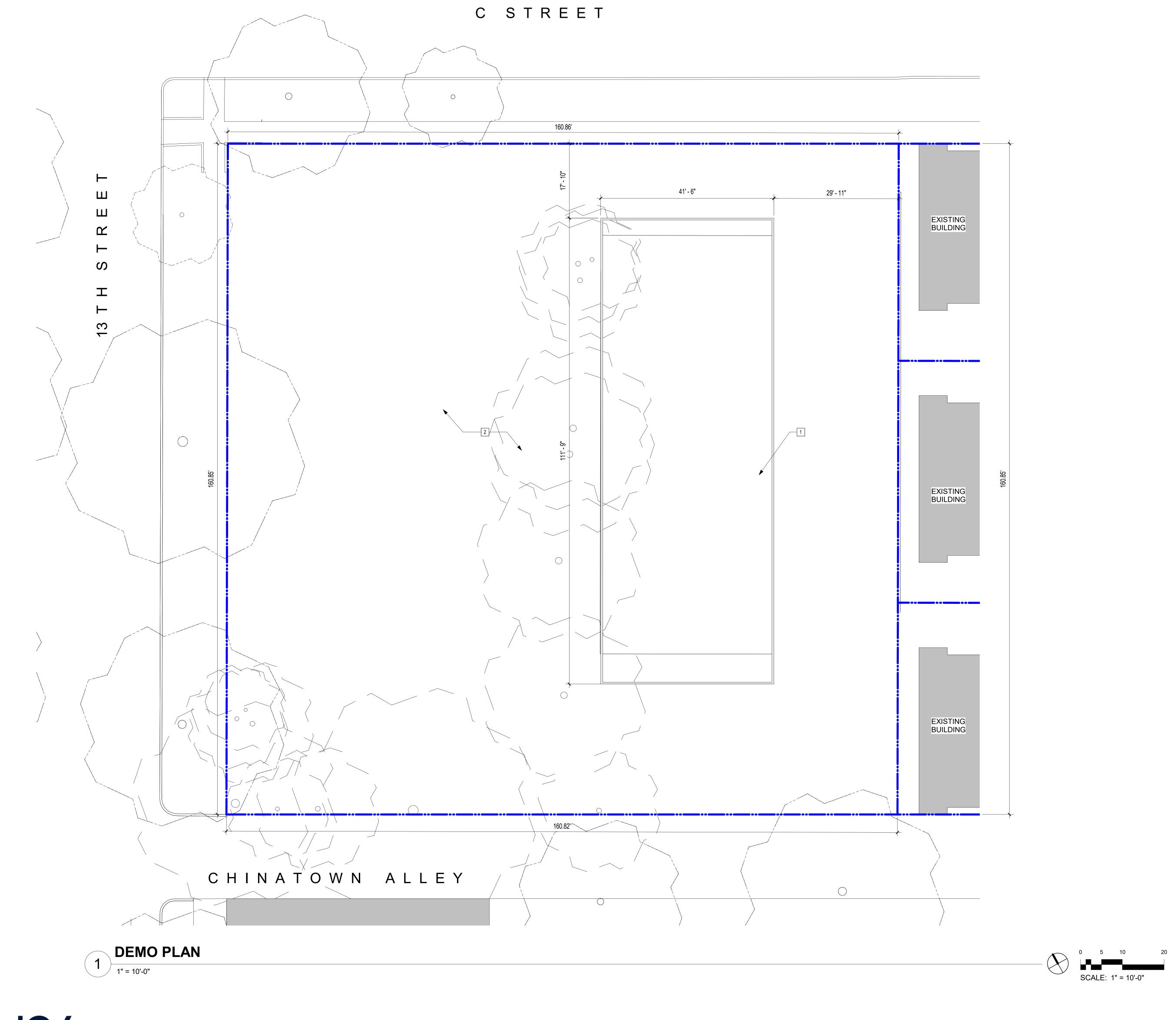


Democracy Alley-

shap da











SHEET NOTES - SD-102

EXISTING BUILDING TO BE DEMOLISHED EXISTING PAVEMENT TO BE REMOVED THROUGHOUT THE SITE

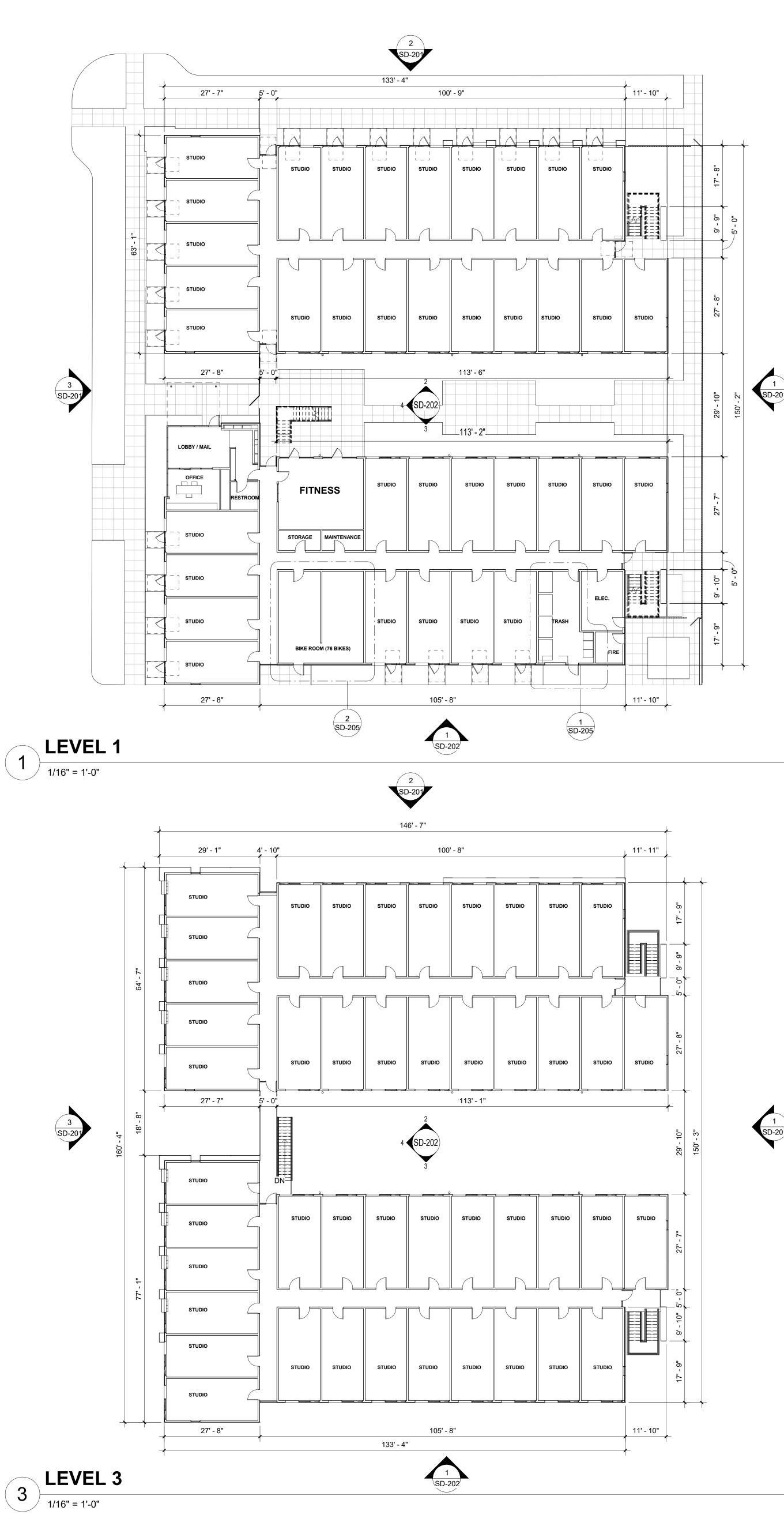
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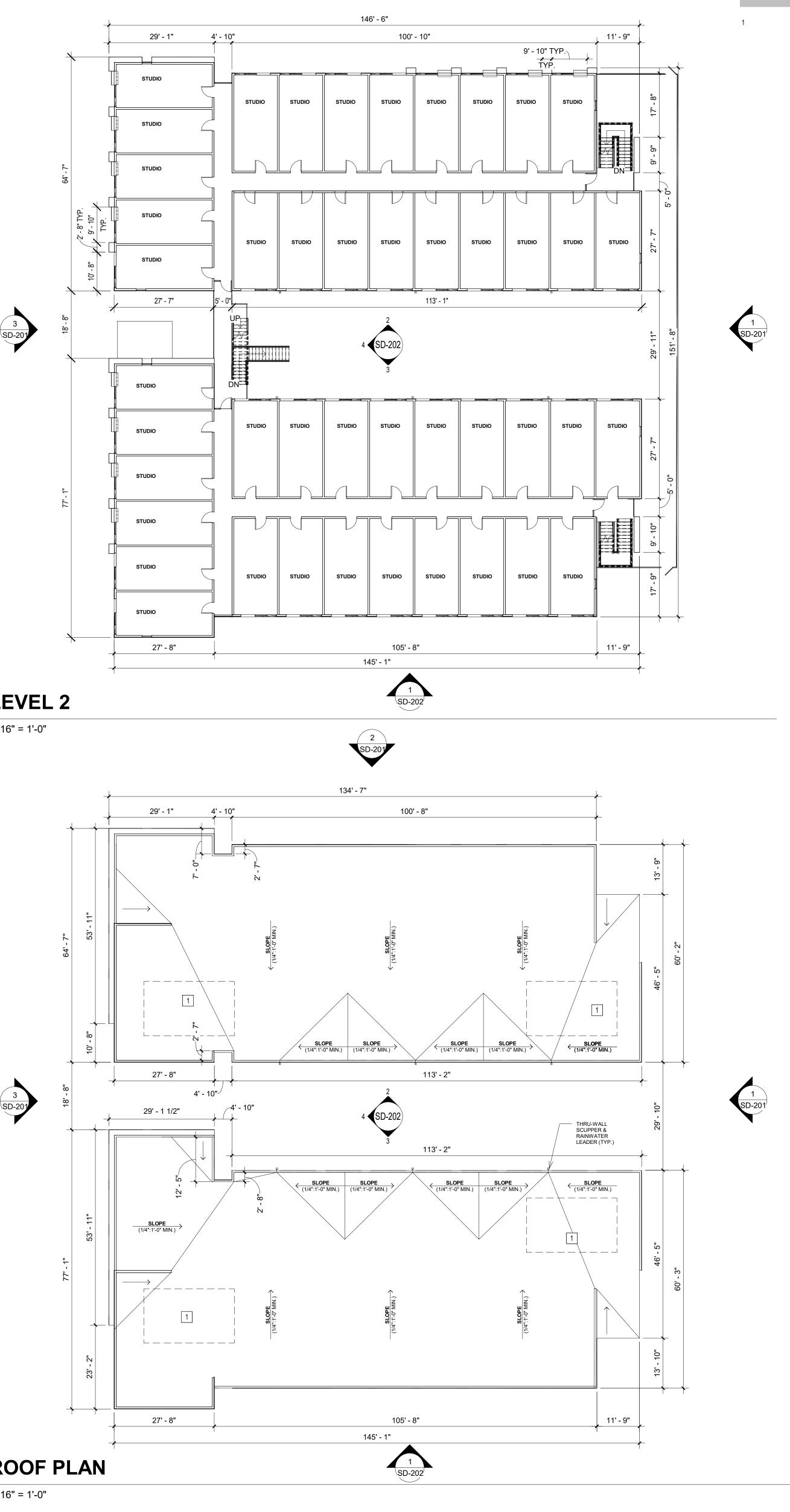




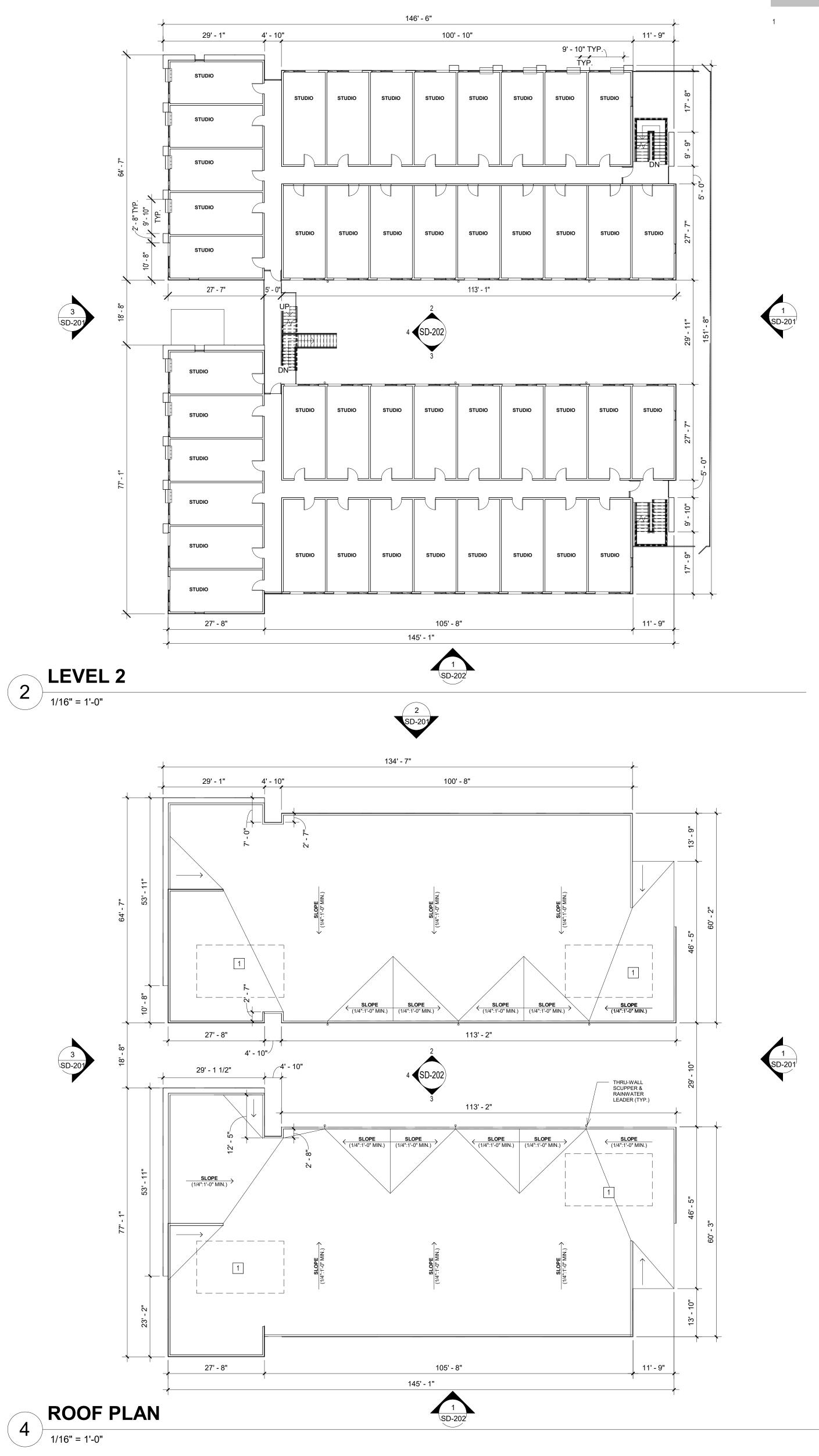








2 **LEVEL 2** 1/16" = 1'-0"





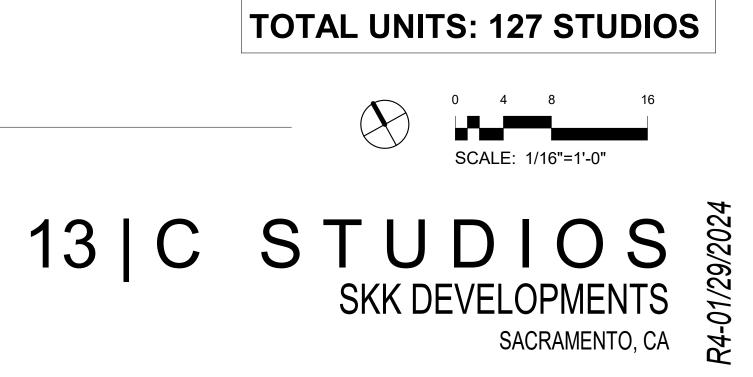


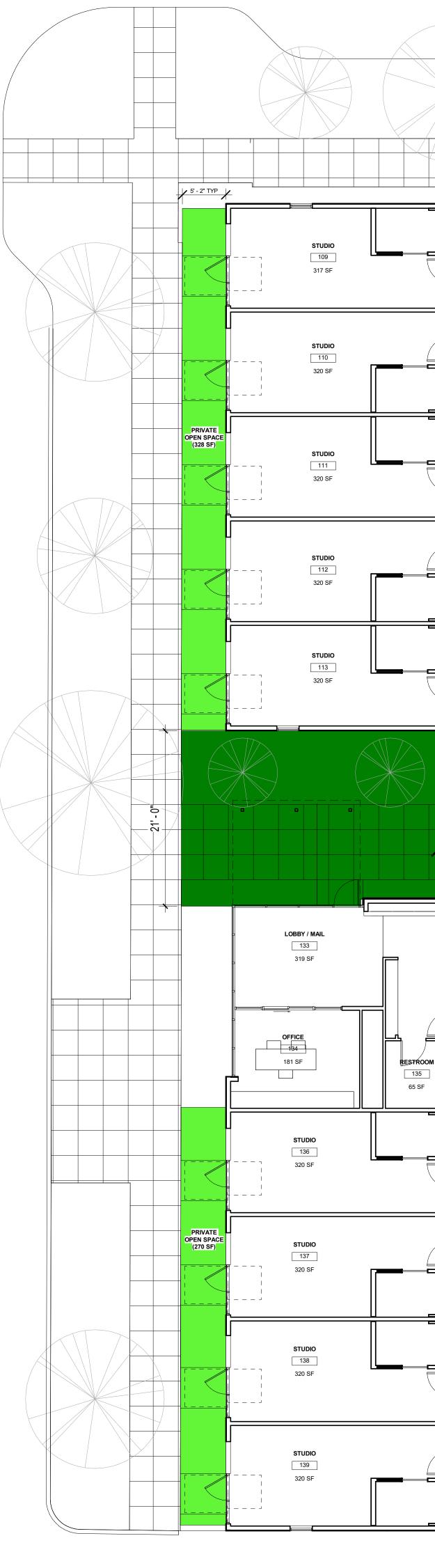


AREA OF MECHANICAL EQUIPMENT, SET BACK TO AVOID ALL STREET LEVEL SIGHT LINES



SD-1 FLOOR PLANS & ROOF PI

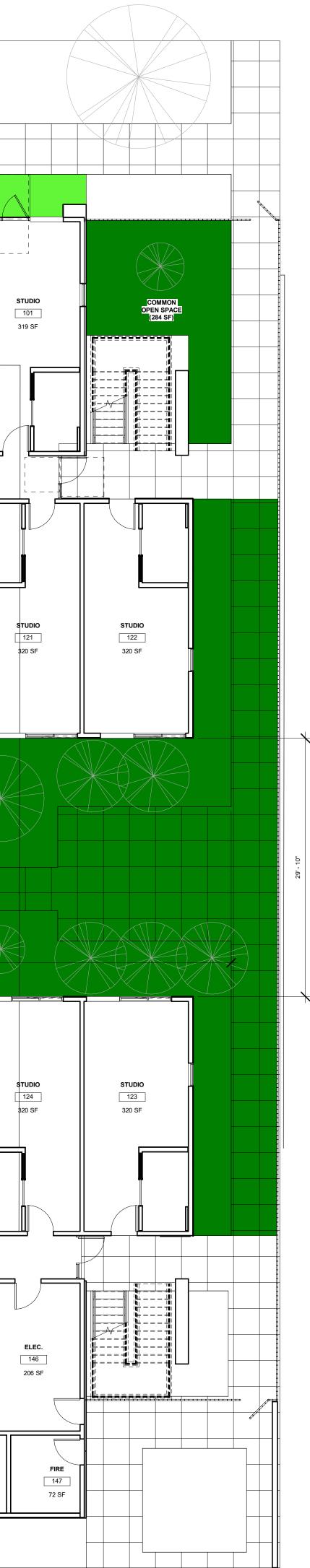


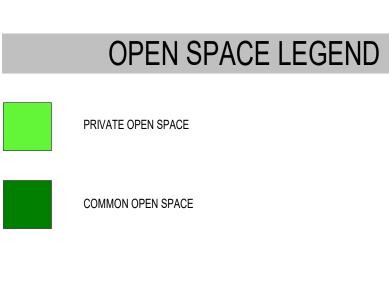


1 **SD_LEVEL 1_OPEN SPACE** 1/8" = 1'-0"



			PRIVATE OPE SPACE (475 SF)				
STUDIO				STUDIO			۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱ ۱
108 319 SF	107 319 SF	106 319 SF	105 319 SF	104 319 SF	103 319 SF	102 319 SF	3
STUDIO 114 320 SF	STUDIO 115 320 SF	STUDIO 116 320 SF	STUDIO 117 320 SF	STUDIO 118 320 SF	STUDIO 119 320 SF	STUDIO 120 320 SF	S 3
				COMN OPENS (5,022	ION PACE SF)		
FITNESS 130 500 SF		STUDIO 129 320 SF	STUDIO 128 320 SF	STUDIO 127 320 SF	STUDIO 149 320 SF	STUDIO 125 320 SF	S
STORAGE 131 71 SF	MAINTENANCE 132 71 SF						
	DM (76 BIKES)	STUDIO 141 320 SF	STUDIO 142 320 SF	STUDIO 143 320 SF	STUDIO 144 320 SF	TRASH 145 356 SF	
	342 SF			PRIVATE OPEN SPACE (284 SF)			





OPEN SPACE CALCULATIONS TOTAL OPEN SPACE REQ'D: 6,300 S.F.

PROVIDED: PRIVATE OPEN SPACE: 1,357 S.F. COMMON OPEN SPACE:5,306 S.FTOTAL:6,663 S.F. (MEETS MIN. OPEN SPACE REQ'S)

17.44.050 OPEN SPACE REQUIREMENTS FOR MULTI-UNIT DWELLINGS, WITHIN THE TRADITIONAL OPEN SPACE DISTRICT. TRADITIONAL OPEN SPACE DISTRICT: A COMBINATION OF PRIVATE AND COMMON WIDTH OR DEPTH OF 20 FEET.

13 | C STUDIOS SKK DEVELOPMENTS SACRAMENTO, CA





OPEN SPACE REQUIRED AS PER CITY OF SACRAMENTO MUNICIPAL CODE SECTION

OPEN SPACE SHALL BE PROVIDED FOR NEW MULTI-UNIT DWELLINGS AT A RATIO OF 50 S.F. OF OPEN SPACE PER DWELLING UNIT BEYOND THE MINIMUM REQUIRED FRONT-YARD, SIDE-YARD, AND REAR-YARD SETBACKS. PRIVATE OPEN SPACE MUST HAVE A MINIMUM DEPTH OF 3 FEET. COMMON OPEN SPACE SHALL HAVE A MINIMUM







williams+paddon

$- \frac{RODF}{26^{2} \cdot 0^{4}} \qquad $	1 CEMEN 2 CEMEN 3 CORRU 4 ALUMIN 5 VINYL W 6 OVERH 7 SECURI 9 RAINWA 10 OPEN M 11 ORNAM 12 ENTRY 0 13 LOUVER 15 HOLLOW 16 6' TALL Y 4 4 4 4 4 4 4 4 4 4 4 4 4
$-\frac{\text{LEVEL 3}}{12 \cdot 0^{\circ}} \xrightarrow{\bullet}$	9 RAINWA 10 OPEN M 11 ORNAM 12 ENTRY 13 LOUVER 15 HOLLOV
$\frac{127 - 0^{2}}{12^{2} - 0^{2}}$	
$\frac{\text{LEVEL 1}}{0.00}$	
$\frac{\text{LEVEL 1}}{9 \cdot 9^{2}} \bullet$	
ROOF 36' - 0"	
LEVEL 3 24' - 0"	
LEVEL 2 12' - 0"	
LEVEL 1 0' - 0"	
$ \begin{array}{c} 5 \\ -1 \\ -13 \\ -24' - 0'' \\ \end{array} $	
$ \frac{\text{LEVEL 2}}{12' - 0"}$	
LEVEL 1 0' - 0"	

EGEND

INE (10' ABOVE GRADE) MENT PLASTER TAL METAL MESH GUARD I W/ ENTRY GATE



NT WALL FINISH COLOR MATCH CEMENT PLASTER RMER

LEGEND

ER

TER HITE

FER COAL

METAL PANEL A COTTA AL SALES (W72)

OREFRONT CHARCOAL







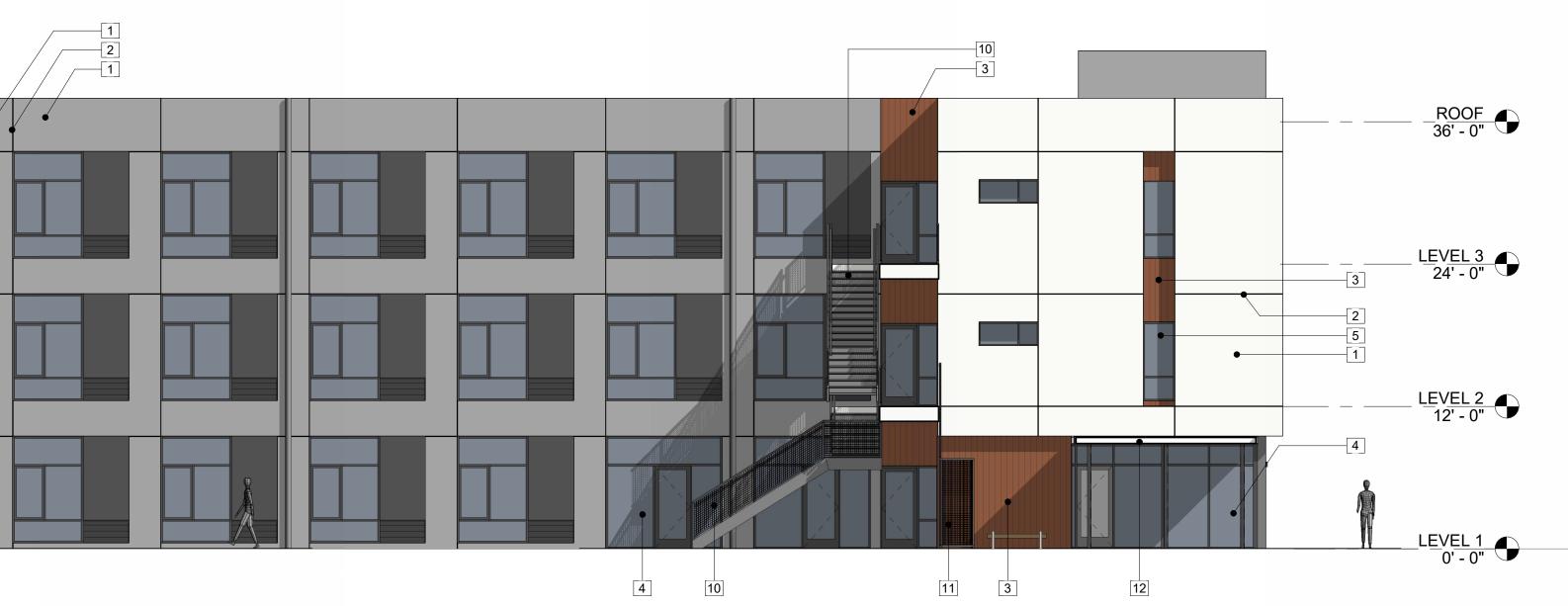
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3 COURTYARD NORTH ELEVATION 1/8" = 1'-0"











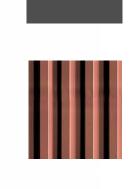
1 CEMENT PLASTER

5 VINYL WINDOW

2 CEMENT PLASTER REVEAL

6 OVERHEAD COILING DOOR

3 CORRUGATED METAL ACCENT PANEL 4 ALUMINUM STOREFRONT SYSTEM



CORRUGATED METAL PANEL COLOR: TERRA COTTA PROFILE: METAL SALES (W72)

ALUMINUM STOREFRONT COLOR: DARK CHARCOAL



KEYNOTE LEGEND

7 SECURITY FENCING AT PROPERTY LINE (10' ABOVE GRADE) 9 RAINWATER LEADER, TO MATCH CEMENT PLASTER 10 OPEN METAL STAIR WITH ORNAMENTAL METAL MESH GUARD



11 ORNAMENTAL METAL MESH SCREEN W/ ENTRY GATE

<u>ROOF</u> 36' - 0"

LEVEL 3 24' - 0"

LEVEL 2 12' - 0"

LEVEL 1 0' - 0"







NORTHWEST CORNER









NOTE: TREES HAVE BEEN OMITTED IN RENDERINGS ON THIS SHEET TO ALLOW VIEWS OF OVERALL BUILDING. FOR RENDERINGS WITH TREES, SEE PHOTOREALISTIC RENDERINGS ON COVER SHEET, NIGHT RENDERING AND DETAIL RENDERING OF MAIN ENTRY.



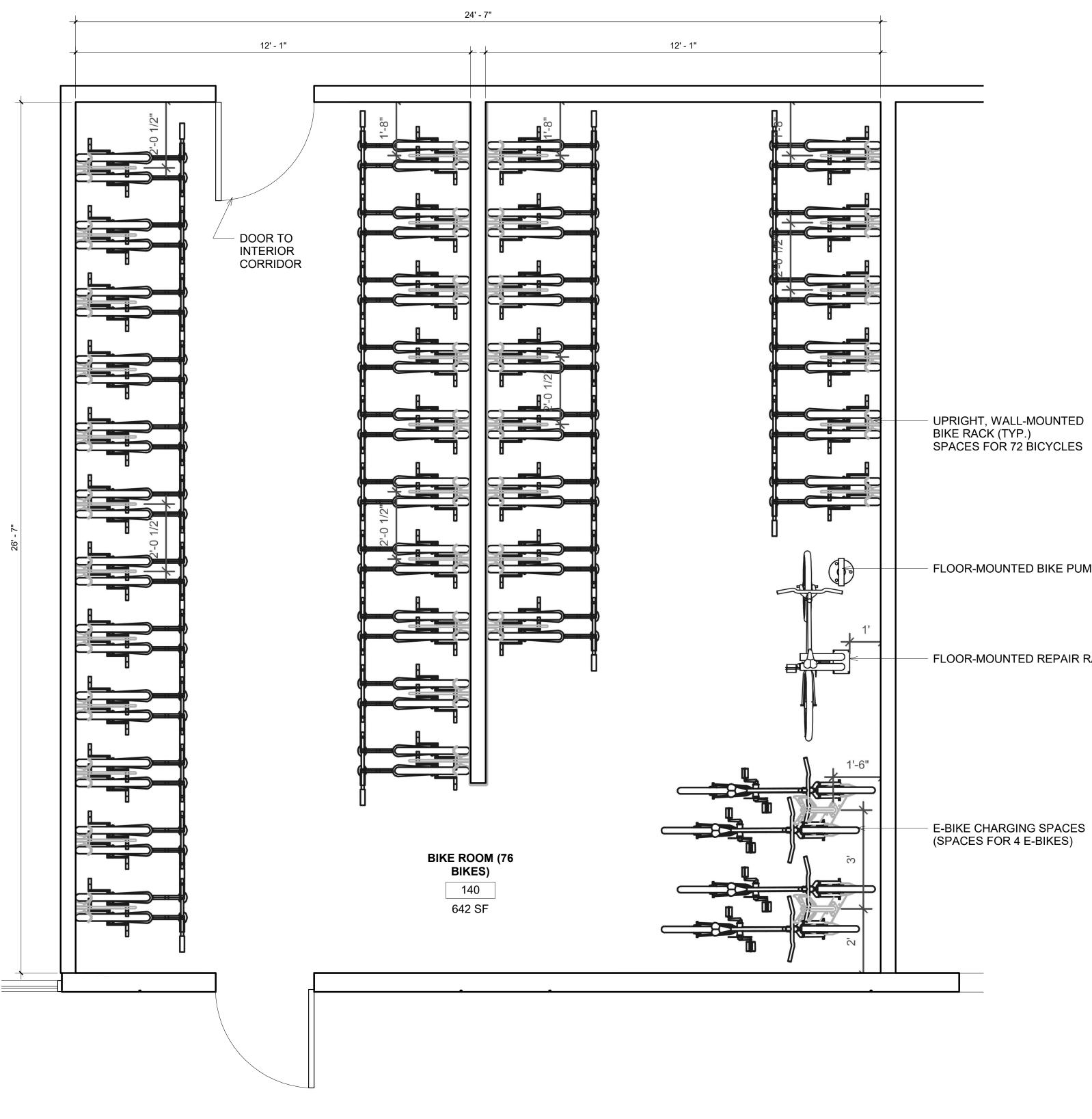












CHINATOWN ALLEY

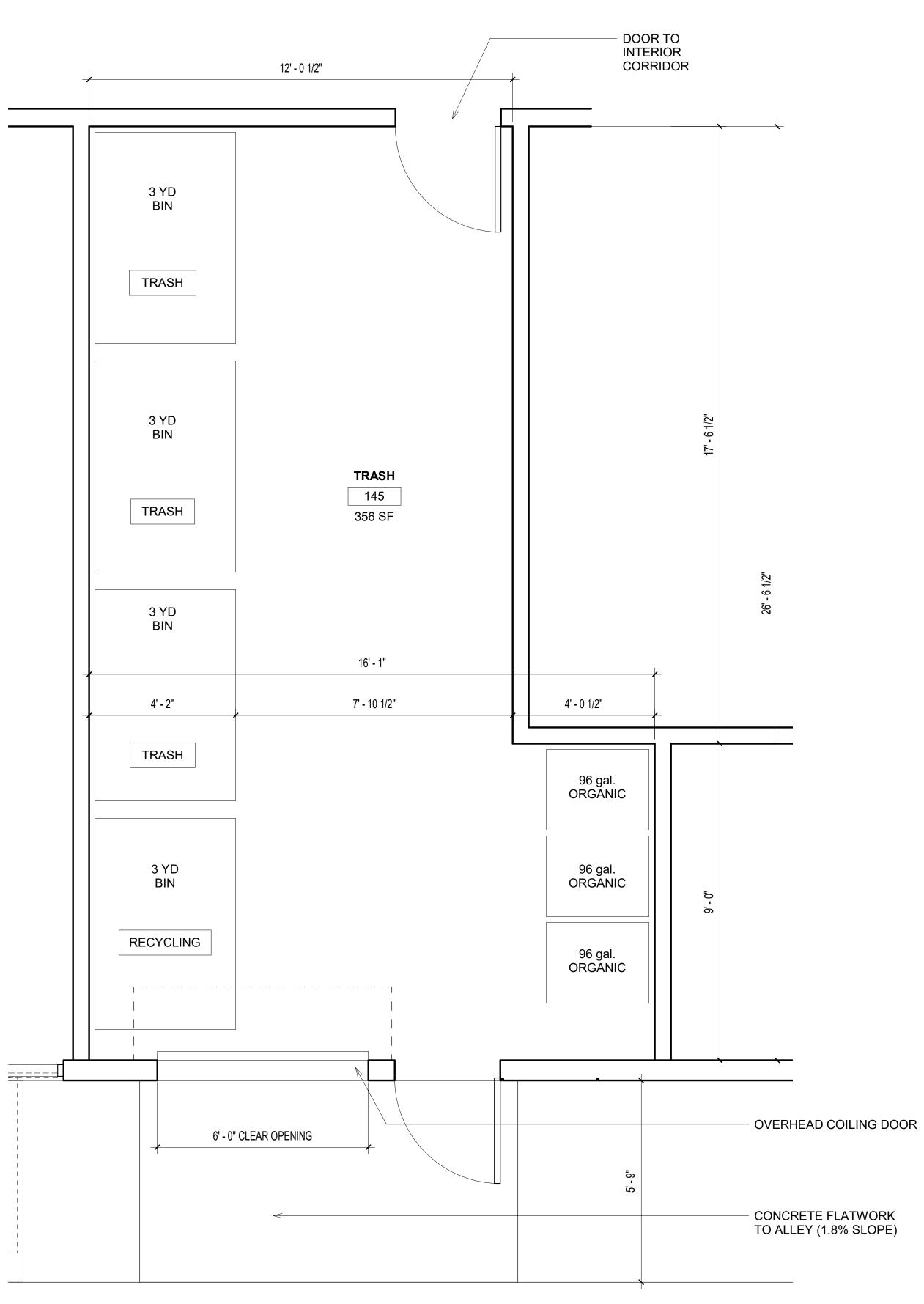




- FLOOR-MOUNTED BIKE PUMP

- FLOOR-MOUNTED REPAIR RACK

E-BIKE CHARGING SPACES (SPACES FOR 4 E-BIKES)



CHINATOWN ALLEY

TRASH ROOM PLAN

1 1/2" = 1'-0"

SOLID WASTE CALCULATIONS	
REQUIRED	PROVIDING
BASED ON 127 UNITS	BASED ON 3 PICKUPS/WEEK
RECYCLING = 8 YD / WEEK (AT 1 YD / 16 UNITS, ROUNDED TO NEAREST YARD)	RECYCLING = 9 YD / WEEK (ONE (1) 3 YARD BIN X 3)
TRASH = 21 YD / WEEK (AT 1 YD / 6 UNITS, ROUNDED TO NEAREST YARD)	TRASH = 27 YD / WEEK (THREE (3) 3 YARD BINS X 3)
ORGANICS = NO VOLUME REQUIREMENT, BUT MUST BE PROVIDED.	ORGANICS = 4.3 YD / WEEK (THREE (3) 96 GAL. BINS X 3)









January 30, 2024 (Original: September 2, 2021)

SKK Developments Attn: Alberto Esquivel 2409 L Street, #200 Sacramento, CA 95816 Via Email: <u>alberto@skkdevelopments.com</u> Via Email (for request for final arborist revisions): kvinge@19six.com

PRELIMINARY ARBORIST FINDINGS FOR A CONCEPTUAL DEVELOPMENT PLAN

RE: 1300 & 1310 C Street, APN #002-084-001 & 002; City of Sacramento jurisdiction

Executive Summary:

Alberto Esquivel of SKK Developments, on behalf of the property owner, contacted California Tree and Landscape Consulting, Inc. to inventory and evaluate the trees on the site or within 25' of development for purposes of evaluating the impacts to the trees from the proposed Conceptual Plan (See Next Page for Plan reference). The property falls under the jurisdiction of the City of Sacramento. See Supporting Information Appendix A –Tree Location Map.

Dave Mercado, ISA Certified Arborist #WE-7311A, was on site August 23, 2021. A total of 21 trees were evaluated on this property, of which 5 are street trees, and 3 offsite trees across Chinatown Alley. There are (2) private protected trees proposed for removal with a waiver of fees due to their condition. (2) of the City street trees, #79327 and 79326 could have root impacts. Tree 79327 may require non-standard pruning. A Tree Protection Plan should be required and civil plans should be evaluated by the project arborist. See Table 2 (below) for mitigation requirements.

Tree Species	Trees Inventoried	Trees located on the Parcel ¹	Protected by Sacramento City Tree Preservation Code	Proposed for Removal (Total Trees)	Protected Trees Proposed for Removal	Trees impacted by the proposed development and requiring special protection measures
Tree of Heaven, Ailanthus altissima	8	8	1 (Private Protected)	8	1	-
Valley Oak, Quercus lobata	2	2	2 (City Trees)	0	0	2 (Tree #79326 & 79327)
Cork Oak, Quercus suber	1	1	0	1	0	
No. California Black Walnut, Juglans hindsii	6	3	1 (City Tree) 1 (Private Protected)	5	2	
Maidenhair Tree, Ginko biloba	1	1	1 (City Trees)	0	0	
Red Maple, Acer rubrum	1	1	1 (City Trees)	0	0	
Red Oak, Acer rubra	1	1	1 (City Trees)	0	0	
Modesto Ash, <i>Fraxinus velutina</i> 'Modesto'	1	1	0	1	0	
Pecan, Carya illinoisis	2	2	0	2	0	
Southern Magnolia, <i>Magnolia</i> grandiflora	1	1	0	1	0	
Total	24	21	8	15	3	2

¹ CalTLC is not a licensed land surveyor. Tree ownership was not determined. Conclusions within this report are based on existing fences or other landmarks which may not represent the actual property boundary.

Tree	Common	Botanical	Total	Condition	Development Status ²	Mitigation	Running Total
12	Tree of Heaven	Ailanthus altissima	26	2 Major Structure or Health Problems	Proposed for Removal with Waiver due to uncorrectable structural defect	0	0
32	No Cal Black Walnut	Juglans hindsii	42	1 Extreme Structure or Health Problems	Proposed for Removal with Waiver due to uncorrectable structural defect	0	0

Table 2 – Mitigation Requirements

See Appendices for specific information on each tree

There have been several revisions to this report. The original report was the result of a conceptual design in 2019 by Cartwright. In 2021, the report was revised based on a conceptual design by Ellis Architects. In 2022, revision were made based on City comments. In September of 2023, additional changes were made. This final revision is based on a request from Karl Vinge of NineteenSix Architects, the original arborist information (2019) and an abbreviated site visit by Edwin Stirtz, ISA Certified Arborist #0510A, on January 30th, 2024, to confirm the condition of the trees (note the Black Walnuts #30 and #32 were downgraded from arborist rating of 2 to an arborist rating of 1 as they are now mostly dead - See Photos), and the plans by Nineteensix Architects and Williams + Paddon, 23_1214_13C_CYC3_ PLANNING SET.

Methods

<u>Appendix B</u> in this report is the detailed inventory and recommendations for the trees. The following terms and Table A – Ratings Description will further explain our findings.

The protected trees evaluated as part of this report have a numbered tag that was placed on each one that is $1-1/8" \ge 1-3/8"$, green anodized aluminum, "acorn" shaped, and labeled: ABACUS, Auburn, CA with 1/4" pre-stamped tree number and Tree Tag. They are attached with a natural colored aluminum 10d nail, installed at approximately 6 feet above ground level on the approximate north side of the tree. The tag should last ~10 – 20+ years depending on the species, before it is enveloped by the trees' normal growth cycle.

A Level 2 – Basic Visual Assessment was performed in accordance with the International Society of Arboriculture's best management practices. This assessment level is limited to the observation of conditions and defects which are readily visible. Additional limiting factors, such as blackberries, poison oak, and/or debris piled at the base of a tree can inhibit the visual assessment.

Tree Location: The GPS location of each tree was collected using the ESRI's ArcGIS collector application on an Apple iPhone or Samsung. The data was then processed in ESRI's ArcMap by Julie McNamara, M.S. GISci, to produce the tree location map.

SKK Developments, 1300 & 1310 C Street, City of Sacramento

Tree Measurements: DBH (diameter breast high) is normally measured at 4'6" (above the average ground height for "Urban Forestry"), but if that varies then the location where it is measured is noted. A swedish caliper was used to measure the DBH for trees less than 23" in diameter and a steel diameter tape for trees greater than 23". A Stanley laser distance meter was used to measure distances. Canopy radius measurements may also have been estimated due to obstructions.

Terms

- The pre-stamped tree number on the tag which is installed at approximately 6 feet above ground level on Field Tag # the north side of the tree. The number listed on the City of Sacramento tree inventory in the ARC GIS system found online at: City Tag # saccity.maps.arcgis.com The species of a tree is listed by our local and correct common name and botanical name by genus Species (capitalized) and species (lower case). Oaks frequently cross-pollinate and hybridize, but the identification is towards the strongest characteristics. DBH Diameter breast high' is normally measured at 4'6" (above the average ground height for "Urban Forestry"), but if that varies then the location where it is measured is noted in the next column "measured at" DSH "Diameter at standard height" is the same as DBH except as follows (according to the City of Sacramento requirements): (1) For a tree that branches at or below 4.5 feet, DSH means the diameter at the narrowest point between the grade and the branching point; and (2) For a tree with a common root system that branches at the ground, DSH means the sum of the diameter of the largest trunk plus one-half the cumulative diameter of the remaining trunks at 4.5 feet above natural grade. The farthest extent of the crown composed of leaves and small twigs. Most trees are not evenly balanced. Canopy radius and This measurement represents the longest extension from the trunk to the outer canopy. The dripline Protection measurement is from the center point of the tree and is shown on the Tree Location Map as a circle. This Zone Area measurement further defines the radius of the protection zone to be specified on any development plans unless otherwise indicated in the arborist recommendations, Appendix 2. The radius of the critical root zone is a circle equal to the trunk diameter inches converted to feet and Critical Root Zone factored by tree age, condition and health pursuant to the industry standard. Best Management Practices: Managing Trees During Construction, the companion publication to the Approved American National Standard, provides guidance regarding minimum tree root protection zones for long term survival. In instances where a tree is multi-stemmed the protected root zone is equal to the extrapolated diameter (sum of the area of each stem converted to a single stem) factored by tree age, condition and health. Arborist Subjective to condition and is based on both the health and structure of the tree. All of the trees were rated Rating for condition, per the recognized national standard as set up by the Council of Tree and Landscape
 - Appraisers and the International Society of Arboriculture (ISA) on a numeric scale of 5 (being the highest) to 0 (the worst condition, dead) as in Chart A. The rating was done in the field at the time of the measuring and inspection.

Arborist Ratings

No problem(s)	Excellent	5
No apparent problem(s)	Good	4
Minor problem(s)	Fair	3
Major problem(s)	Fair to Poor	2



SKK Develo	pments, 1300 & 1310 C Street, C	ity of Sacramento		January 30, 2024
	Extreme problem(s)	Poor	1	
	Dead	Dead	0	

<u>Rating #0:</u> This indicates a tree that has no significant sign of life.

<u>Rating #1</u>: The problems are extreme. This rating is assigned to a tree that has structural and/or health problems that no amount of work or effort can change. The issues may or may not be considered a dangerous situation.

<u>Rating #2:</u> The tree has major problems. If the option is taken to preserve the tree, its condition could be improved with correct arboricultural work including, but not limited to: pruning, cabling, bracing, bolting, guying, spraying, mistletoe removal, vertical mulching, fertilization, etc. If the recommended actions are completed correctly, hazard can be reduced and the rating can be elevated to a 3. If no action is taken the tree is considered a liability and should be removed.

<u>Rating #3:</u> The tree is in fair condition. There are some minor structural or health problems that pose no immediate danger. When the recommended actions in an arborist report are completed correctly the defect(s) can be minimized or eliminated.

<u>Rating #4:</u> The tree is in good condition and there are no apparent problems that a Certified Arborist can see from a visual ground inspection. If potential structural or health problems are tended to at this stage future hazard can be reduced and more serious health problems can be averted.

<u>Rating #5</u>: No problems found from a visual ground inspection. Structurally, these trees have properly spaced branches and near perfect characteristics for the species. Highly rated trees are not common in natural or developed landscapes. No tree is ever perfect especially with the unpredictability of nature, but with this highest rating, the condition should be considered excellent.

Notes: Provide notable details about each tree which are factors considered in the determination of the tree rating including: (a) condition of root crown and/or roots; (b) condition of trunk; (c) condition of limbs and structure; (d) growth history and twig condition; (e) leaf appearance; and (f) dripline environment. Notes also indicate if the standard tree evaluation procedure was not followed (for example - why dbh may have been measured at a location other than the standard 54"). Additionally, notes will list any evaluation limiting factors such as debris at the base of a tree.

Development Recommended actions to increase health and longevity.

Restrictions/Actions

Development Impacts

Projected development impacts are based solely on distance relationships between tree location and grading. Field inspections and findings during the project at the time of grading and trenching can change relative impacts. Closely followed guidelines and requirements can result in a higher chance of survival, while requirements that are overlooked can result in a dramatically lower chance of survival. Impacts are measured as follows:

Impact Term:	Long Term Result of Impact:
Negligible	Tree is unlikely to show any symptoms. Chance of survival post development is excellent. Impacts to the Protected Root Zone are less than 5%.
Minor	Tree is likely to show minor symptoms. Chance of survival post development is good. Impacts to the Protected Root Zone are less than 15% and species tolerance is good.
Moderate	Tree is likely to show moderate symptoms. Chance of survival post development is fair. Impacts to the Protected Root Zone are less than 35% and species tolerance is good or moderate.
Severe	Tree is likely to show moderate symptoms annually and a pattern of decline. Chance of long term survival post development is low. Impacts to the Protected Root Zone are up to 50% and species tolerance is moderate to poor.



Critical

ity of SacramentoJanuary 30, 2024Tree is likely to show moderate to severe symptoms annually and a pattern of
decline. Chance of long term survival post development is negligible. Impacts to
the Protected Root Zone are up to 80%.

Discussion

Trees need to be protected from normal construction practices if they are to remain healthy and viable on the site. Our recommendations are based on experience and the County ordinance requirements to enhance tree longevity. This requires their root zones remain intact and viable despite the use of heavy equipment to install foundations, driveways, underground utilities, and landscape irrigation systems. Simply walking and driving on soil can have serious consequences for tree health. Tree Protection measures should be incorporated into the site plans in order to protect the trees.

Root Structure

The majority of a tree's roots are contained in a radius from the main trunk outward approximately two to three times the canopy of the tree. These roots are located in the top 6" to 3' of soil. It is a common misconception that a tree underground resembles the canopy. The correct root structure of a tree is in the drawing below. All plants' roots need both water and air for survival. Poor canopy development or canopy decline in mature trees after development is often the result of inadequate root space and/or soil compaction.



The reality of where roots are generally located

Our native oak trees are easily damaged or killed by having the soil within the <u>Protected Root Zone</u> (PRZ) disturbed or compacted. All of the work initially performed around protected trees that will be saved should be done by people rather than by wheeled or track type tractors. Oaks are fragile giants that can take little change in soil grade, compaction, or warm season watering. Don't be fooled into believing that warm season watering has no adverse effects on native oaks. Decline and eventual death can take as long as 5-20 years with poor care and inappropriate watering. Oaks can live hundreds of years if treated properly during construction, as well as later with proper pruning, and the appropriate landscape/irrigation design.

Arborist Classifications

There are different types of Arborists:

Tree Removal and/or Pruning Companies: These companies may be licensed by the State of California to do business, but they do not necessarily know anything about trees;

Arborists: Arborist is a broad term. It is intended to mean someone with specialized knowledge of trees but is often used to imply knowledge that is not there.



ISA Certified Arborist: An International Society of Arboriculture Certified Arborist is someone who has been trained and tested to have specialized knowledge of trees. You can look up certified arborists at the International Society of Arboriculture website: isa-arbor.org.

Consulting Arborist: An American Society of Consulting Arborists Registered Consulting Arborist is someone who has been trained and tested to have specialized knowledge of trees and trained and tested to provide high quality reports and documentation. You can look up registered consulting arborists at the American Society of Consulting Arborists website: asca-consultants.org

RECOMMENTATIONS: Summary of Tree Protection Measures for Site Planning

The Owner and/or Developer should ensure the project arborist's protection measures are incorporated into the site plans and followed. Tree specific protection measures can be found in Appendix 2 – Tree Information Data.

- The civil plans should be evaluated by the project arborist prior to any onsite work. Footings and utilities may have detrimental effects on the City street trees. All Impacts should be evaluated by the project arborist and addressed with the City Urban Forestry prior to permits.
- The stumps of the trees to be removed that are within the root zone of the City trees shall be removed using a backhoe or other piece of grading equipment only with supervision by the project arborist. Roots from the other nearby trees may have intertwined and will be required to be severed and cut clean during the removal process. Pulling on the stumps with equipment will likely result in the lifting of the asphalt in the parking areas on the adjacent parcels. In particular, tree 79326 should be monitored during removal of the other trees.
- Clearance pruning should include removal of all the lower foliage that may interfere with equipment PRIOR to having grading or other equipment on site or in the access path. The Project Arborist should approve the extent of foliage elevation and oversee the pruning to be performed by a contractor who is an ISA Certified Arborist.
- Clearly designate an area on the site outside the drip line of all trees on the adjacent parcels where construction materials may be stored and parking can take place. No materials or parking shall take place within the root zones of trees to be retained.
- Sewer line installation and trenching inside the root protection zone of trees to remain on the site shall be directly supervised by the project arborist. A hydraulic or air spade may be required for digging and placement of pipes underneath the roots, or boring of deeper trenches underneath the roots.
- Follow all of the General Development Guidelines, Appendix 3, for all trees not addressed within this report.

Report Prepared by:

Nicole Harrison ISA Certified Arborist #WC-6500AM, TRAQ Member: American Society of Consulting Arborists

Appendix 1 – Tree Location Map showing removals Appendix 2 – Tree Data



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Appendix 1 – Map of the Property showing tree location

Appendix 2 – Tree Data

Tree #	Private Protected Native +12"	Private Protected +24"	City Owned or Street Tree	Species Common Name	Species Botanical Name	DSH	Measure d at	Canopy Radius	Arborist Rating	Developme nt Status	Notes
1				Cork Oak	Quercus suber	19	54	19	3 Fair - Minor Problems	Remove	in corner of fenced area, flare under AC & raising public sidewalk, leans W 20 deg, low branches over driveway and street
2				Pecan	Carya illinoinensi s	6	54	12	3 Fair - Minor Problems	Remove	growing near fence 3", leans SW at 15', 1-sided crown SW, over extended canopy
3				Tree of Heaven	Ailanthus altissima	17	6	17	2 Major Structure or Health Problems	Remove	codominant junction at 2', growing into fence, dead branches to 2", over extended canopy
4				Modesto Ash	Fraxinus velutina 'Modesto,'	23	24	26	2 Major Structure or Health Problems	Remove	growing into fence flare & trunk, codominant junction at 3,5', incl bark, previous broken branches, line clearance pruned to S, over extended canopy
5				Tree of Heaven	Ailanthus altissima	10	54	17	2 Major Structure or Health Problems	Remove	growing thru fence, S leader cut off at 4', 1-sided N, dead branches to 2",
6				Tree of Heaven	Ailanthus altissima	7	54	14	2 Major Structure or Health Problems	Remove	clump of 7 stems growing in corner of fenced area,
7				Southern Magnolia	Magnolia grandiflora	14	60	11	1 Extreme Structure or Health Problems	Remove	severe decay in trunk to 6' and base, dying top, decay in upper crown



SKK Developments, 1300 & 1310 C Street,	City of Sacramento
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January 30, 2024

Tree #	Private Protected Native +12"	Private Protected +24"	City Owned or Street Tree	Species Common Name	Species Botanical Name	DSH	Measure d at	Canopy Radius	Arborist Rating	Developme nt Status	Notes
8				Tree of Heaven	Ailanthus altissima	6	54	13	2 Major Structure or Health Problems	Remove	flare in AC, cut laterals at 1&2', crowded top
9				Tree of Heaven	Ailanthus altissima	15	54	22	2 Major Structure or Health Problems	Remove	flare in AC, growing into fence, codominant junction at 6', dead branches to 2",
10				Northern California Black Walnut	Juglans hindsii	7	54	15	2 Major Structure or Health Problems	Remove	trunk surrounded by firewood pile, growing in AC next to fence, crossing codominant junction at 7', incl bark
11				Northern California Black Walnut	Juglans hindsii	18	24	32	2 Major Structure or Health Problems	Remove	growing in fence and AC, codominant junction at 3.5', dead branches to 2", over extended canopy
12		Yes		Tree of Heaven	Ailanthus altissima	26	18	33	2 Major Structure or Health Problems	Proposed for Removal with waiver for poor condition	growing into fence and AC and steel frame, codominant junction at 3', topped at 15' regrowth, over extended canopy
13				Tree of Heaven	Ailanthus altissima	14	54	26	1 Extreme Structure or Health Problems	Remove	growing around steel frame, leans NE 30 deg at 15', over extended branches over & touching building
14				Northern California Black Walnut	Juglans hindsii	14	54	23	2 Major Structure or Health Problems	Remove	leans E 30 degrees over building, previous topped at 18', 4 codominant junctions growing out, over extended canopy
15				Pecan	Carya illinoinensi s	10	54	23	3 Fair - Minor Problems	Remove	straight trunk, next to walnut & tree of heaven, low branches over drive, over extended canopy



Tree #	Private Protected Native +12"	Private Protected +24"	City Owned or Street Tree	Species Common Name	Species Botanical Name	DSH	Measure d at	Canopy Radius	Arborist Rating	Developme nt Status	Notes
16				Tree of Heaven	Ailanthus altissima	7	54	16	2 Major Structure or Health Problems	Remove	growing under concrete base of fan, next to building, bowed lower trunk, vertical growth & 1-sided crown to NE over and onto bldg. over extended canopy
30				Northern California Black Walnut	Juglans hindsii	18	54	15	1 Extreme Structure or Health Problems	Remove - Contact Property Owner	leans over alley 30 deg increasing with bend to 45 deg, burl SE trunk at 15', line clearance pruned, 1-sided crown to N, buried flare, broken branches, 90% dead crown (See Update 2024 in Photos) Offsite Tree
31		Yes		Northern California Black Walnut	Juglans hindsii	30	54	6	1 Extreme Structure or Health Problems		in property across alley, only low trunk sprouts, leans S into own property, 5' from alley
32		Yes		Northern California Black Walnut	Juglans hindsii	42	12	29	1 Extreme Structure or Health Problems	Proposed for Removal with waiver for poor condition	growing in alley, normal flare, codominant junction at 3', incl bark, E main leader co-dom at 5', S leader extends over opposite property by 29', N leader pruned for line clearance, W main leader co-dom at 9&10', W leader line clearance pruned, N leader co-dom at 20', N branch line clearance pruned, S leader. Ext add'l notes: S leader extends into opposite property by 27', W branch splits at 20', north branch line clearance pruned & extends over alley to fence of subject property, S branch extends over opposite property by 20' (See Update 2024 in Photos)



January 30, 2024

SKK Developments, 1300 & 1310 C Street, City of Sacramento

January	/ 30.	2024
Janaar	,,	

Tree #	Private Protected Native +12"	Private Protected +24"	City Owned or Street Tree	Species Common Name	Species Botanical Name	DSH	Measure d at	Canopy Radius	Arborist Rating	Developme nt Status	Notes
79232			Yes	Maidenhair Tree	Ginko biloba	11	54	15	3 Fair - Minor Problems		parkway tree, slightly buried flare, female fruiting, central leader w 4 long laterals, over extended canopy, trunk wound W side 0-4', low branches over Sidewalk
79233			Yes	Red Oak	Quercus rubra	4	54	8	3 Fair - Minor Problems		parkway tree, good flare, central leader growing SE, low branches over sidewalk & bike lane
79326		Yes	Yes	Valley Oak	Quercus lobata	18	54	26	3 Fair - Minor Problems	Impact - TBD	parkway strip, good flare, lean N 30 degree self correcting, extends 8' into fenced area, over extended canopy
79327		Yes	Yes	Valley Oak	Quercus Iobata	24	54	37	3 Fair - Minor Problems	Impact - TBD	in planter, raised adjacent sidewalk, good flare, extends 25' into fenced area 3 co-dom at 10', over extended canopy, small dead branches
111067			Yes	Red Maple	Acer rubrum	5	48	10	3 Fair - Minor Problems		good flare, long low vertical lateral S, E, NW, central leader a south upper vertical lateral, small branch crossing central leader incl bark, low branches over bike lane, 4,5' N of underground steel vault (electrical?)



APPENDIX 3

GENERAL PRACTICES FOR TREE PROTECTION

Definitions

<u>Root zone</u>: The roots of trees grow fairly close to the surface of the soil, and spread out in a radial direction from the trunk of tree. A general rule of thumb is that they spread 2 to 3 times the radius of the canopy, or 1 to 1 ½ times the height of the tree. It is generally accepted that disturbance to root zones should be kept as far as possible from the trunk of a tree.

<u>Inner Bark</u>: The bark on large valley oaks and coast live oaks is quite thick, usually 1" to 2". If the bark is knocked off a tree, the inner bark, or cambial region, is exposed or removed. The cambial zone is the area of tissue responsible for adding new layers to the tree each year, so by removing it, the tree can only grow new tissue from the edges of the wound. In addition, the wood of the tree is exposed to decay fungi, so the trunk present at the time of the injury becomes susceptible to decay. Tree protection measures require that no activities occur which can knock the bark off the trees.

Methods Used in Tree Protection:

No matter how detailed Tree Protection Measures are in the initial Arborist Report, they will not accomplish their stated purpose unless they are applied to individual trees and a Project Arborist is hired to oversee the construction. The Project Arborist should have the ability to enforce the Protection Measures. The Project Arborist should be hired as soon as possible to assist in design and to become familiar with the project. He must be able to read and understand the project drawings and interpret the specifications. He should also have the ability to cooperate with the contractor, incorporating the contractor's ideas on how to accomplish the protection measures, wherever possible. It is advisable for the Project Arborist to be present at the Pre-Bid tour of the site, to answer questions the contractors may have about Tree Protection Measures. This also lets the contractors know how important tree preservation is to the developer.

<u>Root Protection Zone (RPZ)</u>: Since in most construction projects it is not possible to protect the entire root zone of a tree, a Root Protection Zone is established for each tree to be preserved. The minimum Root Protection Zone is the area underneath the tree's canopy (out to the dripline, or edge of the canopy), plus 10'. The Project Arborist must approve work within the RPZ.

Irrigate, Fertilize, Mulch: Prior to grading on the site near any tree, the area within the Tree Protection fence should be fertilized with 4 pounds of nitrogen per 1000 square feet, and the fertilizer irrigated in. The irrigation should percolate at least 24 inches into the soil. This should be done no less than 2 weeks prior to grading or other root disturbing activities. After irrigating, cover the RPZ with at least 12" of leaf and twig mulch. Such mulch can be obtained from chipping or grinding the limbs of any trees removed on the site. Acceptable mulches can be obtained from nurseries or other commercial sources. Fibrous or shredded redwood or cedar bark mulch shall not be used anywhere on site.

<u>Fence</u>: Fence around the Root Protection Zone and restrict activity therein to prevent soil compaction by vehicles, foot traffic or material storage. The fenced area shall be off limits to all construction equipment, unless there is express written notification provided by the Project Arborist, and impacts are discussed and mitigated prior to work commencing.

No storage or cleaning of equipment or materials, or parking of any equipment can take place within the fenced off area, known as the RPZ.

The fence should be highly visible, and stout enough to keep vehicles and other equipment out. I recommend the fence be made of orange plastic protective fencing, kept in place by t-posts set no farther apart than 6'.

In areas of intense impact, a 6' chain link fence is preferred.

In areas with many trees, the RPZ can be fenced as one unit, rather than separately for each tree.

Where tree trunks are within 3' of the construction area, place 2" by 4" boards vertically against the tree trunks, even if fenced off. Hold the boards in place with wire. Do not nail them directly to the tree. The purpose of the boards is to protect the trunk, should any equipment stray into the RPZ.

<u>Elevate Foliage</u>: Where indicated, remove lower foliage from a tree to prevent limb breakage by equipment. Low foliage can usually be removed without harming the tree, unless more than 25% of the foliage is removed. Branches need to be removed at the anatomically correct location in order to prevent decay organisms from entering the trunk. For this reason, a contractor who is an ISA Certified Arborist should perform all pruning on protected trees.³

<u>Expose and Cut Roots</u>: Breaking roots with a backhoe, or crushing them with a grader, causes significant injury, which may subject the roots to decay. Ripping roots may cause them to splinter toward the base of the tree, creating much more injury than a clean cut would make. At any location where the root zone of a tree will be impacted by a trench or a cut (including a cut required for a fill and compaction), the roots shall be exposed with either a backhoe digging radially to the trunk, by hand digging, or by a hydraulic air spade, and then cut cleanly with a sharp instrument, such as chainsaw with a carbide chain. Once the roots are severed, the area behind the cut should be moistened and mulched. A root protection fence should also be erected to protect the remaining roots, if it is not already in place. Further grading or backhoe work required outside the established RPZ can then continue without further protection measures.

<u>Protect Roots in Deeper Trenches</u>: The location of utilities on the site can be very detrimental to trees. Design the project to use as few trenches as possible, and to keep them away from the major trees to be protected. Wherever possible, in areas where trenches will be very deep, consider boring under the roots of the trees, rather than digging the trench through the roots. This technique can be quite useful for utility trenches and pipelines.

<u>Protect Roots in Small Trenches:</u> After all construction is complete on a site, it is not unusual for the landscape contractor to come in and sever a large number of "preserved" roots during the installation of irrigation systems. The Project Arborist must therefore approve the landscape and irrigation plans. The irrigation system needs to be designed so the main lines are located outside the root zone of major trees, and the secondary lines are either laid on the surface (drip systems), or carefully dug with a hydraulic or air spade, and the flexible pipe fed underneath the major roots.

³ International Society of Arboriculture (ISA), maintains a program of Certifying individuals. Each Certified Arborist has a number and must maintain continuing education credits to remain Certified.



SKK Developments, 1300 & 1310 C Street, City of Sacramento

Design the irrigation system so it can slowly apply water (no more than $\frac{1}{2}$ " to $\frac{1}{2}$ " of water per hour) over a longer period of time. This allows deep soaking of root zones. The system also needs to accommodate infrequent irrigation settings of once or twice a month, rather than several times a week.

<u>Monitoring Tree Health During and After Construction</u>: The Project Arborist should visit the site at least twice a month during construction to be certain the tree protection measures are being followed, to monitor the health of impacted trees, and make recommendations as to irrigation or other needs. After construction is complete, the arborist should monitor the site monthly for one year and make recommendations for care where needed. If longer term monitoring is required, the arborist should report this to the developer and the planning agency overseeing the project.



Appendix 4 – Site Photographs

Photos from 2021



City Tree 79327



City Tree 79326 – Pruning includes removal of (1) 6" branch

January 30, 2024

359 Nevada Street, #201, Auburn, CA 95603

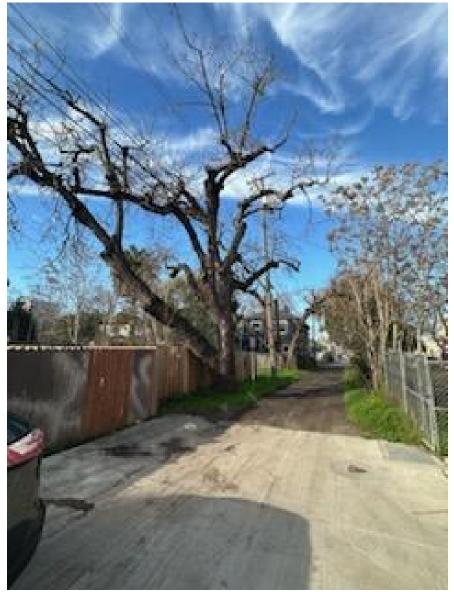
Photos from 2021



Other City Trees require only minor or standard pruning



Site Photos by Edwin Stirtz, January 30, 2024.





Tree #32. 80% dead with weak attachments and several old wounds with incipient decay in all the larger stems. Topped for utility lines.



SKK Developments, 1300 & 1310 C Street, City of Sacramento



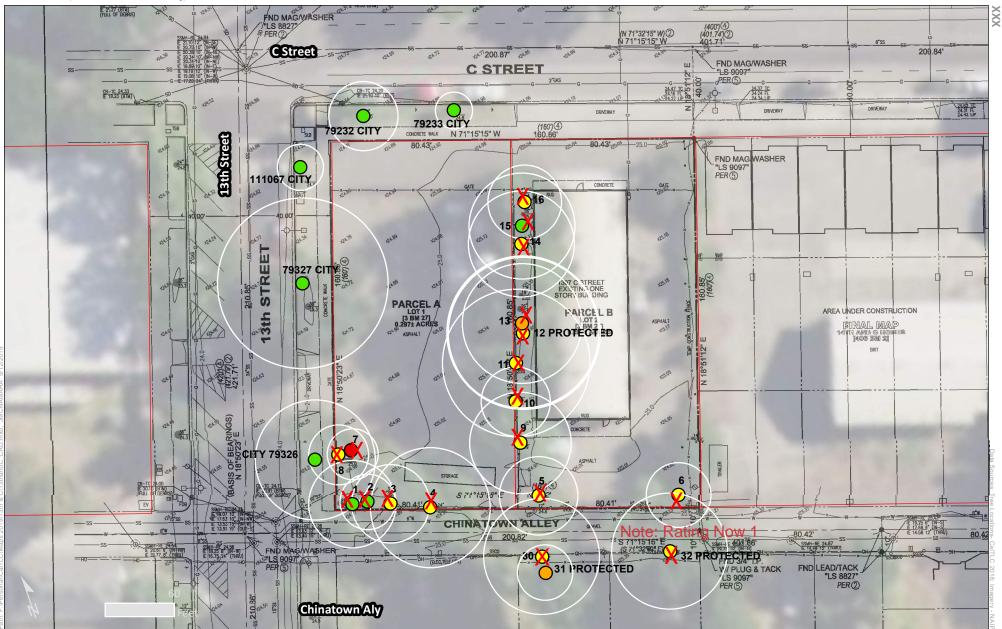
Tree #30. Severe lean, 90% dead, significant old mistletoe canker at 12', weak attachments, all the large branches are dead.



13th Street & C Street

TREE LOCATION MAP

Sacramento, Sacramento County, California.





 >Tree locations are approximate and were collected using ISO apple products.
 >Property line information was downloaded from Sacramento County on 09/12/2019.



9/12/2019 Revised January 30, 2024.



June 2, 2023

Sierra Peterson, Associate Planner Department of Community Development 300 Richards Boulevard, 3rd Floor Sacramento, CA 95811

RE: 13C Apartments (DR23-088)

Dear Sierra,

Thank you for providing the Sacramento Area Bicycle Advocates (SABA) the opportunity to comment on the following development. SABA is a non-profit advocacy organization dedicated to enabling people in all communities to make more and safer trips by bicycle. The 13C project proposes the construction of 127 studio apartments on an underused, infill lot with one existing building which is to be demolished. The site qualifies as a Transit-Oriented Development (TOD) and lies within ¼ mile of the Alkali Flat/La Valentina light rail station. Additionally, significant to the project and our recommendations below is not only the project's general location within the Central City offering residents excellent accessibility to numerous and diverse destinations, but the project resides one block from the newly constructed 12th Street Class I Cycle Track providing access to Two Rivers Bike Trail and all amenities along the way via a bi-directional, physically protected bike lane.

SABA would like to **commend** the project on several counts of thoughtful design. Given the numerous important roles trees play in creating healthy, vibrant communities including providing shade for users of active transportation, it is appreciated that the developers will retain 5 City trees bordering the property. As a TOD development, we commend developers for capitalizing on the parking alternative requirements and providing zero on-site vehicle parking spaces. The elimination of on-site vehicle parking has many benefits including lowering the cost of the construction which can in turn lower the rent for residents and it incentivizes alternative travel modes. Finally, we commend the location of the long-term bike storage room and its immediate access to entering and exiting the site rather than navigating hallways or additional doors as well as the placement of short-term bike racks immediately adjacent to the building entrance.

SABA would like to **recommend** the following additions which we feel capitalize and enhance the existing attributes mentioned above.

With concern to the provision of total long-term bike storage, particularly relative to the fact that zero on-site vehicle parking is provided, we recommend the bicycle storage room capacity be increased to 90 bikes which support secure parking for all residents living on the second and third floors. First floor residents would ideally be provided with in-unit storage options such as wall-hooks and would benefit from not having to carry their bikes up one or two flights of stairs. SABA also recommends with concern

to bike room design and amenities that charging <u>lockers</u> or <u>racks</u> be installed for e-bikes and a bike pump and repair stand be installed to assist with simple maintenance and adjustment needs without needing to transport tools or bikes to and from resident's studios.

Wayfinding signage aligned with biking to key destinations should be included in the building design. We recommend this taking one of two forms: 1) displaying signs indicating biking distances and times as seen in Figure 1 below either in the City owned planter area between the road and sidewalk or on the building, or 2) Install a community bulletin board in the bike storage room which displays a map of the City's bicycle network. Making it easy for residents to understand what destinations are near to them and what prioritized routes are available will help to actualize the convenience of either not owning or not using a private vehicle and subsequently enhance the desirability to live in the building. Suggested wayfinding points may include the 12th Street Class I bike lane, Alkali Flat/La Valentin light rail station, Sacramento Valley Station, and/or regional park such as Sutter's Landing.



Figure 1: Bike Wayfinding Distance and Time.

Finally, pedestrian lighting should be installed in China Alley to ensure a well lit area is provided for residents to see and feel comfortable using the bike room at night. Alleys, particularly dimly lit or those without any lighting can be imposing and dissuade someone from use.

Sheet C3.1 of the plans indicate a vertical curb being installed directly in front of the access pad to the bike room. **We recommend eliminating this or installing a curb ramp** so that people are not having to lift their bikes up or down a curb each time which can be particularly challenging if a bike is used to transport heavy items such as groceries.

Cycling is an equitable, environmentally friendly, and widely beneficial means of transportation addressing a multitude of the City's goals as they pertain to reductions in GHG emissions, traffic collisions, and congestion while improving equity. It is our vision that all people should be able to safely and practically arrive wherever they want to go. While we hope the developer will consider SABA's recommendations, we support this project and look forward to its addition to the Central City.

If you have any questions or comments pertaining to the content of this letter, please feel free to contact me at 707 972-3580 or <u>david@sacbike.org</u>.

Sincerely,

An

David Moore, Policy and Planning, Sacramento Area Bicycle Advocates

**links are to demonstrate concepts only and not meant to recommend individual company or product.*



June 1, 2023

VIA EMAIL

Sierra Peterson, Associate Planner City of Sacramento, Planning Division, Department of Community Development 300 Richards Boulevard, 3rd Floor Sacramento, CA 95811

RE: 13C Studios (DR23-088)

Dear Sierra:

Thank you for routing the 13C Studios project to Civic Thread. The project proposes construction of a three-story multi-family building containing 127 studio apartments in the M-1-SPD and Central City SPD with a deviation for required open space and permitting for the removal of 16 on-site trees. The location of the project qualifies it as both a TOD development within a quarter mile of the Alkali Flat/La Valentin light rail station and as an infill development. Being located in the Central City makes the project highly supportive of biking and walking as a means of accessing nearby destinations and as a TOD, provides regional access without the reliance on a personal vehicle.

Civic Thread would like to commend several design choices made by developers. Capitalizing on the project site's walking and biking accessibility and the City's TOD parking ordinance, the project provides zero on-site vehicle parking stalls. While providing incentives to use walking or biking are necessary, disincentivizing vehicle parking further supports resident's choice to walk, bike, or take transit, and numerous City congestion, VMT reduction, and GHG emission reduction goals. Retaining the City trees along the perimeter of the project support efforts to combat the Urban Heat Island effect and maintains comfortable ambient temperatures for pedestrian travel during the hot summer months. Finally, the access door to the secure bicycle parking area is ideally located in China Alley providing easy, single door access to and from the bike room.

With the positive attributes of this project noted, Civic Thread would like to make the following recommendation which we feel enhance the already commendable characteristics of the project.

Given the absence of on-site vehicle parking and significant efficiencies gained via biking versus walking, we strongly feel the development should provide long-term bike parking accommodations at a 1:1 ratio. We recommend increasing the bicycle storage room capacity to 90 from the stated 64 which would provide all residents on the second and third floors with secure ground level parking opportunities. Dedicated in-unit bike parking provisions such as wall hooks should be located in the 37 ground floor units. Additionally requested bike parking supports both current and future demand for bicycle travel.

China Alley should be equipped with appropriate pedestrian lighting to ensure residents feel safe accessing the bike room at night while not producing excessive light pollution to nearby neighbors. The access door to the bike storage room should be secure and accessible only via key or fob provided to residents.

Given the project's lack of on-site vehicle parking and likelihood that demand for onstreet parking will increase as a result of the project, we strongly recommend the developers work with City of Sacramento staff to "daylight" the southeastern corner of the 13th Street and C Street intersection. Daylighting is the term used to eliminate any visual barriers, often times vehicular parking, within, generally, 20 to 25 feet from crosswalks or curb cuts. **Specifically, we urge the developers to incorporate the installation of "No Parking Any Time" signs and painting the curbs red as indicated by the red lines in Figure 1, below.** Pedestrians are placed at significantly higher risk of collisions when they are unable to see or be seen by drivers, even if a stop sign is present.



Figure 1: 13C No Parking Any Time Locations

909 12th Street, Suite 125, Sacramento, CA 95814 916-446-9255 • www.civicthread.org Finally, to establish a sense of placemaking to the exterior of the building which currently resembles the aesthetics of many new residential developments being constructed in the Central City, **we recommend developers hire a local artist(s) to complete a mural on the side facing either C or 13th Street.** This would support the local arts that make the Central City a vibrant place to live as well as provide the development with a unique

Civic Thread is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling. The benefits include improved public health and physical fitness, better air quality, a stronger sense of cohesion and safety in neighborhoods, and more sustainable communities and local economies.

Please notify Civic Thread of future routings or notices for this project.

identity and gravity within the Central City landscape.

Sincerely,

David Moore

David Moore Community Planning Specialist June 15, 2023

Community Development Department City of Sacramento 915 I Street Sacramento, CA 95814

Re: Development Proposal for 13C Studios (1300 & 1310 C Street)

Hello Ms. Peterson,

I am writing on behalf of Midtown Association to express our support for the proposed development at 1300 and 1310 C Street, 13C Studios.

Midtown Association is a Property Based Improvement District that represents over 1,200 properties in the Central City. Our mission is to make Midtown the center for culture, creativity, and vibrancy in Sacramento's urban core. A central pillar of that mission is to pursue the development of responsible urban infill projects that will supply the city with a diverse housing stock available to meet the needs of people of all income levels. This proposed project offers a type of development in which the city is severely lacking.

The need for a robust housing supply cannot be overstated. In concert with our current shortage of residential units is a need for urban areas to adapt to a changed, post-covid landscape. To take full advantage of the opportunities present within the central city, we must look to spaces that are underutilized with the potential for innovative development.

13C Studios is a 127-unit residential development proposed development within an area of the central city that has been historically underutilized. Recently, high-density projects in the area have begun to help transition old, unutilized commercial buildings into multifamily, residential projects that uplift the surrounding community by bringing in new residents and vibrancy. The proposed project would bring 127 workforce housing units within blocks of public transit hubs and major corridors. Additionally, fifty percent of all units will be restricted to affordable housing.

In order to maximize density and affordability, the project applicant is requesting a deviation from the city's open air space requirements of fourteen percent. We ask that the City joins Midtown Association in supporting this project by approving entitlement for this project with the requested deviation.

Thank you,

Emily Baime Michaels Executive Director Midtown Association



THE CENTER FOR CULTURE, CREATIVITY & VIBRANCY IN SACRAMENTO'S URBAN CORE.

From:	Natalie Setliff
To:	Sierra Peterson
Cc:	Christopher Gould
Subject:	AgencyCounter: DR23-088 inquiry
Date:	Monday, May 15, 2023 5:05:03 PM
Attachments:	image.png

Hi Sierra,

I am writing to express my concern regarding the proposed '1300 C' development at the corner of C and 13th St in the Mansion Flats neighborhood where my husband and I live.

We purchased our home in the Icon @14C development last year and moved in last summer. We love living here and are excited for new things to come to the neighborhood so it can grow and become more vibrant. I am very pro-housing and pro-affordable housing at that. The unhoused population in Sacramento must be addressed, the sioner the better.

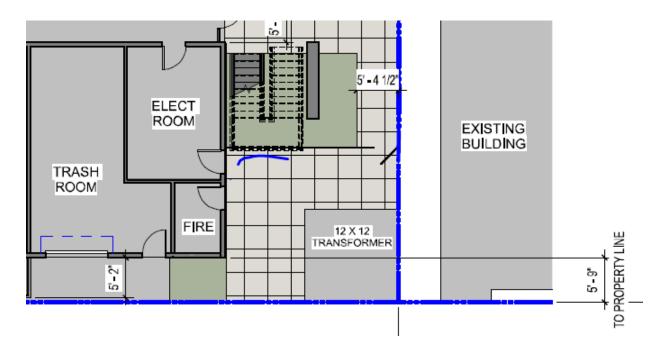
I say all this to reiterate my support for housing, and high-density housing at that. However, as a direct neighbor of this new proposed development, I and my neighbors in our community have some serious concerns.

Our primary concern relates to the high volume of very small units planned. When we originally purchased our home, we were informed that the proposed plans were to include 18 new units total with mixed use retail space - a much welcomed and necessary addition to the neighborhood where amenities are not exactly abundant. 18 is a reasonable amount of homes for the size of the lot and the available street parking for the area. To cram more than **120** tiny units into this space and not allow for any parking is an extreme oversight, and negligent to the existing neighbors.

Parking is difficult enough as it is where many have garages, especially for guests. The area is not sufficiently built up with amenities that offer basic necessities like grocery shopping within walking distance. The idea that residents will just not have cars is delusional. The light rail is completely insufficient as a response to this issue, either, and biking is unsafe with the complete lack of bike lanes and safe drivers. The other factor is these 120+ homes will likely have guests - they will have absolutely no where to park. This is how resentment builds up in a community.

With this volume of residents in this small a space, Chinatown alley, where our garage is located, is going to be wildly overrun with traffic, rendering it impossible to come and go safely out of our garage.

Our other concern for our individual home is the plans that have been drawn that depict a large 12' x 12' transformer literally right outside, directly below our bedroom window (Our home, 30 Icon Walk, is labeled "Existing building" in the screenshot below - I'd like to point out that it is a *home*, not just a "building". The 'trash room' being merely a few more feet away is also a huge concern. This does not feel safe and is a complete eyesore, along with the noise pollution that will come with it. I do not understand why this unit would be situated directly to an existing home, it is an affront to existing communities and not neighborly in the slightest.



Lastly, I will leave you with this. The proposed development has zero character, whatsoever. It is a massive concrete box designed to cram as many people in as possible into tiny studios that will be completely unattractive to any would-be resident. With no parking and barely any space inside these tiny, unattractive studio apartments, how can the developer expect to actually fill these units? Midtown and downtown Sacramento need more high-density housing, that is not a disputable fact. But Sacramento also has charm, and these units fly in the face of that.

Please consider current residents' feedback when approaching this project's approval and advise how we can continue to submit comments so that a project that can exist in harmony with the neighborhood can be achieved.

Thank you for your time and attention.

Best,

Natalie Setliff

Hi Sierra,

I'm a neighbor to the proposed development at 1300 C Street in Sacramento (DR23-088). I wanted to submit a few comments for the Planning Department's consideration as they review the development. If you don't mind confirming receipt of these comments I would appreciate it.

Thanks! Josh Bardet

- 1. **Street Improvements** Given the residential density of the proposed project with respect to adjacent uses, it would behoove the city to require and the developer to install basic improvements on adjacent streets. C Street receives a relative large volume of through traffic driving from 12th to 15th and 16th Streets. This includes commercial traffic traveling to nearby properties such as Amador Buses and Blue Diamond Almond Trucks. In the interest of lowering speed of vehicles through the neighborhood and promoting pedestrian safety (especially for a transit oriented development), I would encourage the installation of a stop controlled intersection and bulb outs at 13th and C Street as part of this project. (Bulb outs on the South side of C S treet with the understanding they are not feasible on the North side due to Amador bus movements).
- 2. **Open Space Requirement** I strongly oppose the traditional open space requirement (50SF/unit) be waived for this project, as was proposed by the developer, without an alternative proposal to meet the city's intent in adopting that requirement. I reject the notion that affordable housing doesn't pencil with an additional 10 sf of open space allocated per unit (above the ~40SF currently proposed).
- 3. **Bicycle Parking** This development is proposed with the assumption that no residents or their visitors will use personal vehicles. If that is the case, it seems that more than 1 bicycle parking stall per 2 units should be provided on site (ideally, at least 1 space per unit).
- 4. **Design and Layout** The rendering of adjacent setbacks and location of adjacent structures is misleading on SD-series sheets (plan views as well as the perspective views on SD-203). Existing town homes on C are set back a minimum of 5'-7" from the property line. I would request that a consistent setback be maintained along the frontage as part of the new development.
- 5. **Design and Layout** The Sidewalk on C Street fronting the adjacent property (1312 C Street) is 8 foot wide, and was improved from 6 feet as part of the adjacent development. A consistent improvement to width should be made to the proposed project's frontage such that the entire block includes an 8 foot wide sidewalk.
- 6. **Design and Layout** The proposed development shows fencing and screening at the back of the walk (C Street and 13th Street), providing for no visual continuity between the the proposed building and the street. This screening is also inconsistent with all adjacent residential uses on the block and should not be permitted in the proposed location. My comment is to revise the design such that a fence a the property line is not necessary and some setback, however small, can be included.
- 7. Application Details it is unclear if there are 126 or 127 units. The project narrative

indicates 127 whereas plans indicate 126.

8. **Retail** - Mansion Flats, especially the north side near C Street, would benefit tremendously from additional retail spaces. It would be nice for this project to include at least some ground floor retail to support not only its own residents but the surrounding neighborhood in general.

Hello Sierra

I own and reside at 1312 C St and have several concerns about the new proposal from SSK for the lot at 1300 C Street.

When I purchased my property 3 years ago it was with the understanding that there would be a condo development some time in the future, rather than high density apartments. I am significant concerned about my future property value based on this proposal.

I am concerned about traffic on C Street. There is already a large amount of traffic that cuts through between 12th and 16th. Many of them speeding and a surprising number of people who run the stop sign at 14th and C or slam their breaks on at the last second. I am concerned that adding that number of units will greatly increase the traffic and noise that it generates. There is already significant noise from the aforementioned traffic, the train tracks, and the businesses on the north side of C St. It is difficult for me to imagine the overall increase in noise that would occur with that many people living in such a dense space-music, voices, televisions, and the foot traffic.

Parking is a serious concern. I live in a development that was built in 3 phases of 7 units each. Since the third phase was finished and occupied and it has become increasingly difficult to park on C St and 14th for both myself and my guests. Yes some individuals will not have cars but I would like to see the statistics on the percent of car ownership in similar type buildings. Even the low cost units on 12th Street between C and E have parking.

And although there is nearby transit there are no nearby grocery stores. There are small corner stores but they do not carry the type of perishables needed for everyday consumption.

I am concerned for my view and privacy. It was one thing to plan for the eventuality that I would likely have a condo looking out my west facing windows but now the proposal would include significantly more individuals that I would be confronted with. I don't want to see their private business any more than I want them to see mine. I am also concerned about my natural light being blocked.

I am opposed to this plan for this lot. I have seen no other similar sized lot with this many units proposed. I am concerned that SSK must be receiving significant incentives to build this many affordable units. I am curious how will the property be managed. How will the affordable units be subsidized. Is there a plan to use the units for housing the homeless or those with mental illness. How will these individuals be supported.

I do not support this proposal. I think the location is not ideal for that many affordable units. There is no nearby grocery, health care, etc and I feel the impact on the neighborhood is significant.

Amy D. Haynes 831-206-1392

Sent from my iPhone

From:	Jeff Kessler
To:	Sierra Peterson
Cc:	Joshua@regionbusiness.org; Allie Weill
Subject:	Re: Fw: DR23-088 - Project Routing
Date:	Monday, June 5, 2023 6:55:00 PM
Attachments:	image001.png

Hello,

I'm writing in to strongly support this project. My one reservation is that the amount of bike racking/parking and general bike infrastructure to support this project is severely underwhelming.

* This project only allocates bike parking for approximately half of the residences. The reality is that this project is positioned to promote active transport around Sacramento, which means bike parking should be sufficient to cover 1.2 to 1.5 times the number of units, when accounting for numerous bikes owned by individuals and guests biking to properties. Given the small footprint for each unit, it is unreasonable to suggest that residents sacrifice critical indoor space within their apartments for bicycle storage. Providing adequate, safe bike storage and rack systems

is especially important to protect ebicycles, which are often more expensive than many individuals' cars, and substantially improve car-free travel within the Sacramento area.

Additionally:

* the bike infrastructure in the area needs substantial improvements. 16th Street is a fast-paced arterial road that creates significant disconnectivity in the area. One lane should be removed along 16th street so it can be dedicated as a two-way bike and pedestrian way.

* The bike infrastructure along 13th is lacking. Around G street the bike lane drops out, and is replaced by angled-in parking. With an influx of additional units, providing more dedicated bike connectivity on the surrounding streets is essential for avoiding reliance on automobiles.

* Please improve the genera planning around bikes and active transport if you are designing dense properties without parking considerations on-site.

* I also think it will be essential to start instituting paid-for parking permits, rather than the free parking on streets currently provided. Allocating street parking to residences without collecting use fees is contributing to an over-abundance of vehicles fighting for limited parking spots. We need to reduce free parking to further promote unit uptake from folks less interested in driving, and to better support transit and other options less reliant on automobile dependency.

Finally, I would love for the consideration of a mixed-use property, that includes retail/commercial space on the ground floor to provide ample third spaces within the community to convene.

Thanks for considering my comments, Jeff Kessler

On Fri, May 12, 2023, 2:00 PM sean wright <<u>emailsean@msn.com</u>> wrote: | FYI

From: Sierra Peterson <<u>SPeterson@cityofsacramento.org</u>> Sent: Thursday, May 11, 2023 2:39 PM

To: Sierra Peterson <<u>SPeterson@cityofsacramento.org</u>>

Cc: Katie Valenzuela (City) <<u>kvalenzuela@cityofsacramento.org</u>>; Zachary Freels

<ZFreels@cityofsacramento.org>; Karlo Felix <KFelix@cityofsacramento.org>; Zarah Lacson <<u>ZLacson@cityofsacramento.org</u>>; Jennifer Donlon Wyant <<u>JDonlonWyant@cityofsacramento.org</u>>; SFD Planning Entitlements cplanningentitlements@sfd.cityofsacramento.org; DOU Entitlement Review <<u>douentitlementreview@citvofsacramento.org</u>>; Kaleb Haile <<u>KHaile@citvofsacramento.org</u>>; Bridgette Williams <<u>BWilliams@cityofsacramento.org</u>>; Jason Hone <<u>JHone@cityofsacramento.org</u>>; Elise Gumm <EGumm@cityofsacramento.org>; Bruce Monighan <BMonighan@cityofsacramento.org>; Matthew Sites <<u>MSites@cityofsacramento.org</u>>; Manuel Martinez <<u>MMartinez@cityofsacramento.org</u>>; D3.Local.Development@dot.ca.gov <D3.Local.Development@dot.ca.gov>; wsaunders@cadanet.org <wsaunders@cadanet.org>; jimd@sac-city.k12.ca.us <jimd@sac-city.k12.ca.us>; Crystal Hoff <crystalhoff@scusd.edu>; Amna Javed <<u>Amna-Javed@scusd.edu</u>>; cat.creswell@comcast.net <<u>cat.creswell@comcast.net</u>>; <u>info@housesac.org</u> <<u>info@housesac.org</u>>; Devin Strecker <devin@riverdistrict.net>; sean wright <emailsean@msn.com>; Anna Tekautz <<u>ATekautz@cityofsacramento.org</u>>; Saul Moran <<u>SMoran@cityofsacramento.org</u>>; Nelson Baligod <<u>NBaligod@citvofsacramento.org</u>>; Jeffrey Heeren <<u>JHeeren@citvofsacramento.org</u>>; Kourtney Burdick <<u>KBurdick@cityofsacramento.org</u>>; Tom Pace <<u>TPace@cityofsacramento.org</u>>; Stacia Cosgrove <<u>SCosgrove@cityofsacramento.org</u>>; Leslie Fritzsche <<u>LFritzsche@cityofsacramento.org</u>>; Lynette Hall <<u>LHall@cityofsacramento.org</u>>; Tom Buford <<u>TBuford@cityofsacramento.org</u>>; Scott Johnson <<u>SRJohnson@cityofsacramento.org</u>>; Ron Bess <<u>RBess@cityofsacramento.org</u>>; Greg Sandlund <<u>GSandlund@cityofsacramento.org</u>>; Matt Hertel <<u>MHertel@cityofsacramento.org</u>>; Remi Mendoza <<u>RMendoza@cityofsacramento.org</u>>; Nguyen N. Nguyen <<u>NNNguyen@cityofsacramento.org</u>>; Kelli Trapani <<u>KTrapani@cityofsacramento.org</u>>; Randy Singh <<u>RSingh@cityofsacramento.org</u>>; Sheri Smith <<u>SSmith@cityofsacramento.org</u>>; Eric Frederick <<u>EFrederick@cityofsacramento.org</u>>; Jessica Steinhauer Listeinhauer@cityofsacramento.org>; Dana Repan < DRepan@cityofsacramento.org>; pgeplanreview@pge.com <pgeplanreview@pge.com>; Matt Young <<u>myoung@pd.cityofsacramento.org</u>>; projectreview@airquality.org <projectreview@airquality.org>; sasddevservices@sacsewer.com <<u>sasddevservices@sacsewer.com</u>>; armstrongro@sacsewer.com <<u>armstrongro@sacsewer.com</u>>; RSW Plan Review <rswplanreview@cityofsacramento.org>; entitlements@smud.org <entitlements@smud.org>; Kevin A. Hocker <<u>KHocker@cityofsacramento.org</u>>; Colleen M Costa <<u>cmcosta@cityofsacramento.org</u>>; rtplanning@sacrt.com <rtplanning@sacrt.com>; kanderson@aiacv.org <kanderson@aiacv.org>; ProjectReview@civicthread.org <ProjectReview@civicthread.org>; kmatthews@auburnrancheria.com <kmatthews@auburnrancheria.com>; ACheng@auburnrancheria.com CPD@wiltonrancheria-nsn.gov <CPD@wiltonrancheria-nsn.gov>; KPerry@ssband.org <KPerry@ssband.org>; ivan@bvtribe.com <ivan@bvtribe.com>; debra banks <deb@sacbike.org>; bvandermeer@biziournals.com <<u>bvandermeer@bizjournals.com</u>>; <u>rlillis@sacbee.com</u> <<u>rlillis@sacbee.com</u>>; <u>tclift@sacbee.com</u> <tclift@sacbee.com>; itaylor@kmaxtv.com <itaylor@kmaxtv.com>; Preservation.Sacramento@gmail.com <Preservation.Sacramento@gmail.com>; ProjectReview@civicthread.org <ProjectReview@civicthread.org>; Joshua@RegionBusiness.org < Joshua@RegionBusiness.org> Subject: DR23-088 - Project Routing

Hello,

The Planning Division has received a new development application. Please see the information below about project file number **DR22-088**. The application materials can be accessed from the **ATTACHMENTS** link below.

Please send your <u>written</u> comments to me by 6/6/23. Do not hesitate to contact me if you have any questions about the project or issues accessing the application materials.

PROJECT INFORMATION	
Project Name:	13C Studios
Project Summary:	A request for: 1) Site Plan and Design Review for the construction of 126 studio apartment units in the Light Industrial (M-1-SPD) zone and Central City SPD with a deviation for required open space; and 2) Tree Permit for removal of 16 on-site trees.
Project Location:	1300 and 1310 C Street
Council District:	4
Assessor Parcel Number:	002-0084-001 and 002-0084-002
Applicant Name:	Albert Esquivel, SKK Developments
Applicant Phone	916-730-9328
Number:	
Applicant Email:	alberto@skkdevelopments.com
MRC Date:	N/A

NEIGHBORHOOD GROUPS

Preservation Sacramento, The River District (PBID), Alkali And Mansion Flats Historic Neighborhood Association

ATTACHMENTS	
Citizen Portal	Accela Citizen Access (Click on Attachments under " <i>Record Info</i> " tab to access attachments)
AGENCY COUNTER ^{NEW!}	https://agco.link/zDSwlb

Thank you,

Sierra Peterson

Associate Planner | Central Area | Planning Division

300 Richards Boulevard, 3rd Floor

Sacramento, CA 95811-0218

Email spectro.org Email spectro.org

Website portal.cityofsacramento.org/cdd



Good afternoon Sierra,

Please consider the following comments while conducting the Site Plan and Design Review of planning application DR23-088 on APN 002-0084-001-0000.

I will start by noting that as a property owner on C Street near the proposed project, I am excited to see continued infill in the Mansion Flats neighborhood and support ongoing efforts to help combat the housing shortage in the Sacramento area. The property, in its current condition, is an eyesore, and I am fully supportive of these parcels being developed, though I certainly preferred the previous development proposal for 16 townhomes each with garages.

However, as with many of our neighbors, I have significant concerns over the development proposal as it currently stands. Specifically, the majority of my concerns are over the parking and traffic associated with the construction of 126 new studio apartments without a single dedicated parking space. During the Alkali & Mansion Flats Historic Neighborhood Association meeting on June 5, it was disclosed that it has been assumed that 70% of new residents would have a vehicle. While I believe that number is low given that a fair share of the units would be likely to house couples and other two-resident partnerships, even this overly optimistic percentage would result in 88 new resident vehicles that would need to be parked on the street in an area where street parking is already limited. This does not even consider that these residents will have guests who are also likely to have vehicles that will also require parking. It should go without saying that this would significantly strain parking across a multi-block radius while also adding significant traffic safety concerns on a street where drivers cutting between 12th and 16th frequently drive double the speed limit and fail to stop at the stop sign on 14th.

As both an environmentalist and an environmental planner, I would absolutely love to see more residents utilizing public transit and reducing reliance on motor vehicles; however, I have to be realistic in acknowledging that we are a long way from this being a reality. Allowing developers to construct such a large number of units without any parking would not further this cause, even if that is an intent. Instead, it allows them to maximize their profit while creating harmful impacts to the adjacent properties and wider community. I would propose that you deny SKK's current proposal and require them to modify their application to include below ground parking for 50% of the units. This would still encourage public transit utilization and, although it would also still increase local street parking demands, it would be by a much less significant amount.

In addition to the parking, as you can imagine, I am concerned about the noise associated with such a large number of new residents and would be much more supportive of a development with fewer residents.

Finally, I do not believe this developer should be permitted to deviate from the City's open space requirements. The City of Sacramento has codified the need for 25 square feet of open space per dwelling unit, and this code should be upheld during this Plan and Site Review. There are many social and health benefits that were no doubt taken into consideration when this land use requirement was codified, and

those should not be forgotten. The wider Midtown area is such a desirable place to leave because of the extent of trees and open spaces, and this new development should be consistent with that aesthetic and legal requirement.

Ultimately, I believe our neighborhood would benefit from a lower density housing development consistent with the open space requirements codified by the City, and with parking for half of residents. Given that the applicant requires a zoning change for the subject parcels, I will also note that I believe mixed use is the best use of space for infill development at this location as one of the last areas of the city center that has not been fully developed and is generally limited in commercial real estate despite having ample housing.

I hope you will thoughtfully consider these concerns, as well as those of my neighbors, during this review process.

Thank you,

Jacqueline

Hi Ms. Peterson,

My name is Kristen Caprino, I live at Icon at 14th and C. I recently learned that SKK Developments plans to build a three story residential building with 127 apartments right next door to my house. This is a massive change from their original plan to construct single family homes on that lot. My home is located at the end of my development and my windows face directly to the lot that SKK plans to develop on. While I do not oppose the idea of some development next to my home, I do not believe squeezing as many units as possible on the small lot is the best use of the space (albeit certainly the most economically advantageous for SKK).

My neighbors and I have been discussing the impact this development will have on our homes. We all moved into these homes within the last three years and could have never anticipated there would be 120+ people moving in directly next door. The increase in noise and traffic and decrease in available street parking that will inevitably result from this project will undoubtedly have a negative impact on the community. Many of us moved to this area of midtown because it is quieter than other areas and has available street parking but if these plans proceed as proposed, the peace we enjoy in this area will be destroyed.

The plans could be somewhat conceivable if they weren't quite so large. I know SKK has built large developments along 16th street including H16, Eleanor, and the Mansion, but those developments are easily distinguished from 13 and C as those developments were built on an existing major egress road. That area has always been densely populated and has always had major traffic along 16th. 13 and C is simply not the same as 16th, and a large apartment building as proposed here would be completely out of place.

It is unfortunate there was a stark change from the original proposed plans for this project but I hope the City strongly considers its residents opinions and best interests over the hopes for increased revenues of a large development company.

Thank you very much for your time, Kristen Caprino

Sierra Peterson

Subject:

FW: Proposed Development 1300 C Street

From: Christopher Gould <<u>christopher.e.gould@gmail.com</u>>
Sent: Wednesday, December 27, 2023 10:48:52 AM
To: Katie Valenzuela (City) <<u>kvalenzuela@cityofsacramento.org</u>>; Tom Pace <<u>TPace@cityofsacramento.org</u>>
Cc: Erica DiScala <<u>EDiScala@cityofsacramento.org</u>>
Subject: Re: Proposed Development 1300 C Street

Hi Katie,

Thank you for your reply. Despite the new law being passed doesn't mean it's the right thing to allow so many new residences (125+) without parking and the fact they're using the light rail as justification is a joke. Whomever is approving this should visit the 12th street station any time during the day, but especially at night, and they'll realize that it is incredibly unsafe and underutilized due to the fact that it is effectively a space where unhoused people camp out and hang around. The light rail is also not operating 24hrs a day which presents another barrier to using this public transportation as a legitimate alternative to having a car, let alone having a car and parking it on a street where sufficient parking doesn't come close to existing to cover current residents and 125+ new ones.

If they did any due diligence and drove past here on Sunday nights when you aren't allowed to park on C street because of street sweeping they would see there are hardly any parking spaces available and that's just with current residents. Same goes for 14th street on Wednesdays and Thursdays. The other commercial building kitty corner to C&14th is becoming much more active as well which is great bringing people to the area and providing entertainment, sadly this will likely be impacted when patrons can't find parking spaces. Not to mention the dispensary opening back up on C&13th. Parking is going to become so incredibly contentious.

Hi Tom - I have not heard from the Senior planner, perhaps he could reach out via email to set a time for a zoom call.

Thanks, Chris

On Thu, Dec 14, 2023 at 3:37 PM Tom Pace <<u>TPace@cityofsacramento.org</u>> wrote:

I've asked Karlo Felix, Senior Planner, to reach out to Mr. Gould to discuss his concerns.

From: Katie Valenzuela (City) <<u>kvalenzuela@cityofsacramento.org</u>>
Sent: Thursday, December 14, 2023 3:10 PM
To: Christopher Gould <<u>christopher.e.gould@gmail.com</u>>
Cc: Erica DiScala <<u>EDiScala@cityofsacramento.org</u>>; Tom Pace <<u>TPace@cityofsacramento.org</u>>
Subject: RE: Proposed Development 1300 C Street

Hello Christopher,

Thank you very much for reaching out about your concerns. I'm looping in Tom Pace from Community Development so he can help ensure the right planner is informed about your concerns and can respond appropriately.

Just as background, in case SKK didn't mention it, they have agreed to deed restrict the affordable units for people with vouchers. Those housing vouchers will cover the rent for those affordable units (half of the planned units, if I remember correctly), so we'll be able to use them to get people currently or at risk of experiencing homelessness into housing at this building when completed.

Many of those folks may have cars, but state law has been changed to prohibit cities from requiring parking at any new building. What we can do, however, is be proactive about residential parking restrictions to ensure that the cars from these new tenants do not simply flood the streets when the building is completed.

I will defer to planning staff to weigh in on other design considerations. I definitely appreciate you naming specific concerns so they address those to the best of their ability.

I will say that SKK has completed numerous projects in our community that have been very successful. They shifted this project after the City agreed to provide some gap financing to allow much-needed affordable units to be included. They are very invested in the area – as are we at the City – so I'm sure will be willing to make any changes possible to ensure community concerns are addressed.

Best,

Katie

From: Christopher Gould <<u>christopher.e.gould@gmail.com</u>>
Sent: Wednesday, December 13, 2023 4:06 PM
To: D4 Constituent Services <<u>d4constituentservices@cityofsacramento.org</u>>; Katie Valenzuela (City)
<<u>kvalenzuela@cityofsacramento.org</u>>
Subject: Proposed Development 1300 C Street

Hi Katie,

I am writing to you as your constituent in District 4 and a concerned neighbor of a proposed development at 1300 C street, with the hope you might be able to advise what type of action or support the community could get to oppose such a development.

Per the City of Sacramento DR023-088 (project code) the plan is for 126 units (420sqft each) to be built on the vacant lot, with <u>no parking</u> to be provided on the lot because it meets a requirement to be within 0.5 miles of public transport (light rail).

It is great to see the vacant lots being developed but this is not the right fit or solution for the area, it's only going to create more issues and resentment in the community and to justify no parking because it's in close proximity to the light rail is a stretch.

We attended a presentation and listened to the responses to the concerns raised by the community members during a meeting in June held by the developer (SKK) and at the time were increasingly more concerned about this project than we were previously. The feedback presented did absolutely nothing to assuage any concerns, and in fact exposed just how little thought has gone into not only the project itself, but the impact it will have on the surrounding community and how it will impact the neighborhood.

Below is further information on many of the issues shared by surrounding neighbors. Several emails have been sent to the City Planner and at this stage no final decision has been made, they stated it would need to be approved at director level and there will be a hearing scheduled at that time.

<u>Parking</u>

The developer stated at least 70% of occupants are expected to have vehicles based on existing properties, therefore that would require approximately 90 street parking spaces for the residents alone with no consideration for their guests. There is barely enough parking for current residents and homeowners on C Street as it is, and the addition of these studios will only increase tension in the community, fighting over scarce parking spaces.

Affordability/Target Demographic:

A 420sqft. studio with no parking, and only 43SF of open living equates to 14% less space than the minimum required by State law of 50 SF per unit for a multi-family residential project. These plans are openly violating regulations that exist for a reason. It was stated the rental price will be \$1400 per month for affordable and \$1500-\$1600 for market rate. They indicated that the target demographic is largely service workers earning ~\$5,000/month – we cannot comprehend where they obtained their market research they claimed to do, but it is woefully lacking and out of touch.

Noise/Light Pollution:

This many residents on this size of a parcel, with guests and small living space will undoubtedly lead to the outdoor area being overused which is insufficient in size per code and will come with excessive noise pollution.

As the owners of the home that sits directly next to the planned transformer, gate and outdoor staircase area, it is completely obvious that not a single thought nor care was considered for existing homes, furthermore their plans just refer to surrounding homes as 'buildings'. The placement of the aforementioned items is done so to avoid a nuisance and eyesore to their own residents, not existing neighbors. The sound pollution from tenants opening and slamming the gate, bounding up and down outdoor stairs, and ripping out the existing beautiful trees to make way for this building will severely diminish not only our property value but more importantly our quality of life and other neighbors sharing the boundary.

Amenities/Transport:

<u>Bicycles -</u> The bicycle parking proposed is preposterously inadequate and further demonstrates the developers' intentions of cramming as many units in as possible without any consideration for quality of life of the tenants. If the goal of a development is to discourage tenants from owning or using cars, a 2 to 1 unit to bicycle parking offering is antithetical to that very foundation. Not only that, developing a non-parking apartment building where there aren't even existing bike lanes indicates the developers have not even considered the actual impact of the surrounding environment. When the lack of bicycle parking and unsafe streets for cyclists was brought to SKK's attention, they appeared unaware and uncaring during the meeting, virtually shrugging their shoulders, and dismissing it as a "city issue," that we just have to live with.

Light Rail

This location is across the street and directly on the street where known homeless camp-outs and weekly outreach events are located. There are constant issues of overdoses, camping out with fires, and blocking the sidewalk on C St leading to the 12th St. light rail station. It is simply not safe to travel on foot after dark or in inclement weather. We have lived here for nearly 2 years now and have never once even considered taking the light rail to get anywhere in the city, as it is both dangerous and inconvenient. The location will deter and prevent the intended demographic of young single people, especially women, from even wanting to access the nearby light rail station on 12th St, which is arguably one of the most unsafe locations. This will directly increase the already crowded number of residents fighting for parking spaces on C, leading to tense neighbor relations with the surrounding homeowners and residents.

The lack of inclusion of mixed-use, commercial or retail space into this project only further substantiates the utter disregard for developing a thriving community space that can contribute to the overall betterment of this particular area of Mansion Flats. The insular, highly secured/gated nature of the project demonstrates the lack of community built into the project and is evidentiary of the developers single-threaded goal: to squeeze as many people into as small a space as possible.

Looking Forward:

SKK indicated they will hire their third-party management contractor to manage this property and will likely on-sell it at some point, therefore further demonstrating they do not have a vested interest in the future of the area.

They admitted that the current market would not allow them to make enough profit or have enough buyers for a more reasonable proposal, such as the 16 mixed-use condos they had proposed previously.

Thank you and appreciate your time,

Christopher Gould

CITY OF SACRAMENTO COMMUNITY DEVELOPMENT DEPARTMENT DIRECTOR HEARING

300 Richards Blvd, 3rd Floor, Sacramento, CA 95811

STAFF RECOMMENDATION

Staff recommends the Design Director approve with conditions the Site Plan and Design Review for an addition to a single family residence for project known as file **DR24-006**. Draft Findings of Fact and Conditions of Approval for the project are included below.

Project Information:

<u>Site Plan and Design Review</u> to construct an attached carport to an existing singleunit dwelling with a deviation to be within the side yard setback on a 0.21-acre lot in the Single-Unit Dwelling Zone (R-1) within the Citywide Design Review District.

Location:		1036 46th St, Sacramento, CA 95819 (District 4)
Assessor's Parcel Number	S:	008-0152-004-0000
Applicant:		Rich Cazneaux 1036 46th St, Sacramento, CA 95819
Property Owner:		Rich Cazneaux 1036 46th St, Sacramento, CA 95819
Project Planner:		Rod Lawlor, (916) 204-9787
General Plan Designation: Community Plan Area: Design Review Area: Existing Land Use of Site: Existing Zoning:		Traditional Neighborhood Low Density (TNLD) East Sacramento Citywide SPDR Residential Single-Unit Dwelling Zone (R-1)
Surrounding Zoning and La	and Us	e:
North (interior): South (interior): East (front): West (rear):	· · ·	Residential Residential
Site Information:		
Existing Property Size:		60' x 150' (0.21 acre)

Parking Required: Topography: Street Improvements: Utilities:	One Flat Existing Existing	
Setback Information:	Desuined	Dranaadu
Interior side-yard (South)	Required: 5' - 0"	Proposed: 3' - 2"

Project Analysis:

The project site consists of an existing single-family residence located at 1036 46th St. The applicant is proposing to construct a 13' x 26' attached carport. The roof will utilize clay tiles and the walls and columns will be smooth stucco to match existing.

The proposed side-yard setback for the carport is 3' - 2", which does not meet Sacramento City Code (SCC) 17.204.240(B) minimum requirements for interior side-yard setbacks:

- 1. Unless paragraph 2 or 3 of this subsection applies, the minimum interior side-yard setback is five feet.
- 2. A minimum interior side-yard setback of three feet applies to interior lots having a width of less than 52 feet and corner lots having a width of less than 62 feet

The applicant is requesting a deviation through a public hearing to allow the carport to be built within the side-yard setback. The reasons given for requesting this deviation include:

- The residence once had a carport that was removed by previous owners. The current owners would like to bring back the carport to match the neighborhood look and charm.
- Several neighbors on the block have carports with side-yard setbacks that do not meet minimum setback requirements, with setbacks ranging from 0" to 12".
- A 13' wide carport will allow for the accommodation of a vehicle and provide access to a side entrance to the residence an entrance that has gone unused without a carport.

Staff supports the deviation and finds that the project is consistent with the purpose and intent of applicable design guidelines and development standards in that: 1) The proposed side setback is a minor encroachment into the required setback; 2) the proposed side encroachment does not significantly impact the neighbor to the south; and 3), the encroachment does not negatively impact the established assemblage of homes on the block and complements them through the use of an established accessory structure found throughout the neighborhood.

Property owners, tenants, and neighborhood groups within 500 feet of the subject site received notification of the public hearing occurring on Thursday, May 9, 2024. Neighborhood associations that were notified for the hearing include Preservation Sacramento, East Sacramento Improvement Association, East Sac Give Back, and East Sacramento Chamber

of Commerce. Staff have received one response in support for this project, from the East Sacramento Community Association Land Use Committee (Exhibit B).

Environmental Considerations:

The project is also determined to be exempt from the provisions of the California Environmental Quality Act (CEQA) under Class 11, Section Number 15311, Accessory Structures.

200-Year Flood Protection:

"State Law (SB 5) and Planning and Development Code chapter 17.810 require that the City must make specific findings prior to approving certain entitlements for projects within a flood hazard zone. The purpose is to ensure that new development will have protection from a 200-year flood event or will achieve that protection by 2025. The project site is within a flood hazard zone and is an area covered by SAFCA's Improvements to the State Plan of Flood Control System, and specific findings related to the level of protection have been incorporated as part of this project. Even though the project site is within a flood hazard zone, the local flood management agency, SAFCA, has made adequate progress on the construction of a flood protection system that will ensure protection from a 200-year flood event or will achieve that protection by 2025. This is based on the SAFCA Urban level of flood protection plan, adequate progress baseline report, and adequate progress toward an urban level of flood protection engineer's report that were accepted by City Council Resolution No. 2016-0226 on June 21, 2016 and the SAFCA 2023 Adequate Progress Annual Report accepted by City Council Resolution No. 2023-0337 on October 24, 2023."

Draft Findings of Fact:

- 1. The design, layout, and physical characteristics of the proposed project are consistent with the general plan and any applicable specific plan or transit village plan in that the proposal adheres to the goals and policies of the general plan land use designation of Traditional Neighborhood Low Density.
- 2. The design, layout, and physical characteristics of the proposed project are consistent with all applicable design guidelines and with all applicable development standards in that the proposal meets all applicable development standards for the R-1 zone and the design standards for single-unit dwellings, <u>with exception</u> for the side-yard setback. Justification for the deviations can be found within the Project Analysis section of the staff report.
- 3. All streets and other public access ways and facilities, parking facilities, and utility infrastructure are adequate to serve the subject site and comply with all applicable design guidelines and development standards.
- 4. The design, layout, and physical characteristics of the proposed project are visually and functionally compatible with the surrounding neighborhood in that the exterior design and massing of the project are compatible with the design of the adjacent residential dwellings.

- 5. The design, layout, and physical characteristics of the proposed project ensure energy consumption is minimized and use of renewable energy sources is encouraged. The proposed carport will assist in the passive shading and cooling of the façade in which it is attached.
- 6. The design, layout, and physical characteristics of the proposed project are not detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will not result in the creation of a nuisance and will not be a detriment to the neighborhood.
- 7. The project site is within an area for which the local flood-management agency has made adequate progress (as defined in California Government Code section 65007) on the construction of a flood-protection system that, for the area intended to be protected by the system, will result in flood protection equal to or greater than the urban level of flood protection in urban areas for property located within a flood-hazard zone, as demonstrated by the SAFCA Urban Level of Flood Protection Plan and Adequate Progress Baseline Report and the SAFCA Adequate Progress Toward an Urban Level of Flood Protection Engineer's Report, each accepted by the City Council on June 21, 2016 (Resolution No. 2016-0226), and the SAFCA 2023 Adequate Progress Annual Report accepted by the City Council on October 24, 2023 (Resolution No. 2023-0337).

DRAFT CONDITIONS OF APPROVALS:

Planning / Design Review

- 1. The proposed construction of the attached carport is approved per attached plans and conditions of approval.
- 2. Any modifications to the approved project shall be subject to the review and approval of Planning staff (and may require additional entitlements).
- 3. The carport shall be located a minimum of 3' 2" away from the south property line per approved plans.
- 4. Provide the following building materials on the residential addition as indicated per approved plans:
 - a. Clay tile roof to match existing
 - b. 2x6 fascia to match existing
 - c. Smooth coat stucco siding to match existing
- 5. The applicant shall obtain all necessary building and encroachment permits prior to commencing construction. No permits shall be issued within the 10-day appeal period.
- 6. Plans shall be consistent with the attached exhibits. If there are any discrepancies between the exhibits and the conditions, the conditions will take precedence.

- 7. All other notes and drawings on the final plans as submitted by the applicant are deemed conditions of approval. Any work that differs from the final set of plans approved by the Planning staff shall be subject to review and approval prior to issuance of a building permit.
- 8. The applicant shall obtain all necessary building permits prior to commencing construction. No permits shall be issued within the 10-day reconsideration period.
- 9. This approval shall expire in three (3) years from the approval date.

Advisory Notes

The following advisory notes are informational in nature:

- ADV.1. Trees on adjacent parcels While not required by city code for unprotected trees, the applicant has an obligation to protect trees owned by others on adjacent properties and should obtain permission to perform any work such as pruning or excavation within the dripline of such tree. Case law in California requires that reasonable care be taken to protect trees owned by others.
- ADV.2. A 1-hour rated wall is required on the south side where the fire separation distance is less than 5'. Openings in the south wall cannot exceed 25%.

ATTACHMENTS:

Exhibit A: Site Plan, Floor Plan, Elevations Exhibit B: Letter of Support

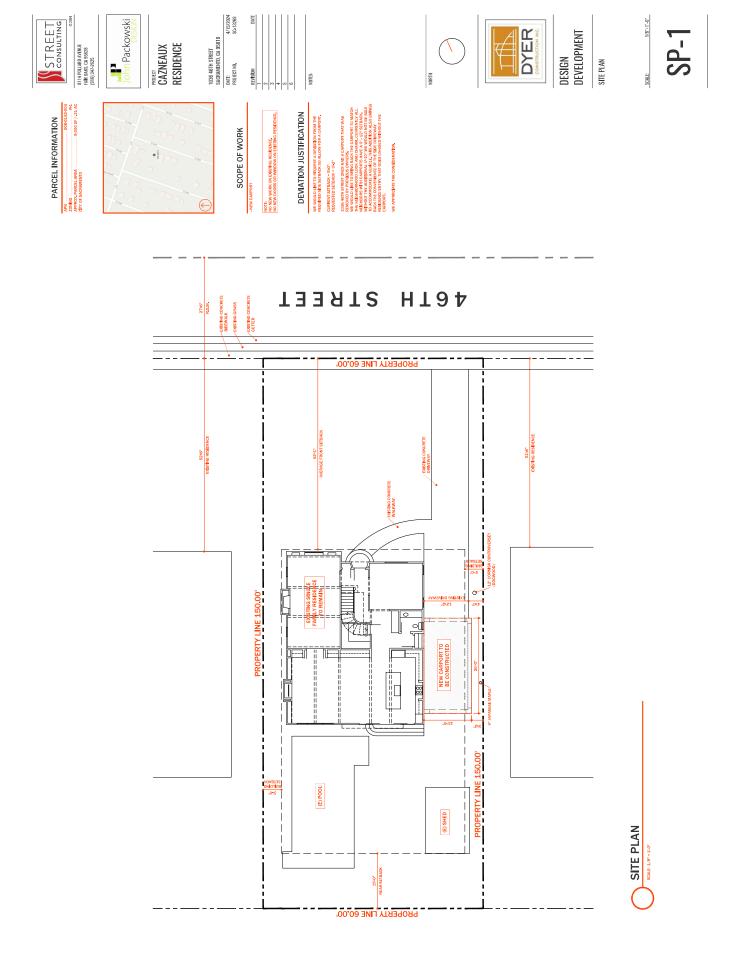
Date: 2 -07'00'

Digitally signed by Rod Lawlor Date: 2024.05.02 15:18:01 -07'00'

Rod Lawlor Urban Design Staff

2024 16:10 PDT) Matthew Site

Matthew Sites, AIA, LEED AP Senior Architect





3,359 SF 216 SF 338 SF

EXISTING RESIDENCE DETACHED 1 CAR GARAGE NEW CARPORT

PROJECT DATA









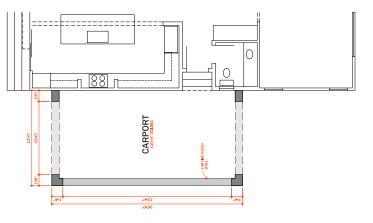
1/4"=1'-0" SCALE:

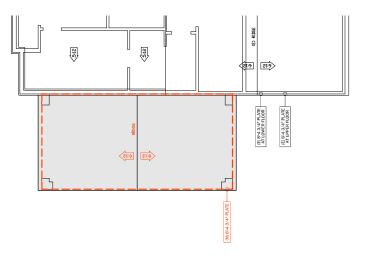
A-1

EXISTING WALL FRAMING
2x6 DF#2 AT 15" 0.0, WOOD FRAMED WALL

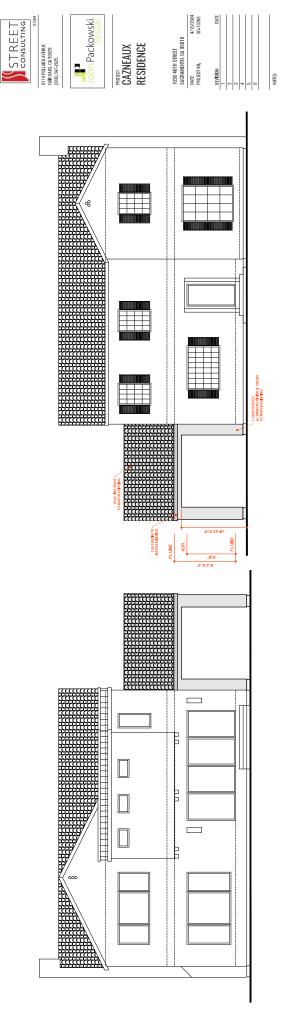
WALL LEGEND





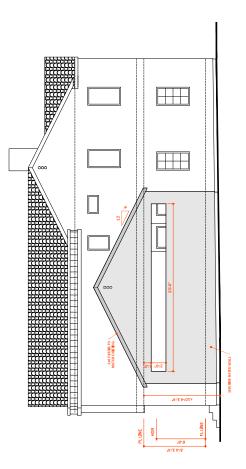
















Hi Mr. Lawlor,

My apologies for the delayed response.

The East Sacramento Community Association Land Use Committee is supportive of the proposed projet.

Thank you,

Brian Holloway ESCA Land Use Chair

916-996-2019

From: Rod Lawlor <RLawlor@cityofsacramento.org>

Date: Monday, February 5, 2024 at 4:38 PM

To: Rod Lawlor <RLawlor@cityofsacramento.org>

Subject: Project Routing: DR24-006, New Attached Carport (1036 46th St)

The Planning Division has received a new development application. Please see the information below regarding project file number **DR24-006**. The application materials can be accessed from the **ATTACHMENTS** link below.

Please send your written comments to me by 2/26/2024. Do not hesitate to contact me if you have any questions about the project or issues accessing the application materials.

PROJECT INFORMATIO	PROJECT INFORMATION	
Project Name:	New Attached Carport	
Project Summary:	Request for Site Plan and Design Review to construct a new attached carport to the existing single-unit dwelling on a 0.21-acre parcel in the Single-Unit Dwelling Zone (R-1) within the Citywide Design Review Area. Applicant is requesting a side yard setback deviation.	
Project Location:	1036 46 th St, Sacramento, CA 95819	
Council District:	4, Represented by Mayor Steinberg's office	
Assessor Parcel Number:	008-0152-004-0000	
Applicant Name:	Rich Cazneaux	
Applicant Phone Number:	(916) 717-4444	
Applicant Email:	rich@eastsac.com	

NEIGHBORHOOD GROUPS

East Sacramento Improvement Association East Sac Give Back East Sacramento Chamber Of Commerce East Sacramento Preservation (ESP) Preservation Sacramento

ATTACHMENTS	5
Citizen Portal	Accela Citizen Access (Attachments under <i>"Record Info"</i> tab to access attachments)
Agency Counter	https://agco.link/gxbwvu

Rod Lawlor

Development Services Technician II

City of Sacramento Community Development Department 300 Richards Blvd., 3rd Floor | Sacramento, CA 95811 (916) 204-9787 <u>rlawlor@cityofsacramento.org</u>

sacramento I 2040

City of Sacramento General Plan Update and Climate Action Plan

Interested in the City's General Plan Update and Climate Action & Adaptation Plan? Visit <u>Sacramento 2040</u> to learn more!